



EAST CYPRESS CORRIDOR SPECIFIC PLAN

Adopted March 13, 2006

FINAL

East Cypress Corridor Specific Plan

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LIST OF ACRONYMS AND ABBREVIATIONS

American Disabilities Act (ADA)	Federal Emergency Management Agency (FEMA)
California Environmental Quality Act (CEQA)	Final Development Plan (FDP)
Central Valley Project (CVP)	General Plan Amendment (GPA)
Commercial (CO)	Homeowner Association(s) (HOA)
Community Development Director (Director)	Ironhouse Sanitary District (ISD)
Contra Costa County Flood Control and Water Conservation District (CCCFCWCD)	Local Agency Formation Commission (LAFCO)
Contra Costa County Public Works Department (CCCPWD)	Los Vaqueros Project (LVP)
Contra Costa Water District (CCWD)	Mean Sea Level (MSL)
Covenants, Conditions and Restrictions (CC&R's)	Million gallons (Mg)
Diablo Water District (DWD)	Million gallons per day (Mgd)
Dutch Slough Tidal Marsh and Floodplain Restoration Project (Dutch Slough Restoration Area)	Mitigation Monitoring Program (MMP)
East Bay Regional Park District (EBRPD)	National Pollutant Discharge Elimination System (NPDES)
East Cypress Corridor Specific Plan Area (Specific Plan Area)	Pacific Gas and Electric (PG&E)
East Cypress Corridor Specific Plan (Specific Plan)	Preliminary Development Plan (PDP)
East County Fire Protection District (ECFPD)	Reclamation District 799 (RD799)
Environmental Impact Report (EIR)	Sphere of Influence (SOI)
	“Specific Plan” (SP)
	“Specific Plan-1” (SP-1)
	Village Center (VC)
	Western Area Power Administration (WAPA)

SECTION 1. SPECIFIC PLAN EXECUTIVE SUMMARY

The East Cypress Corridor Specific Plan (Specific Plan) encompasses approximately 2,546 acres of land located within the City of Oakley Sphere of Influence (SOI) boundaries. The regional context and local setting of the Specific Plan are illustrated in the “Regional Location Map,” Exhibit 1 and the “Vicinity Map,” Exhibit 2. The Specific Plan Area is planned for annexation to the City of Oakley pursuant to adoption of the East Cypress Corridor Specific Plan and certification of the accompanying Environmental Impact Report (EIR).

The East Cypress Corridor Specific Plan has been divided into six planning areas as illustrated on Exhibit 3, “Land Use Planning Areas.” Planning Areas 1-4 comprise approximately 1,646 acres proposed for development of residential, commercial, park, open space and public facility uses as part of the adoption of the East Cypress Corridor Specific Plan. Development plans for Planning Areas 1-4 are proposed by KB Home, Shea Homes, D.R. Horton and Bethel Island, LLC. Planning Area 5 includes approximately 269 acres currently being developed by Shea Homes as the Summer Lake South planned community of up to 628 residential dwelling units. The zoning and development plans for Summer Lake South in Planning Area 5 were approved by Contra Costa County in 1993. Planning Area 6 includes approximately 631 acres of property owned by individual homeowners and other landowners. No development proposals within Planning Area 6 are being considered with the adoption of the Specific Plan.

An EIR has been prepared as part of the review of the Specific Plan by the City of Oakley. Within the EIR, the proposed development plans for Planning Areas 1, 3 and 4 have been

evaluated at a “project level.” The proposed development for Planning Area 2 and the potential future development of Planning Area 6 are analyzed at a programmatic level in the EIR. The potential development capacity of these areas is addressed in the Specific Plan. Land uses within Planning Area 6 are consistent with the City’s General Plan land use designations. Development within Planning Area 5 has been approved for development pursuant to certification of an EIR by Contra Costa County in 1993, as referred to in the East Cypress Corridor Specific Plan EIR.

The purpose of the Specific Plan is to provide the City of Oakley with a comprehensive plan that controls development within the East Cypress Corridor Area (Specific Plan Area), ensuring the creation of a livable community designed as a group of compatible neighborhoods with connectivity to parks, open space, schools and commercial services. The Specific Plan will also provide the City of Oakley with a mechanism to manage growth leading to the installation of adequate infrastructure and public services for the new and existing neighborhoods within the Specific Plan Area.

The Specific Plan establishes the land use plan, infrastructure plan, development regulations and design guidelines which will govern development of a master planned community. The master development plan as illustrated in the “Illustrative Land Use Master Plan,” Exhibit 4, is consistent with the goals and policies of the City of Oakley General Plan, which combines livable residential neighborhoods served by open space and recreational areas. Planned retail uses will provide for the commercial needs of the community and the surrounding area. The Specific Plan assures that develop-



Exhibit 1
Regional Location Map



ment of new land uses is implemented in a manner compatible with the surrounding existing residential and agricultural land uses.

A network of multi-use trails, on-street bike lanes, and pedestrian corridors provides bicycle and pedestrian access linking residential, open space and commercial areas. Bicycle and pedestrian accessibility is also provided between residential development and planned elementary and middle school sites distributed throughout the Specific Plan Area.

The City of Oakley General Plan designates the Specific Plan Area for development of up to 5,763 residential dwelling units in a variety of density ranges. It also includes commercial, agricultural, delta recreation, commercial recre-

ation, public, and semi-public land use designations. Residential land uses are further delineated with areas assigned for Agriculture – Limited Single Family Residential, Single Family Residential Very Low density, Single Family Residential Low density, Single Family Residential Medium density, Single Family Residential High density, Multi-Family Residential Low density, Multi-Family Residential Medium density and Mobile Home categories. Residential development totaling 5,759 residential units¹ is proposed within the Specific Plan Area. The Specific Plan also includes ten natural gas well sites located throughout the Specific Plan Area.

¹ Includes 544 existing residences and 1,330 residential units approved earlier by the County.



Exhibit 2
Vicinity Map





Exhibit 3
Land Use Planning Areas



As part of the project, a General Plan Amendment (GPA) will be adopted by the City. The GPA will change the General Plan Land Use Diagram to “Specific Plan” (SP) in order to redistribute land uses within the Specific Plan Area. This will allow for a greater variety of residential housing types, an increase in total commercial acreage from 69 to 93 acres, a change in the land use designation from Agricultural Limited to a park land use for approximately 9 acres, and a change in land use designation of approximately 6 acres from Delta Recreation to Light Industrial.

GOVERNING DOCUMENTS

Development within the East Cypress Corridor Specific Plan Area will be governed by several documents as follows:

- The City of Oakley 2020 General Plan adopted December 16, 2002, as amended, which establishes policies governing land use, circulation, housing, conservation and open space, noise, safety and public facilities within the East Cypress Corridor Specific Plan Area.
- The East Cypress Corridor Specific Plan to include a Land Use Plan, Infrastructure Plan, Design Guidelines, Development Regulations and an Implementation Plan.
- The City of Oakley Municipal Code, as applicable to the project, where the East Cypress Corridor Specific Plan is silent on development standards and regulations.
- The City of Oakley Municipal Code regulating the subdivision of land within the East Cypress Corridor Specific Plan Area.
- Covenants, Conditions and Restrictions (CC&R’s) to be established by the developers within the East Cypress Corridor Specific Plan as a means of ensuring and enforcing quality design and development of the master planned community.



View of Dutch Slough looking north

SPECIFIC PLAN COMPONENTS

The East Cypress Corridor Specific Plan is organized into the following sections in addition to Section 1, Executive Summary.

SECTION 2 INTRODUCTION

The Introduction serves to acquaint the reader with:

- The project setting
- A general description of the project proposal
- The vision and goals of the East Cypress Corridor Specific Plan
- The entitlements to accompany the East Cypress Corridor Specific Plan
- The relationship of the East Cypress Corridor Specific Plan to the City of Oakley General Plan, as amended, and the City of Oakley Zoning Ordinance

**SECTION 3
EXISTING CONDITIONS**

This section describes the setting for the East Cypress Corridor Specific Plan outlining the existing physical conditions on and around the Specific Plan Area.

**SECTION 4
LAND USE**

The Land Use Section describes residential and commercial planning areas, open space areas, parks, trails and public facilities within the planned community.

**SECTION 5
INFRASTRUCTURE AND
COMMUNITY FACILITIES**

This section provides information on circulation improvements, planned backbone water, sewer, and storm drain systems, grading for the proposed development and a discussion of public utilities and services for the Specific Plan Area.

**SECTION 6
DEVELOPMENT REGULATIONS**

The Development Regulations specify the permitted uses and standards regulating the development of various land uses. Landscape development standards are also provided. The relationship of the Specific Plan development regulations to the City of Oakley Zoning Ordinance is described in this section.

**SECTION 7
DESIGN GUIDELINES**

The East Cypress Corridor Specific Plan Design Guidelines are intended to ensure that a high quality of site planning, landscaping and architectural design is achieved for the development. Streetscapes, entries, edge treatments, walls, fencing, lighting, signage, building setbacks and architectural styles are addressed in the Design Guidelines.

**SECTION 8
IMPLEMENTATION**

The policies and procedures for the City's review and approval of specific development proposals within the Specific Plan Area are presented in this section. The Implementation section provides the procedures for interpreting and amending the East Cypress Corridor Specific Plan as necessary. A summary of project financing and maintenance responsibilities for new and existing development within the Specific Plan Area is also included.

**SECTION 9
GENERAL PLAN CONSISTENCY**

This section includes a description of applicable policies outlined in the City of Oakley General Plan and a description of the relationship of the East Cypress Corridor Specific Plan to each policy.

**APPENDIX A
CONCEPTUAL SITE PLANS,
PLANNING AREAS 1-4**

**APPENDIX B
6' WOOD FENCE DETAIL**



Exhibit 4
 Illustrative Land Use Master Plan



SECTION 2. INTRODUCTION

2.1 SPECIFIC PLAN OVERVIEW

The East Cypress Corridor Specific Plan is an approximately 2,546 acre area within the City of Oakley SOI located on the easterly boundary of the current City limits. The area is predominantly vacant, agricultural land with residential development located at the northerly and easterly boundary of the area and along the southern side of East Cypress Road. The City of Oakley plans to annex the area and proposes that a specific plan be adopted in order to ensure that future development is consistent with the City's General Plan goals to maintain the character and best qualities of the community and to ensure that the City of Oakley remains an economically viable city following the annexation of the area.

Within the entire 2,546 acre Specific Plan Area, approximately 1,646 acres, comprised of Planning Areas 1-4, are proposed for development of a new community combining environmental stewardship with safe and manageable neighborhoods designed at a human scale and located within walking distance of commercial areas, preserved open space, recreation areas, schools, and social gathering places.

Approximately 269 acres comprising Planning Area 5 are being developed by Shea Homes as the planned Summer Lake South residential community of 628 new residential dwelling units pursuant to land use approvals granted by Contra Costa County in 1993. The remaining approximately 631 acres of the Specific Plan Area, referred to as Planning Area 6, consist of existing residential and agricultural uses to be maintained and served by public facilities and commercial services planned as part of the new development.

2.2 PROJECT SETTING

The Specific Plan Area encompasses nearly 4 square miles of predominantly vacant land located south of Dutch Slough, west of Sandmound Slough, north of Rock Slough and northeast of the Contra Costa Canal. The Specific Plan Area is bisected by East Cypress Road in an east/west direction terminating at Bethel Island Road. Bethel Island and Jersey Island Roads traverse the area in a north/south direction. The Specific Plan Area is located adjacent to and east of the Dutch Slough Tidal Marsh and Floodplain Restoration Project (Dutch Slough Restoration Area), within two miles of the city limits of Brentwood, within four miles of the city limits of Antioch and within one mile of the town of Knightsen.

2.3 PROJECT HISTORY

The majority of the Specific Plan Area has historically been used for agricultural purposes including dairy farming and grazing. Developed properties within the Specific Plan Area include single family and multi-family residences located predominantly along the easterly and northerly boundaries behind non-engineered levees constructed adjacent to Dutch Slough and Sandmound Slough. These levees were originally built to protect agricultural uses and limited residences. Many of the existing residential uses were originally developed as vacation and week-end residences that have since evolved into year round occupied units. Rural residential lots are located adjacent to and south of East Cypress Road between the Contra Costa Canal at Jersey Island Road and Bethel Island Road.

In 1993, Contra Costa County approved a development plan and rezoning for approximately 678 acres of the Specific Plan Area permitting development by Shea Homes of up to 1,330 residential dwelling units, plus up to 200 second units, a golf course, lakes and open space.

On December 16, 2002, the City of Oakley adopted the City's 2020 General Plan designating the Specific Plan Area as the East Cypress Corridor Expansion Area and designating urban land uses for future development. On August 19, 2003, the Contra Costa County Local Agency Formation Commission (LAFCO) approved a proposed amendment to the City of Oakley SOI to include the Expansion Area within the City's SOI. On February 14, 2004, the City of Oakley determined that a specific plan should be prepared for the East Cypress Corridor Expansion Area and authorized the preparation of the East Cypress Corridor Specific Plan to implement the General Plan and to comprehensively plan for the staged annexation of the properties within the Specific Plan Area.

2.4 PURPOSE AND OBJECTIVES

2.4.1 PURPOSE

The City of Oakley has identified the need for a specific plan for the East Cypress Corridor Expansion Area in order to provide for appropriate growth management and comprehensive planning of new residential, commercial, light industrial, open space, and recreational land uses accompanied by adequate infrastructure and public facilities compatible with existing agriculture and residential land uses. Continued operation of existing natural gas wells and provision of sites for additional new gas wells, located within the Specific Plan Area, are also goals of the Specific Plan.

2.4.2 OBJECTIVES

The City of Oakley will pre-zone the Specific Plan Area as Specific Plan 1, "SP-1." Upon approval of the annexation requests submitted to LAFCO by the City, the Specific Plan Area will be rezoned to "SP-1." The Specific Plan will establish the land use plan, development standards, infrastructure requirements, and implementation requirements for the "SP-1" zoning. The City of Oakley will also amend the General Plan as follows:

- The General Plan Land Use Diagram will be changed for the Specific Plan Area to reflect the East Cypress Corridor Specific Plan land use plan as illustrated in Exhibit 6, "Land Use Plan."
- The General Plan allocation of commercial acreage for the Specific Plan Area will be increased from 69 to 93 acres in order to allow for development of a Village Center in Planning Area 1.
- Approximately 9 acres of residential land uses adjacent to East Cypress Road currently designated as "Agriculture Limited" will be changed to a park land use designation to accommodate the development of a community park within the Specific Plan Area. Approximately 6 acres of Delta Recreation land use will be changed to a Light Industrial land use designation to accommodate an existing boat storage facility, and the development of future public facilities.

The proposed General Plan Amendment will allow for more flexibility in the distribution of residential uses, commercial centers, and recreational and open space areas to efficiently implement the General Plan goals and objectives.

The East Cypress Corridor Specific Plan establishes the type and distribution of land uses, defines the development regulations and design guidelines for each land use, establishes loca-

tions for schools, preserves eleven gas well sites to accommodate new and existing gas wells, and describes the infrastructure requirements and public facilities necessary to serve the Specific Plan Area. The East Cypress Corridor Specific Plan also establishes the procedures and requirements to approve new development within the Specific Plan Area.

Upon certification of the project EIR and adoption of the Specific Plan and pre-zoning for the Specific Plan Area, the City will also formally submit their application for annexation of the Specific Plan Area as three separate but concurrent proposed boundary reorganizations. This action will facilitate control of developing properties pursuant to policies adopted as part of the Specific Plan.

2.5 COMMUNITY VISION

The vision for the East Cypress Corridor Specific Plan is established through the application of key guiding principles as described in the following list of goals.

Goal: Create a Livable Environment

The East Cypress Corridor Specific Plan combines traditional neighborhood design with a combination of employment and residential uses that balance housing, recreation, commercial services, and public facilities within a livable community. A livable community includes features such as:

- Preservation and enhancement of natural habitat.
- Facilitation of mobility through alternatives such as biking and walking.
- Opportunities for informal neighborhood interaction.
- Diverse architectural design of a high quality.
- Connectivity among neighborhoods.

- Diversity and choice of housing types and opportunities for a variety of lifestyles and economic segments of the marketplace.
- Commercial and service retail opportunities connected to residential areas through a network of bicycle and pedestrian trails.
- Schools within walking distance for students.
- Passive and active recreational opportunities located throughout the community.

Goal: Provide a Balance of New Land Uses with Natural Features and Existing Land Uses

Land uses and natural features are arranged within the Specific Plan Area to address the geographical features and environmental character of the site as follows:

- A human scale of development is planned, oriented to pedestrian activities with connectivity among residential neighborhoods, public facilities, job centers and open space/recreational areas.
- Neighborhoods are designed to allow for the preservation and enhancement of natural open space and provide residents with a rural, outdoor experience as well as active and passive recreational opportunities.
- Approximately 119 acres of wetlands and natural dune area, 5% of the Specific Plan Area, will be preserved as part of the development of Planning Areas 1, 2 and 5. Additional resource areas will be set aside as development of Planning Area 6 occurs.
- Centrally located commercial areas will be linked to residential neighborhoods by a network of bicycle and pedestrian trails.
- Public facilities will be developed to serve the areas proposed for new development as well as existing residential uses within the community.

- Accommodation is made for continued production of natural gas within existing active gas well sites and standards applied for development of new gas well sites.
- Accommodation is made for continued agricultural operations in areas not proposed for development as part of the Specific Plan.
- The continued use of the delta area for existing recreational activities is provided for.

Goal: Plan for a Circulation System Serving Motorists, Bicyclists, Equestrians and Pedestrians

The circulation plan for the East Cypress Corridor Specific Plan provides for comprehensive circulation through a system of arterial, collector and local streets which accommodate bicycle and pedestrian travel as well as the safe and efficient movement of automobiles. The Specific Plan accommodates equestrian travel as well.

- Bethel Island Road will be extended south from the intersection of East Cypress Road to Rock Slough. Bethel Island Road improvements will include a crossing at Rock Slough to be constructed in phases pursuant to the requirements of the City. The crossing at Rock Slough will be funded by the development and by mitigation funds collected by Contra Costa County.
- Arterial and collector streets will include landscaped parkways and pedestrian walkways separated from the street to create an intimate environment promoting social interaction.
- Streets will be designed to slow vehicular traffic through the use of traffic calming devices such as roundabouts, open space, landscaped areas and narrowed intersections to influence a driver's peripheral vision to encourage drivers to proceed more slowly.
- A system of bikeways will be integrated into the design of the community to encourage bicycle travel as an alternative to the automobile.

- A system of equestrian and multi-use trails will be built on top of the proposed engineered levees.

Goal: Provide for Adequate Public Community Facilities

The East Cypress Corridor Specific Plan provides needed public facilities to serve the community as follows:

- Development of three public elementary schools and one middle school.
- Development of a fire station to serve the community.
- Development of a comprehensive engineered levee system for protection from the 100-year storm event consistent with Federal Emergency Management Agency (FEMA) requirements.
- Accommodation for continued production of natural gas within existing active gas well sites and provision of standards for development of new gas well sites.
- Provision of new water, sewer and drainage facilities to serve existing and future residents within the Specific Plan Area.
- Provision of connections of planned bike paths to City bikeway systems.
- Financial participation in the construction of a new bridge at Bethel Island Road and Rock Slough.

Goal: Combine Existing Natural Features with New Parks and Open Space

Significant natural habitat and scenic features are preserved and enhanced through environmental stewardship forming an integral component of the East Cypress Corridor Specific Plan. New public parks and open space amenities are provided to enhance the recreational opportunities to existing and future Oakley residents as follows:

- Provision of approximately 164 acres, 6% of the Specific Plan Area, as natural open space areas promoting a rural, outdoor experience to include multi-use trails allowing bicycle and pedestrian access to open space.
- Preservation of significant archaeological sites occurring throughout the community to be maintained as permanent open space, parks and/or public facilities.
- Provision of approximately 273 acres, 11% of the Specific Plan Area, as community parks, neighborhood parks and lakes connected by an open space corridor and trails, providing connections within walking distance to residential areas.
- A multi-use bicycle/pedestrian trail system will be developed within a central greenway connecting the northerly and southerly portions of the Specific Plan Area. This greenway also forms the backbone of the open space/trail system connecting to trails developed within roadways and engineered levees. Future connections will be provided to the Dutch Slough Restoration Area adjacent to the Specific Plan Area on the west.

Goal: Promote Exceptional Architecture and Site Planning

- Diverse and varied architecture combined with comprehensive site planning within the East Cypress Corridor Specific Plan will produce neighborhoods that have aesthetic and functional appeal, preserve residents' privacy, and retain environmentally and culturally sensitive areas within the Specific Plan Area.
- Streets will be linked together in a manner which accommodates pedestrians and cyclists connecting neighborhoods, open space, public facilities and recreational areas.
- A variety of housing including attached and detached housing types will be distributed throughout the Specific Plan Area within

identifiable neighborhoods, located close to schools, parks and open space.

- Higher density residential development will be located adjacent to amenities such as commercial uses, trails and open space.
- Residential neighborhoods are sited to maximize open space uses, and to preserve natural wetlands and dune areas.
- Residential neighborhoods are designed with houses addressing the street by:
 - Designing homes to a more human scale with porches, stoops and walkways creating opportunities for neighborly interaction.
 - Minimizing views of garage doors through setback requirements, location, design features and landscaping.
 - Incorporating varied architectural styles and elements within each neighborhood.
- Native plant materials and non-invasive ornamental landscape materials will be incorporated into the landscape plan for common areas and passive parks.
- Use of stone, timbers, metal and stucco within a hierarchy of gateways and entrance features to establish identifiable community character.
- Residential development will be equipped with technology for internet access allowing residents to shop and work on-line, thereby reducing vehicle trips.
- Streets are oriented in an east/west direction wherever feasible, allowing for a southerly orientation of residential development to promote energy efficiency.
- Large canopy trees will be utilized in public areas such as parkways, medians and the community park to achieve natural ventilation and cooling.

2.6 SPECIFIC PLAN SUMMARY

Of the total 2,546 acre Specific Plan Area, approximately 1,646 acres are proposed for development of a community within Planning Areas 1-4 with up to 4,036 residential dwelling units of varying housing types within a livable community setting. This includes 150 potential units within the Village Center. Approximately 269 acres of the Specific Plan Area are under development by Shea Homes as the 628 unit Summer Lake South planned community, pursuant to land use approvals granted by Contra Costa County in 1993. The remaining approximately 631 acres of the Specific Plan Area are not proposed for development as part of the Specific Plan. This area includes existing residential and agricultural land uses with General Plan land use designations that permit a maximum of 1,095 residential dwelling units. The East Cypress Corridor Specific Plan provides for connectivity of all residential areas to centrally located parks, and to commercial and recreational facilities through a network of pedestrian sidewalks, bicycle trails and landscaped greenways.

The East Cypress Corridor Specific Plan is comprised of six planning areas as described below. Existing ownerships and development entities for each Planning Area are illustrated on the “Planning Area/Ownership Map,” Exhibit 5.

PLANNING AREA 1

Approximately 704 acres of the Specific Plan Area are owned by Dal Porto/Lesher and proposed for development by KB Home. This Planning Area will contain 1,700 residential dwelling units in a variety of housing types as well as recreational and open space amenities. A Village Commercial Center is proposed on approximately 40 net acres located at the north-west corner of East Cypress and Bethel Island Roads, subject to special conditions as described in Section 8.10.2.2. An approximately 12 acre site will be reserved for development of an elementary school. In addition to the proposed

new development within Planning Area 1, approximately 105 acres will be reserved as natural wetlands/dunes areas, and approximately 30 acres will be provided as a lake amenity. Three gas well sites totaling 3.9 acres are located within Planning Area 1.

PLANNING AREA 2

Approximately 409 acres of the Specific Plan Area are owned by Shea Homes and proposed for development as Summer Lake North. Planning Area 2 was originally approved by Contra Costa County in 1993 for development of 702 dwelling units and a golf course. As part of the East Cypress Corridor Specific Plan, Shea Homes is proposing that this area be developed with 816 residential dwelling units in a variety of housing types, public recreational and open space amenities. Approximately 20 acres will be reserved for development of a middle school. Approximately 1.7 acres are proposed for development of a small local commercial center. Approximately 1.2 acres will be reserved for development of a fire station and lift station. Approximately 6 acres within Planning Area 2 are designated for light industrial land uses, and may consist of an existing boat storage facility and future public facilities for maintenance of the existing and proposed levees. Two gas wells on approximately 5.5 acres total are also located in Planning Area 2.

PLANNING AREA 3

Approximately 182 acres of the Specific Plan Area are owned by Dal Porto and proposed for development by D.R. Horton. Within this new community, 400 residential dwelling units will be developed in a variety of single family detached residential dwelling types along with approximately 23 acres of open space, and approximately 33 acres of parks and recreational amenities. Planning Area 3 also contains one existing 2.4 acre gas well site. Contra Costa has approved a second gas well for this same site in Planning Area 3.

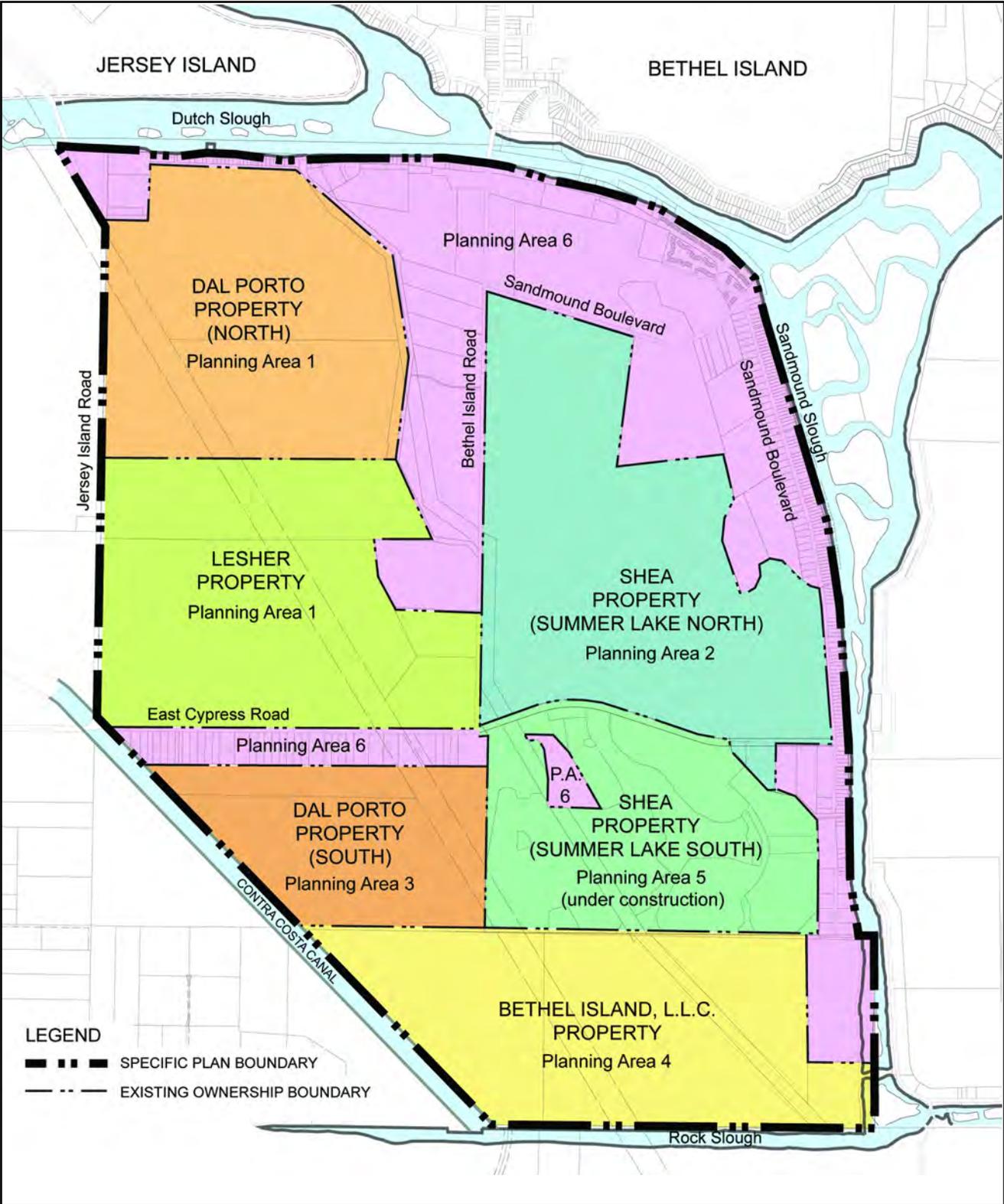


Exhibit 5
Planning Area/Ownership Map



PLANNING AREA 4

Approximately 351 acres are owned and proposed for development by Bethel Island LLC. This area is proposed for development of 1,120 single family and multi-family units, and approximately 58 acres of parks and open space amenities. A 10 acre site for development of an elementary school will be reserved within Planning Area 4. Three gas well sites totaling 6.3 acres also are located within Planning Area 4.

PLANNING AREA 5

Approximately 269 acres of the Specific Plan Area comprise the existing Summer Lake South development located south of East Cypress Road. Shea Homes is developing the Summer Lake South residential planned community to include 628 residential units, approximately 36 acres of parks and open space amenities, approximately 4 acres of wetland/dune area, and an approximately 10 acre site for an elementary school. Pursuant to approvals by Contra Costa County in 1993, Planning Area 5 is considered an “existing condition” within the Specific Plan Area since it is an approved development and is under construction, with occupancy anticipated in May 2006.

PLANNING AREA 6

The remaining approximately 631 acres within the Specific Plan Area are existing, privately owned single family and multi-family residences, mobile homes, commercial and vacant properties with General Plan land use designations of agricultural, commercial and residential uses. A total of 1,095 residential units are possible within Planning Area 6 to include existing residences and the potential future build out of vacant areas.

2.7 LAND USE SUMMARY

The East Cypress Corridor Specific Plan proposes the incorporation of existing developed areas, including the Summer Lake South devel-

opment and the existing land uses within Planning Area 6, with the development of a new planned community comprised of single family detached residential dwelling units, multi-family residential dwelling units, commercial uses, parks, open space uses, natural wetlands/dune areas, public schools and other public facilities.

A central feature of the Specific Plan will be the development of a comprehensive system of engineered levees designed to protect new development within this levee system from a 100-year storm event. A public bicycle trail system will be constructed on the top of the proposed levee system as part of the overall bicycle trail network planned for the new community. The levee trails will connect to a system of landscaped greenways to be developed within power line easements and utility rights-of-way linking parks and open space areas to residential and commercial areas. The greenways and levee trail system throughout the entire community include pedestrian and bicycle paths connecting to the community’s local street system. Residents will be able to walk or bike to parks, schools, and commercial uses located within the Specific Plan Area and to surrounding areas.

Natural wetlands/dune areas will be preserved within the Specific Plan Area as part of the development of Planning Areas 1, 2 and 5. With the development of Planning Area 6, additional natural wetlands/dunes will be preserved. The “Land Use Plan,” Exhibit 6, illustrates the distribution of land uses within the Specific Plan Area.

The key elements of the East Cypress Corridor Land Use Plan are described below and in Table 1, which follows. A detailed description of each land use is provided in Section 4, “Land Use.”

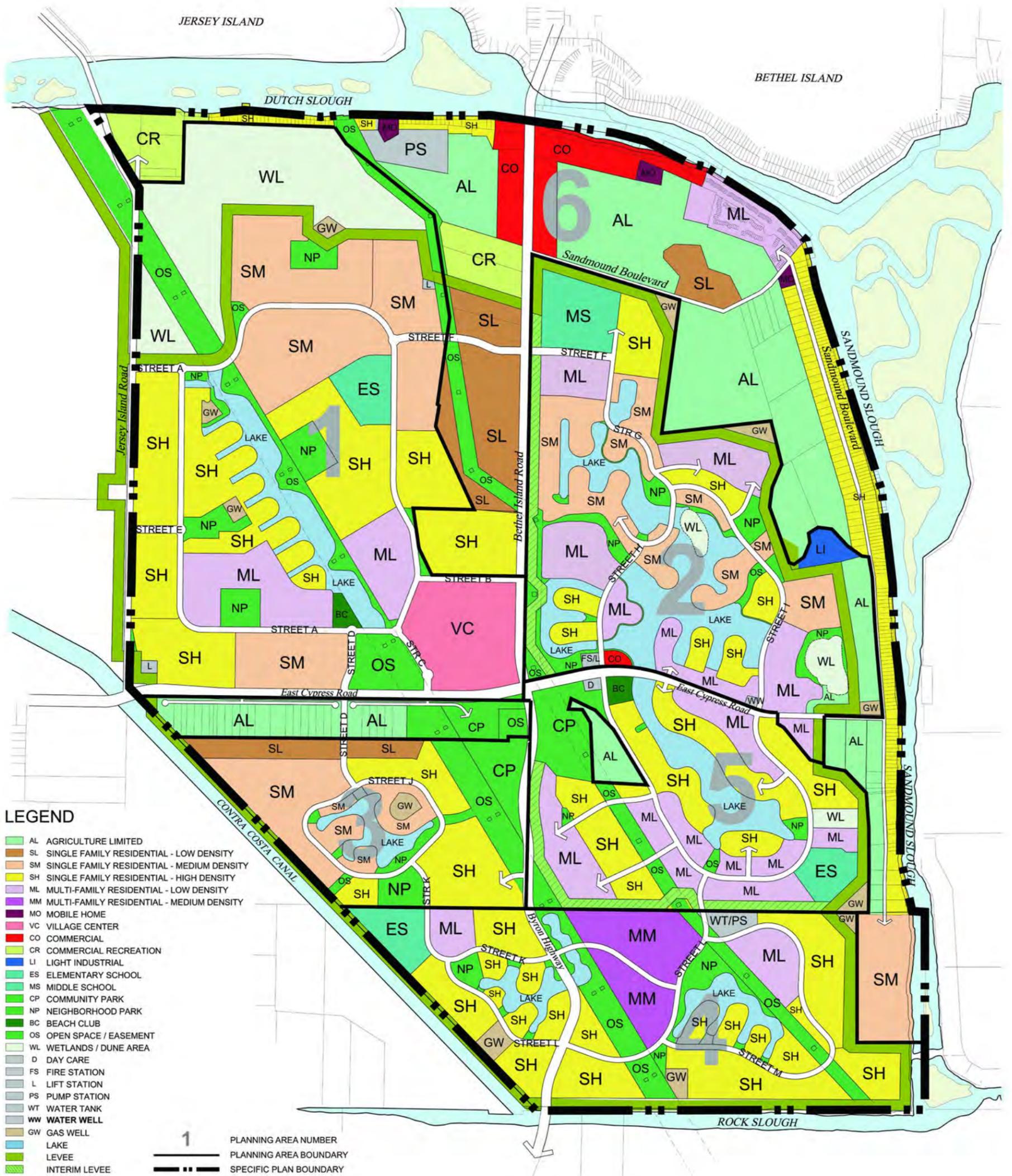


Exhibit 6
Land Use Plan



Planning Area	I (Leshar / Dal Porto North)			II (Summer Lake North)			III (Dal Porto South)		IV (Bethel Island, LLC)		V (Summer Lake South)		VI			TOTALS		
	Acres	Units	Sq. Ft.	Acres	Units ³	Sq. Ft.	Acres	Units	Acres	Units	Acres	Units ³	Acres	Units	Sq. Ft.	Acres	Units	Sq. Ft.
Agriculture - Limited <i>Max. density = 1.0 du/ac</i> (AL)				14.8	12								257.6	243		272.4	255	
Single Family Residential - Low <i>Max. density = 2.3 du/ac</i> (SL)							19.2	23					60.3	175		79.5	198	
Single Family Residential - Medium <i>Max. density = 3.8 du/ac</i> (SM)	150.0	487		75.6	233		52.8	180					35.3	58		313.7	958	
Single Family Residential - High <i>Max. density = 5.5 du/ac</i> (SH)	175.9	766		44.0	200		44.8	197	138.0	584	77.4	272	103.6	405		583.7	2,424	
Multi-Family Residential - Low <i>Max. density = 9.6 du/ac (Detached)</i> (ML)	55.0	297		78.9	371				23.8	132	71.3	356				229.0	1,156	
Multi-Family Residential - Low <i>Max. density = 9.0 du/ac (Attached)</i> (ML)													19.9	163		19.9	163	
Multi-Family Residential - Medium <i>Max. density = 12.0 du/ac</i> (MM)									41.7	404						41.7	404	
Mobile Home <i>Max. density = 9.6 du/ac</i> (MO)													4.3	33		4.3	33	
Village Center (VC)	46.6 ¹	150 ²	435,600													46.6	150	435,600
Commercial (CO)				1.7		10,000							44.3	13	193,000	46.0	13	203,000
Commercial Recreation (CR)													37.3	5	162,500	37.3	5	162,500
Light Industrial (LI)				5.7												5.7		
Schools ⁴	12.4			20.0					10.0		10.0					52.4		
Community Facilities ⁵	0.9			1.8					3.5		1.0		12.1			19.3		
Community Parks (CP)							12.2				19.8		9.3			41.3		
Neighborhood Parks (NP)	22.4			18.2			11.0		13.5		2.7					67.8		
Beach Club (BC)	2.6										3.0					5.6		
Lakes	30.3			70.5			10.4		22.6		24.9					158.7		
Levees	25.2			36.5			3.2		38.4		9.6		4.5			117.4		
Interim Levees				14.9							18.5					33.4		
Open Space / Easements (OS)	55.7			6.9			22.7		44.6		13.0		20.6			163.5		
Wetlands / Dune Area (WL)	105.2			10.4							3.6					119.2		
Gas Well Sites (GW)	3.9			5.5			2.4		6.3		2.1					20.2		
Roads (Bethel Island, Jersey Island, Byron Highway & East Cypress)	17.7			3.6			3.8		8.6		12.1		21.6			67.4		
TOTALS:	703.8	1,700	435,600	409.0	816	10,000	182.5	400	351.0	1,120	269.0	628	630.7	1,095⁶	355,500⁷	2,546.0	5,759	801,100
Percentage Totals:	27.6%	29.5%	54.4%	16.0%	14.2%	1.2%	7.2%	6.9%	13.8%	19.4%	10.6%	10.9%	24.8%	19.1%	44.4%	100.0%	100.0%	100.0%

NOTES: 1. Commercial square footage based on approximately 40 net acres. (46 acres include Streets A, B, C and portions of slope associated with elevated intersection at East Cypress and Bethel Island Roads)
 2. Up to 150 units may replace approximately 20 acres of commercial (217,800 S.F.)
 3. Does not include 200 potential entitled second units in Planning Areas 2 & 5 combined.
 4. Schools include the following land uses: Elementary School (ES) and Middle School (MS).

5. Community facilities include the following land uses: Day Care (D); Fire Station (FS); Lift Station (L); Pump Station (PS); Water Well (WW); and Water Tank (WT).
 6. Planning Area 6 reflects the loss of 5 units from the original 544 existing units.
 7. Actual commercial square footage may be substantially less than 355,500 sf.
 8. All acreages are based on gross area, except as noted.

Table 1
Overall Project Summary

2.7.1 RESIDENTIAL LAND USE

The City of Oakley General Plan permits a total of 5,763 residential units within the Specific Plan Area. New development proposed for Planning Areas 1-4, the existing residential development within Summer Lake South in Planning Area 5 and Planning Area 6 as well as the potential for future development within Planning Area 6 will result in up to 5,759 residential units. Residential land uses comprise approximately 1,542 acres of the Specific Plan Area with a variety of residential housing types planned including single family detached homes on lots ranging from 2,100 square feet up to semi-custom lots of one acre in size and multi-family residential dwelling units.

2.7.2 COMMERCIAL LAND USE

Approximately 44 acres located adjacent to Bethel Island Road between Dutch Slough and Sandmound Boulevard are designated in the General Plan for up to approximately 193,000 square feet of commercial uses to be developed in the future within Planning Area 6.

Approximately 37 acres are designated for development of up to 163,000 square feet of commercial recreation uses within Planning Area 6.

Approximately 40 net acres within Planning Area 1 are proposed for development of Village Center commercial uses to include retail and service businesses to serve the Specific Plan Area. The Village Center area located at the northwest corner of East Cypress Road and Bethel Island Road is suitable for development of community retail uses such as a supermarket, drug store, and service commercial and business/professional office uses. The Village Center commercial site will be planned for use by a commercial developer. The theme and site plan for the Village Center will be reviewed and approved by the Planning Commission and/or the City Council prior to development of the

site. A minimum of 20 acres of the Village Center will be reserved for development of commercial uses. In the event the entire Village Center site is not sold or leased to one or more commercial entities pursuant to the provisions of the Specific Plan, up to 150 residential units could be developed on the remainder of the Village Center site. Within Planning Area 2, another approximately 1.7 acres are designated for development of approximately 10,000 square feet of local serving commercial uses.

2.7.3 PARKS, LAKES, AND AMENITIES

The East Cypress Corridor Specific Plan includes the development of approximately 273 acres, 11% of the Specific Plan Area, as parks, lakes and beach clubs uses. Community parks will be improved as active recreational areas to include soccer fields and baseball diamonds as well as open play areas, picnic tables and informal gathering areas. Neighborhood parks will be improved with open play areas, picnic and barbeque facilities and informal gathering areas, consistent with the City of Oakley Parks and Recreation Master Plan.

A signature feature of the planned community is the provision of approximately 159 acres of lakes, or 6% of the Specific Plan Area, located adjacent to parks and open space accessible by trails within the greenway network or via sidewalks and on-street bike trails. The lakes will be developed within the existing Summer Lake South development and within the new development proposed in Planning Areas 1-4.

2.7.4 GREENWAYS AND TRAILS

The City of Oakley General Plan establishes a multi-use trail system connecting the Specific Plan Area to existing and planned bikeways within the remainder of the City. The East Cypress Corridor Specific Plan provides for development of greenways and trails linking

each Planning Area to parks, open space, natural wetlands/dune areas, commercial areas and schools. These greenways and trails offer equestrian, pedestrian, and bicycle access throughout the entire Specific Plan Area connecting existing development to public facilities, commercial uses, and parks.

2.7.5 OPEN SPACE AND NATURAL AREAS

Approximately 164 acres within the Specific Plan Area will be reserved as open space within public utility corridors and easements. This open space system will provide the backbone for a multi-use trail system connecting residential areas with parks, schools and commercial uses, and connecting to a trail system to be developed on the engineered levees and within roadways. Levee slopes adjacent to the trails will be improved with hydroseed to promote a landscaped appearance for the levee trail system.

Approximately 119 acres within Planning Areas 1, 2 and 5 will be preserved as natural wetlands and dune area of which approximately 105 acres are provided within Planning Area 1. Public access to this area will be limited, however opportunities for visual access to the wetland/dunes area within Planning Area 1 may be provided. This may take the form of a “nature overlook” developed as part of the levee trail system adjacent to the wetlands/dunes, or at viewpoints along the trail system. As Planning Area 6 is developed, additional wetland areas will be set aside.

2.7.6 LEVEES

A comprehensive engineered levee system will be constructed as part of the development of the Specific Plan for flood control purposes consistent with FEMA requirements for an urban levee. The levee system will protect new development while enhancing protection of

existing residences within the Specific Plan Area. The engineered levee system will be constructed as a hydroseeded earthen levee with bank stabilization, as appropriate, adjacent to the waterways of the San Joaquin River including the Contra Costa Canal, Rock Slough, Sandmound Slough, and the wetlands and agricultural areas adjacent to Dutch Slough. As part of the comprehensive levee system, an engineered levee will be constructed adjacent to the westerly right-of-way of Jersey Island Road. The earthen levee will be designed with appropriate freeboard above the 100-year storm water elevation to provide permanent flood protection to the Specific Plan Area from flooding of the San Joaquin River waterways. The engineered levee will extend completely around the boundaries of the proposed new development within the Specific Plan Area but will not surround the wetlands/dune area or the existing agricultural and residential areas located north and east of the development area. The new master levee system will remove existing single family residential areas adjacent to East Cypress Road from the 100-year storm event floodplain and will provide improved emergency access to existing residents along Sandmound Boulevard, Dutch Slough Road and adjoining areas in the event of a breach of the existing non-engineered levee surrounding these areas. The engineered levee system will also serve as a primary component of the comprehensive development of multi-use trails within the Specific Plan Area. Multi-use equestrian, biking and pedestrian trails will be developed on the top of each levee connecting to multi-use trails within arterial roadways and greenways within the Specific Plan Area.

The new engineered, earthen levee constructed with the project will also provide benefit to the existing residents within Planning Area 6 and those residents on Bethel Island. By bringing the majority of Bethel Island and Jersey Island Roads within the protection of a FEMA-stan-

dard urban levee, the engineered levee will provide a reduction in time required to reach safe haven in the event of a levee breach in the existing non-engineered levee.

The new development will pay an annual assessment to Reclamation District 799 (RD 799). This assessment will be determined by the Reclamation District at the time of development. The assessment will be dedicated to the upgrades and improvements of the existing non-engineered levee. The new development will also pay an annual assessment, consistent with the existing residences of the reclamation district, toward RD 799's ongoing maintenance and inspection program. RD 799 currently inspects and maintains the levees within the district, as it will continue to do in the future. Once RD 799 has taken ownership of the new levees in the project area, it will inspect and maintain those as well.

2.7.7 SCHOOLS

The East Cypress Corridor Specific Plan provides for development of three elementary schools and one middle school to serve the K-8 school age needs of the community. Sites for elementary schools will be located within Planning Areas 1, 4, and 5. A 20 acre site for development of a middle school will be reserved within Planning Area 2. All school sites will be accessible by pedestrians and bicyclists via the proposed multi-use trail network as well as by automobile.

2.8 AUTHORITY AND REQUIREMENTS OF THE SPECIFIC PLAN

State of California Government Code, Title 7, Division 1, Chapter 3, Article 8, Section 65450-57 grants authority to cities to adopt specific plans for purposes of implementing the goals and policies of their General Plans. The Government Code specifies that specific plans may be adopted either by resolution or by ordi-

nance and that the specific plan is required to be consistent with the General Plan. The Government Code sets forth the minimum requirements and review procedures for specific plans including provision of a land use plan, infrastructure and public services plan, criteria and standards for development, and implementation measures. The Government Code also states that specific plans may address any other subjects which, in the judgment of the city, are necessary or desirable for implementation of the General Plan.

California Government Code Section 65451 sets forth the minimum requirements and review procedures for specific plans as follows:

A Specific Plan shall include a text and a diagram or diagrams which specify all of the following in detail:

- 1) *The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan;*
- 2) *The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan;*
- 3) *Standards and criteria by which improvements will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable, and*
- 4) *A program of implementation measures including regulations, programs, public works projects and the financing measures necessary to carry out paragraphs 1, 2, and 3 above.*
- 5) *The Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan.*

The East Cypress Corridor Specific Plan is designed to meet the requirements of the State of California Government Code.

2.9 DEVELOPMENT APPROVAL COMPONENTS

The adoption of the East Cypress Corridor Specific Plan is one step in a process leading to the development of the Specific Plan Area. The components of the development approval process for the East Cypress Corridor Specific Plan Area are discussed below.

2.9.1 GENERAL PLAN AMENDMENT

The City of Oakley will adopt a General Plan Amendment changing the General Plan Land Use Diagram for the Specific Plan Area to “Specific Plan” (SP) which will reflect the East Cypress Corridor Specific Plan Land Use Plan. The General Plan Amendment will enable the City to implement General Plan land use policies within the Specific Plan Area in a manner which addresses the physical characteristics of the Specific Plan Area and to comprehensively plan for a community of compatible neighborhoods adequately served by commercial and recreational uses and providing for the school needs of children residing in the community. As part of the General Plan Amendment the City will also change the allocation of commercial land uses increasing the total allowable commercial acres from 69 acres to 93 acres, change the land use designation of approximately 9 acres from “agriculture limited” to a park designation, and change the land use designation of approximately 6 acres from “delta recreation” to “light industrial” in order to implement the goals and objectives of the General Plan. This action will enable the City to more efficiently plan a pattern of land uses for the Specific Plan Area.

2.9.2 PRE-ZONING FOR ANNEXATION

Concurrently with the adoption of the East Cypress Corridor Specific Plan the City of Oakley will pre-zone the Specific Plan Area to “Specific Plan-1” (SP-1) in order for the annexation of the Specific Plan Area to take place. The pre-zoning of the Specific Plan Area to “SP-1” will establish the zoning regulations for the Specific Plan Area and, as such, the requirements of the Specific Plan shall take precedence over the City of Oakley Zoning Ordinance. In instances where the Specific Plan is silent, the City of Oakley Zoning Ordinance shall prevail.

2.9.3 SPECIFIC PLAN

The East Cypress Corridor Specific Plan, when adopted, will serve as a legal document which implements the General Plan land use designation of SP and the zoning district classification of “SP-1” for the Specific Plan Area. The Specific Plan will serve as a “blueprint” for development by establishing the distribution of land use and the criteria for development of each land use as set forth herein. The Specific Plan establishes the development requirements and guidelines to be applied to each phase of development within the Specific Plan Area and will meet the requirements of a Preliminary Development Plan pursuant to the City’s Zoning Ordinance requirements for development approval.

2.9.4 ANNEXATION

Following the approval by the City of Oakley of the General Plan Amendment, Pre-Zoning, Specific Plan and certification of the EIR for the East Cypress Corridor Specific Plan, the City will file three concurrent boundary reorganization applications with LAFCO to annex the Specific Plan Area into the City of Oakley, the Contra Costa Water District, and the Diablo Water District. The boundary reorganization

applications will include a municipal services plan as part of the applications. Annexation is delineated to occur in three concurrent areas. Annexation Area I includes the developing property in Planning Area 5, the areas proposed for development within Planning Areas 1-4, and several contiguous properties to these areas located within Planning Area 6. These contiguous properties contain collectively fewer than 12 registered voters. Annexation Area II includes the strip of land along East Cypress Road within Planning Area 6 which includes 35 residential parcels and approximately 30 registered voters. Annexation Area III includes the remaining approximately 472 acres at the northern and eastern edges of Planning Area 6 which contain approximately 400 registered voters. It is the City's intent to pursue concurrent annexation of all three areas with a plan also submitted describing how Annexation Areas II and III could continue to receive services from the County in the event a majority of the voters elect for these areas not to be annexed.

2.9.5 SUBDIVISION MAPS/FINAL DEVELOPMENT PLANS

Tentative Tract Maps and Final Development Plans for the development within the Specific Plan Area will be reviewed and approved by the City pursuant to the approval of the East Cypress Corridor Specific Plan. Tentative Tract Maps and Final Development Plans will be prepared pursuant to the applicable provisions of the State of California Subdivision Map Act (Government Code Section 66410 through 66499), the City of Oakley Subdivision Ordinance and Zoning Ordinance, and consistent with the applicable provisions contained within the East Cypress Corridor Specific Plan.

2.9.6 DEVELOPMENT AGREEMENTS

Development Agreements for Planning Areas 1-4 will be approved by the City of Oakley. The Development Agreements will provide a frame-

work for the development of these Planning Areas establishing provisions related to phasing of land use, timing of infrastructure and public facilities, and provisions for infrastructure financing.

2.10 RELATIONSHIP OF THE SPECIFIC PLAN TO THE CITY OF OAKLEY GENERAL PLAN

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of a specific plan as an implementation tool for elements contained in the local general plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the general plan.

The City of Oakley General Plan sets forth the following topics to address the state mandated general plan elements:

- Land Use
- Circulation
- Growth Management
- Economic Development
- Open Space and Conservation
- Parks and Recreation
- Health and Safety
- Noise
- Housing

The City of Oakley will adopt a General Plan Amendment as part of the adoption of the East Cypress Corridor Specific Plan changing the General Plan Land Use Diagram to a land use designation of SP to reflect the East Cypress Corridor Specific Plan Land Use Plan, to increase the General Plan allocation of commercial acres from 69 acres to 93 acres, to change the land use designation of approximately 9 acres from "agriculture limited" to a

park designation to accommodate the future development of a community park, and to change the land use designation of approximately 6 acres from “delta recreation” to “light industrial.” The East Cypress Corridor Specific Plan is consistent with the policies of the General Plan, as amended.

2.11 CEQA COMPLIANCE

An EIR has been prepared by the City of Oakley for the East Cypress Corridor Specific Plan in accordance with the California Environmental Quality Act (CEQA) and City requirements to provide a detailed analysis of potential environmental impacts associated with the development of the Specific Plan Area. The EIR prepared for the East Cypress Corridor Specific Plan addresses the potential impacts associated with the project at a “project level” for Planning Areas 1, 3 and 4 and at a “program level” for Planning Areas 2 and 6. The EIR includes recommended mitigation measures to reduce potential significant impacts to a less than significant level. The East Cypress Corridor Specific Plan EIR references the EIR previously certified by Contra Costa County in 1993 for Planning Area 5 which is currently under construction. The East Cypress Corridor Specific Plan EIR has been prepared to fulfill the requirement for environmental determinations for all subsequent discretionary and ministerial applications for development within Planning Areas 1, 3, and 4 of the East Cypress Corridor Specific Plan.

2.12 SEVERABILITY

If any regulation, condition, program, or portion of the East Cypress Corridor Specific Plan is held invalid or unenforceable, such portions shall be deemed separate, distinct, and independent provisions and the invalidity of such portions or provisions shall not affect the validity and enforceability of the remaining portions and provisions therein.

SECTION 3. EXISTING CONDITIONS

This section discusses the physical conditions and existing land use within and immediately adjacent to the Specific Plan Area. The existing zoning and General Plan land use designations for the Specific Plan Area are also discussed.

3.1 EXISTING PROJECT SITE LAND USE

The Specific Plan Area has historically been used for agricultural purposes including dairy farming and grazing. Structures and facilities related to agricultural land use remain within the Specific Plan Area. Rural residential uses also exist along East Cypress Road. Residential uses occur along the perimeter of the Specific Plan Area adjacent to Sandmound Boulevard, Sandmound Slough, and Dutch Slough.

A 350 foot wide Pacific Gas and Electric (PG&E) utility easement bisects the Specific Plan Area in a northwest to southeasterly direction between Dutch Slough and Rock Slough. A 200 foot wide Western Area Power Administration (WAPA) public utility easement bisects the Specific Plan Area in a generally north to southeast direction between Dutch Slough and Rock Slough to the east of the PG&E easement.

The majority of the Specific Plan Area is inactive agricultural land. Natural wetlands and dunes occur inside the protection of the existing non-engineered levee system, predominantly in the northwest corner of the Specific Plan Area adjacent to Dutch Slough.

Approximately 544 existing residences are located within Planning Area 6 of the Specific Plan Area. These residences are served by Dutch Slough Road, Sandmound Boulevard, Wells Road, Aspen Road and Tule Tree Lane. Another

35 residences are located adjacent to East Cypress Road between Jersey Island Road and Bethel Island Road within Planning Area 6.

Approximately 269 acres located east of Bethel Island Road and south of the extension of East Cypress Road, known as Summer Lake South, are being developed by Shea Homes pursuant to entitlements approved by Contra Costa County in 1993. Access to the Summer Lake South development will be provided by an extension of East Cypress Road to be completed as part of the development. The planned development includes construction of approximately 3 miles of engineered levee extending from East Cypress Road southerly, easterly, northerly, and westerly around the project boundary to Bethel Island Road. The development will result in approximately 628 residential dwelling units, an elementary school, a 24.9 acre lake, parks and open space. Summer Lake South comprises Planning Area 5 within the Specific Plan Area and is anticipated to have homes ready for occupancy in May 2006. Because it is an approved project under construction prior to the approval of the East Cypress Corridor Specific Plan, the Summer Lake South development is considered an existing land use similar to the existing land uses within Planning Area 6.

Existing site land use features are illustrated on Exhibit 7, "Existing Land Uses."

3.2 SURROUNDING LAND USES

The Specific Plan Area is bounded on the north by Dutch Slough and unincorporated County areas containing rural residential and recreational residential land uses. Unincorporated County areas and agricultural uses also abut the Specific Plan Area on the south, west, and east. The Specific Plan Area is bounded on the south

and southwest by Rock Slough, the Contra Costa Canal, and unincorporated County area. The Specific Plan Area is bounded on the west by the existing City of Oakley and the approximately 1,200 acre planned Dutch Slough Restoration Area. The city limits of Brentwood are within two miles of the Specific Plan Area. The city limits of Antioch are within four miles of the Specific Plan Area. The town of Knightsen is located approximately one mile southwest of the Specific Plan Area.

3.3 TOPOGRAPHY

The Specific Plan Area is located on relatively flat terrain generally ranging in elevation between 10 feet below mean sea level and 8-14 feet above mean sea level. Non-engineered levees are present on the northern, eastern, and southern boundaries of the Specific Plan Area. Topographical features include sand mounds which rise from the landscape and numerous irrigation canals and drainage ditches traversing the length and width of the Specific Plan Area. Exhibit 8, “Existing Topographic Conditions” illustrates the topography of the Specific Plan Area.

3.4 GENERAL PLAN LAND USE AND ZONING

The City of Oakley General Plan assigns the following land use designations for the Specific Plan Area as illustrated on Exhibit 9, “General Plan Land Use.”

- AGRICULTURAL LIMITED (AL) which allows light agriculture and very low density residential uses at a maximum of 1.0 dwelling unit per gross acre and typical parcel sizes between 1.0 –10.00 acres. This land use designation occurs primarily within Planning Area 6 and includes the existing rural residential uses and areas located in the north and northeasterly areas of the Specific Plan Area.

- SINGLE FAMILY RESIDENTIAL, VERY LOW DENSITY (SV) which allows large lot residential development at 0.2 to 1.0 dwelling units per gross acre with typical lot sizes between one and five acres in size.
- SINGLE FAMILY RESIDENTIAL LOW DENSITY (SL)which allows development of 0.8-2.3 dwelling units per gross acre and sites ranging from 14,000 square feet to one acre in size. This land use comprises the majority of the land within Planning Areas 1-4.
- SINGLE FAMILY RESIDENTIAL, MEDIUM DENSITY (SM) which allows development of 2.3-3.8 dwelling units per gross acre with parcel sizes ranging from 8,600 to 14,000 square feet in size.
- SINGLE FAMILY RESIDENTIAL HIGH DENSITY (SH) which allows development of 3.8 to 5.5 dwelling units per gross acre and parcel sizes ranging from 6,000 to 8,600 square feet in size.
- MULTI-FAMILY RESIDENTIAL LOW DENSITY (ML) which allows for development of 5.5 to 9.6 dwelling units per gross acre.
- MOBILE HOME (MH) which allows development of mobile home parks of 5.5 to 9.6 mobile home dwelling units per gross acre.
- COMMERCIAL (CO) which allows development of a general range of retail and service commercial with limited office uses.
- COMMERCIAL RECREATION (CR) – which allows development of a broad range of commercial recreation activities such as marinas and boat facilities, golf courses, restaurants, yacht clubs, driving ranges, and outdoor equipment rentals. This land use category is located within Planning Area 6.
- PUBLIC AND SEMI-PUBLIC FACILITIES (PS) which includes lands owned by public utility companies and sites for public service facilities such as libraries, fire stations, transportation corridors, and schools.

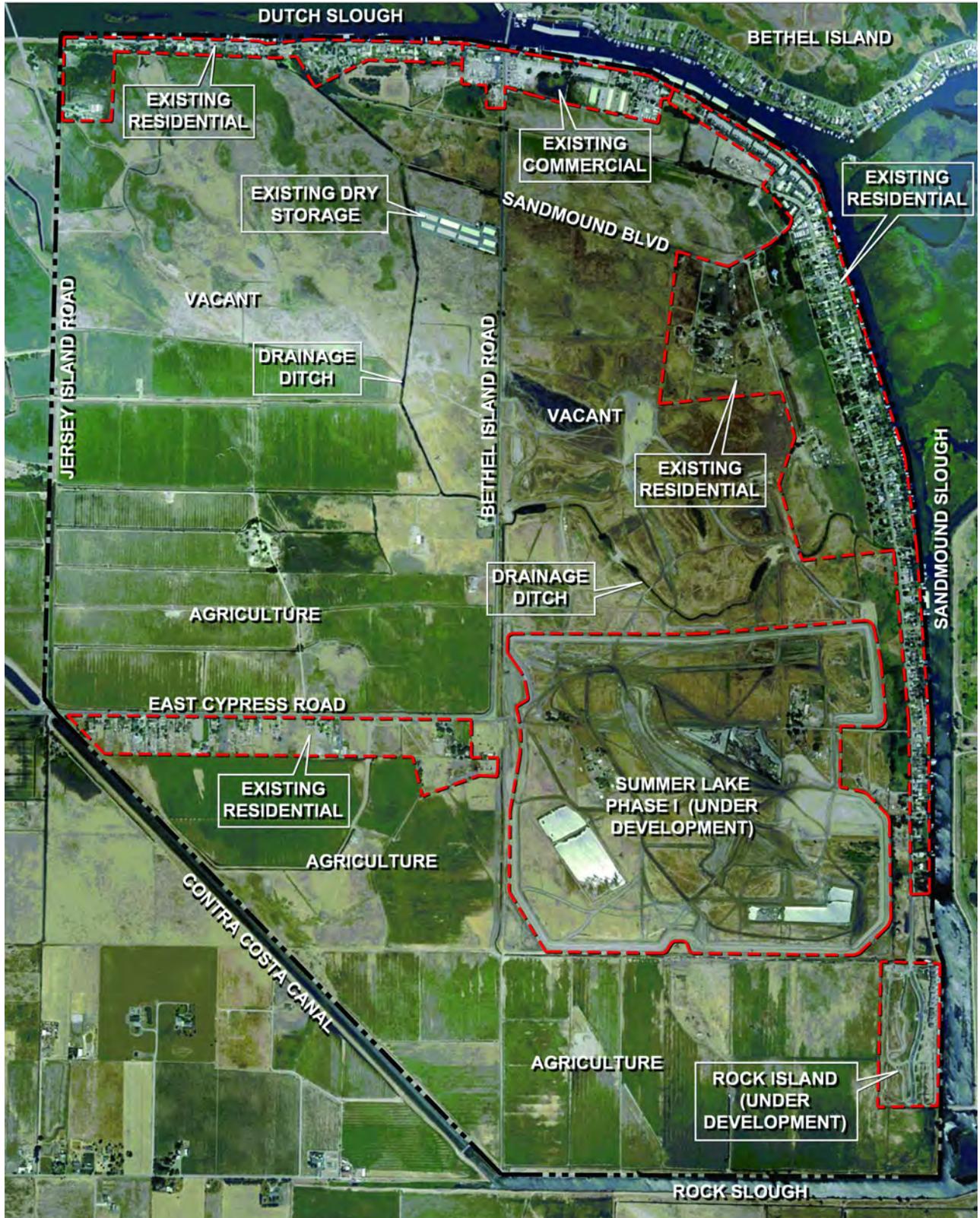


Exhibit 7
Existing Land Uses



SECTION 3. EXISTING CONDITIONS

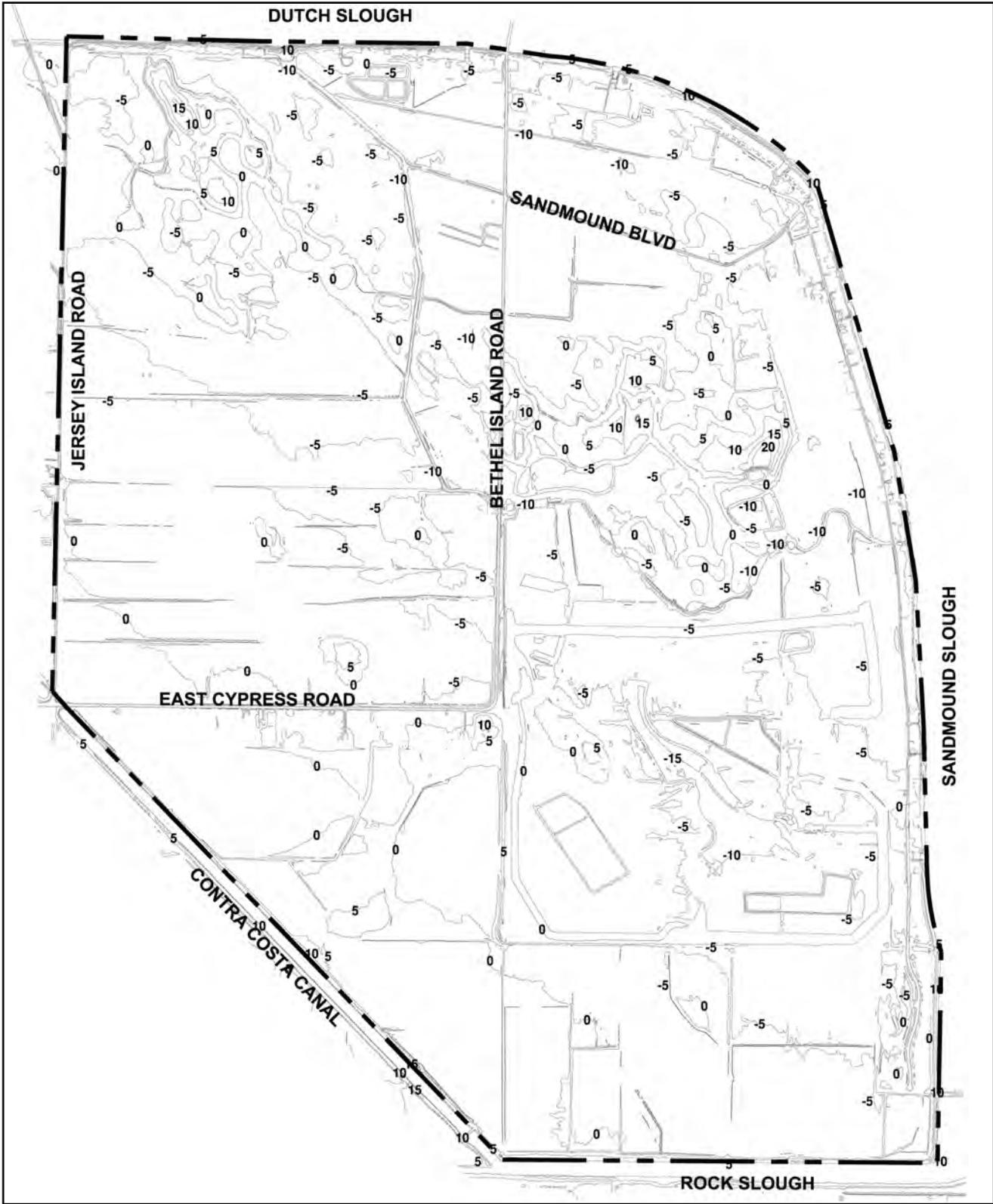


Exhibit 8
Existing Topographic Conditions



- DELTA RECREATION (DR) which allows delta related recreational uses including but not limited to marinas, shooting ranges, hunting clubs, campgrounds, golf courses, and other outdoor recreation complexes.

The General Plan also designates the Specific Plan Area as the Cypress Corridor Expansion Area with a vision for development of low and medium density residential development combined with recreational uses and open space areas with a higher density development node at the intersection of East Cypress Road and Bethel Island Road. The General Plan permits a maximum of 5,763 residential dwelling units and a maximum of 69 acres of commercial uses to be developed within the Specific Plan Area.

The Specific Plan Area is currently zoned by Contra Costa County with a variety of zoning districts including R-6 Single Family Residential, P-1 Planned Unit, A-2 General Agricultural, A-3 Heavy Agricultural, F-1 Water Reclamation, T-1 Mobile Home/Manufactured Home Park, M-12 Multiple Family Residential, and R-B Retail Business. Existing zoning for the Specific Plan Area is illustrated in Exhibit 10, “Existing County Zoning.”

3.5 PHYSICAL SITE CONSIDERATIONS

3.5.1 ARCHAEOLOGICAL SITES

The Specific Plan Area contains seven known significant archaeological sites as illustrated on Exhibit 11, “Site Constraints.” Recommended treatment for these sites within the East Cypress Corridor Specific Plan include the following:

- Avoidance of these archaeological sites as part of project construction; or

- Incorporation of these archaeological sites within parks, green space, or other open space; or
- Covering the archaeological sites with a layer of chemically stable soil before building facilities, such as tennis courts, parking lots, or similar facilities on the site; or
- Deeding the site into a permanent conservation easement.

3.5.2 GAS WELL SITES

The Specific Plan Area contains six active gas well sites operated by various entities as illustrated on Exhibit 11, “Site Constraints.” The gas wells are owned and operated by private holders. These facilities extract natural gas from under the surface of the Specific Plan Area. The collected natural gas is then conveyed to the gas mains in Jersey Island Road, where it is carried to regional gas facilities. The natural gas well sites are located throughout the Specific Plan Area.

Recommended treatment for these facilities within the East Cypress Corridor Specific Plan include appropriate screening of these facilities from residential land uses and maintaining a minimum 150 foot setback from the well head of these facilities to habitable residential land uses. The City of Oakley has recently adopted an ordinance to regulate development of new gas wells.

3.6 EXISTING ACCESS AND CIRCULATION

3.6.1 JERSEY ISLAND ROAD

The westerly boundary of the Specific Plan Area, Jersey Island Road, is a two-lane, rural highway. The road has an existing 60 foot right-of-way. Fewer than five residences front directly onto Jersey Island Road. The southern terminus of Jersey Island Road is its intersec-

SECTION 3. EXISTING CONDITIONS

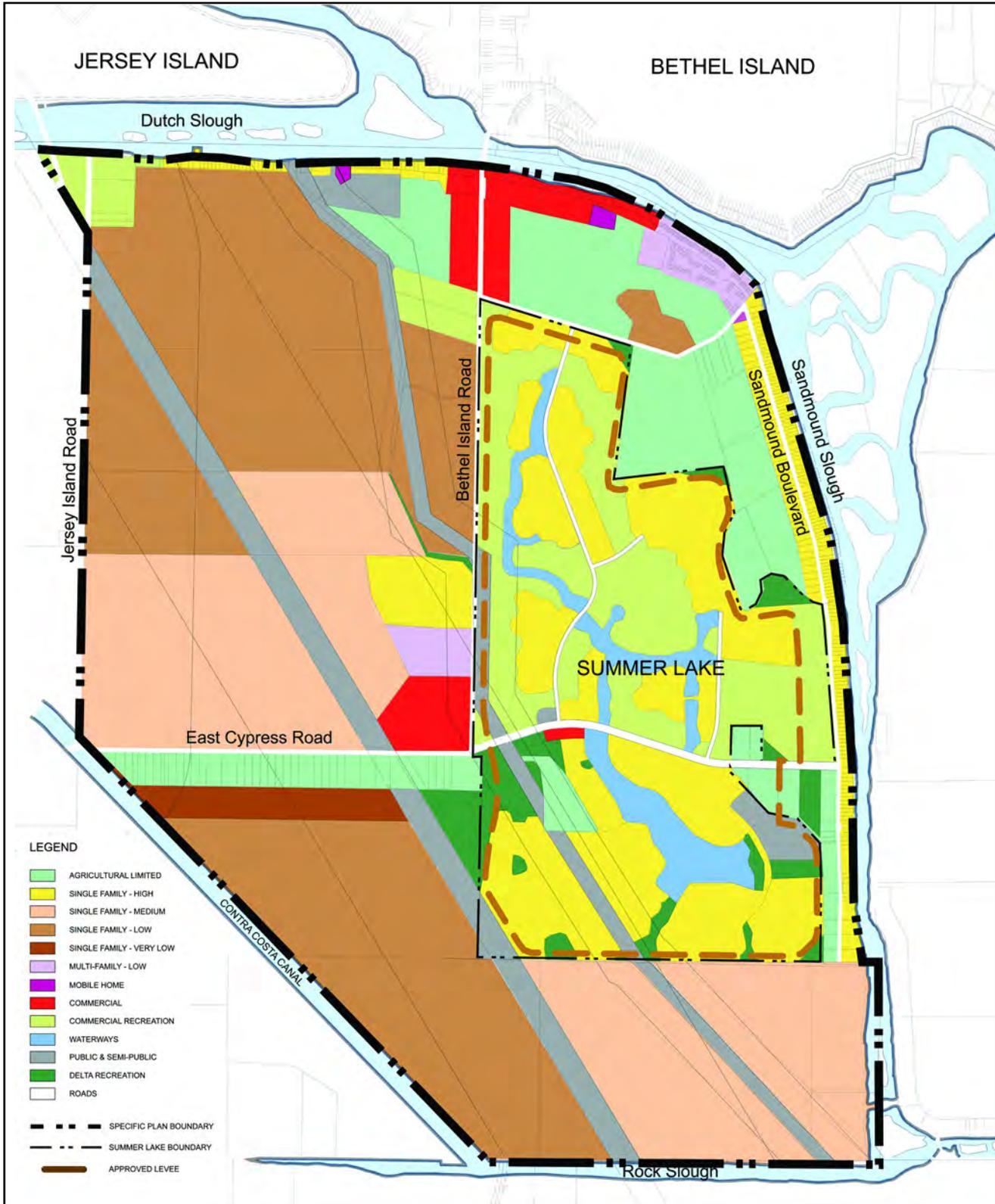


Exhibit 9
General Plan Land Use

Source: City of Oakley 2020 General Plan



tion with East Cypress Road. From its southerly terminus Jersey Island Road continues northerly across the Jersey Island Bridge to Jersey Island. The island is owned by Ironhouse Sanitary District (ISD) with the exception of one parcel that is held in private ownership. There is one residence on the island. ISD purchased the island with the intent of utilizing it as evaporation ponds for its current secondary treatment facilities and contemplates no future commercial ventures.

3.6.2 BETHEL ISLAND ROAD

Bethel Island Road is a two-lane rural highway with a 60 foot right-of-way. At its southerly terminus, the intersection of Bethel Island Road and East Cypress Road turns 90° to the north. From the intersection, Bethel Island Road continues northerly across Dutch Slough to Bethel Island.

Immediately adjacent to the intersection of East Cypress Road and Bethel Island Road, the engineered levee for Summer Lake South is being constructed. The intersection of East Cypress Road and Bethel Island Road is being reconstructed as part of the Summer Lake South community under development. The intersection is being raised to levee height in order to pass over the Summer Lake South levee and provide access to the development. The new intersection will accommodate the ultimately planned 150 foot width of both East Cypress Road and Bethel Island Road as major arterials, four lane divided roadways, when they are improved with the future developments. The new intersection is being raised approximately 20 feet above existing grade. The intersection will be reconfigured to be a four-way controlled intersection. The southerly extension of Bethel Island Road will be closed with a barrier until it is developed in the future. The intersection will be controlled temporarily by stop signs.

The Contra Costa County Public Works Department (CCCPWD) is designing a replacement to the existing Bethel Island Bridge crossing. Federal funds are being utilized to finance the design and construction work. As part of this project, the bridge will remain as a two-lane bridge. CCCPWD is approximately 65% complete in preparing final plans for the replacement bridge and access roadways at this time.

3.6.3 EAST CYPRESS ROAD

Regional access to the Specific Plan Area is provided by East Cypress Road. East Cypress Road bisects the Specific Plan Area in a west to east direction, entering the Specific Plan Area at its intersection with Jersey Island Road and the Contra Costa Canal. East Cypress Road terminates at its intersection to the east with Bethel Island Road. East Cypress Road is a two-lane, rural highway with an existing 60 foot right-of-way. The existing roadway has no curb, gutter, sidewalk or underground storm drain facilities. A drainage ditch is immediately adjacent to each side of the road. There are approximately 35 residences that front onto East Cypress Road within the Specific Plan Area utilizing East Cypress Road as the only access to and from their properties.

The reconstruction of the intersection of Bethel Island Road and East Cypress Road will begin the conversion of East Cypress Road into a major arterial road just north of the existing roadway. In order to provide safe access to the realigned portion of East Cypress Road, existing East Cypress Road will be converted to a frontage road. With the reconstruction of the intersection of Bethel Island Road and East Cypress Road, the easterly end of the frontage road will be rebuilt to a cul-de-sac. The existing residents south of the frontage road will gain access via a connection provided west of the new intersection. From this point to the west, the residents will continue their current access

SECTION 3. EXISTING CONDITIONS

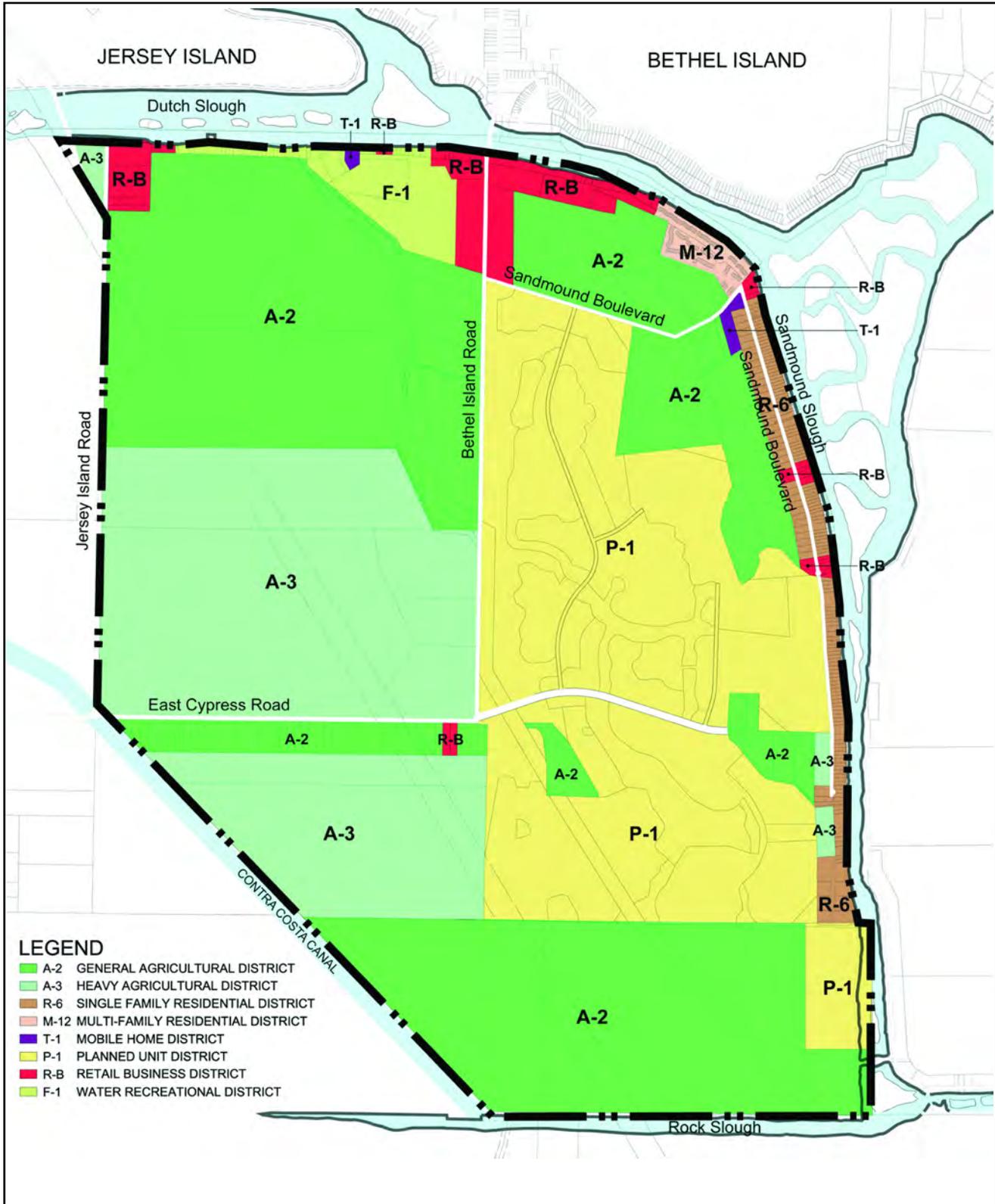


Exhibit 10
Existing County Zoning



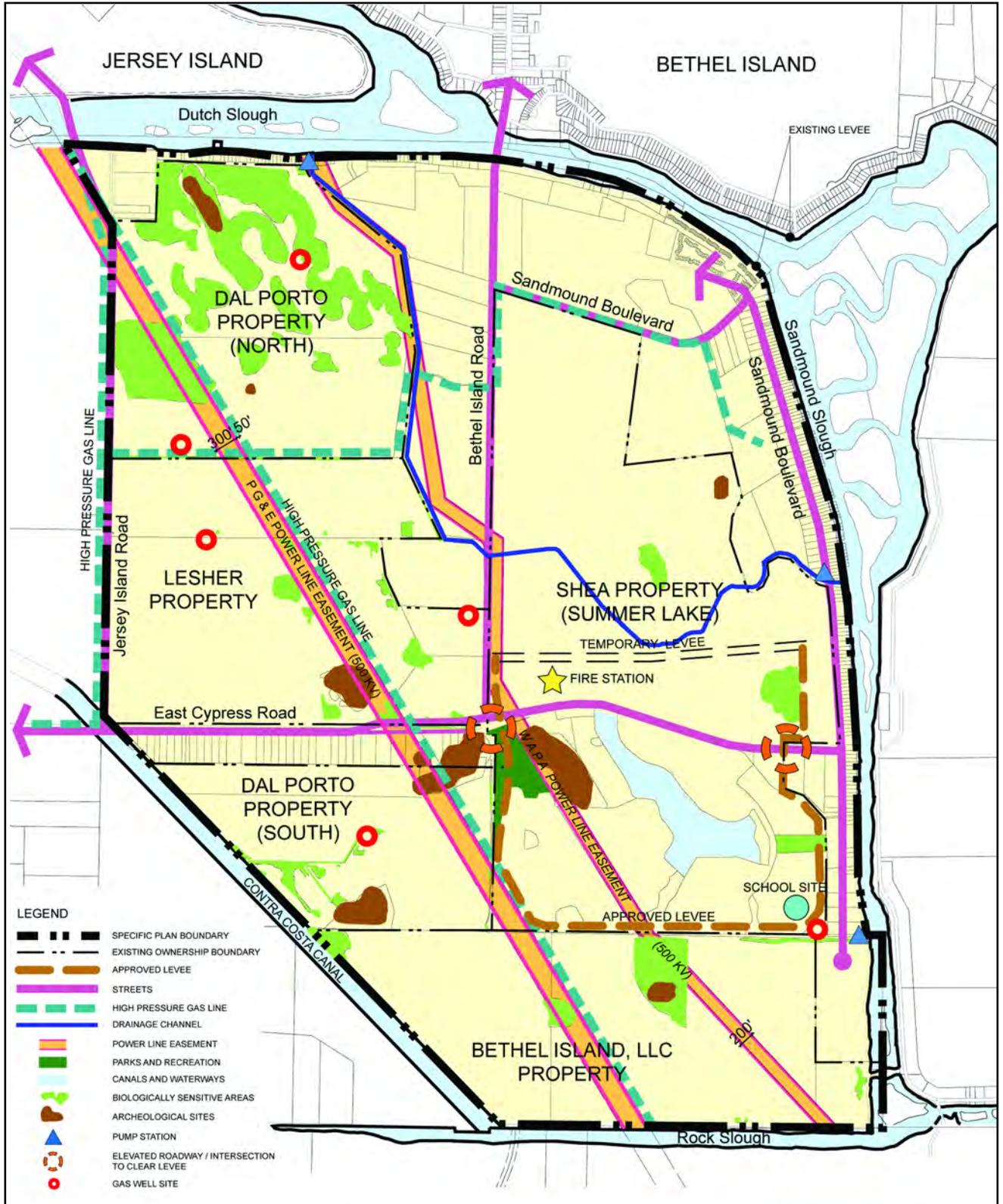


Exhibit 11
Site Constraints



to the frontage road via the existing driveways. The existing roadways are shown in Exhibit, 12 “Existing Circulation.”

3.7 EXISTING INFRASTRUCTURE AND UTILITIES

Existing infrastructure and utility lines within and surrounding the Specific Plan Area are described below.

3.7.1 WATER

Domestic water service within the Specific Plan Area is provided to existing residents through individual private wells, small private water systems and Diablo Water District (DWD). Individual private well systems owned and operated by individual entities are regulated by the Contra Costa County Environmental Health Services Department. These systems are typically utilized by land owners and residents of properties situated on large parcels of land of more than one acre in size.

Small private water systems are owned and operated by both DWD and various private entities within the Specific Plan Area. These entities function to operate and maintain community wells for the benefit of two or more landowners. These water systems are governed by the Contra Costa County Environmental Health Services Department and are required to meet State Health standards. Distribution facilities generally consist of interconnected 2-inch to 8-inch pipes. Water quality is sufficient to meet current State Health Code requirements with no treatment required. Some of the water systems may not be able to meet the new federal arsenic standard set to take effect in January 2006. Those systems failing to meet the new standard may have to provide treatment. Disinfecting water from potential water borne contaminants is achieved through chlorine injection at the wellhead, but the addi-

tion of chlorine has only been confirmed with the systems operated by DWD. DWD operates three of the small water systems:

- Delta Mutual
- Willow Park Marina
- Rock Island

Approximately 628 units are scheduled for construction within the Summer Lake South planned development. As part of residential development, water system improvements to bring water service to the Summer Lake South project are currently underway and will be completed in conjunction with the completion of other improvements for this project.

The residents within Summer Lake South will receive water from DWD’s surface water treatment and distribution system. To provide water facilities for Summer Lake South, the developer has constructed approximately 10,000 linear feet of 24-inch water main to connect to the existing water line at East Cypress Road and Sellers Avenue. The 24-inch water main provides required domestic and fire flows for the project. In order to meet emergency flow requirements, Summer Lake South has installed a well system that is directly tied into the Diablo Water District facilities. The emergency well system automatically provides supplemental flows in the event the water system is unable to meet required demands.

The installation of the 24-inch water main in East Cypress Road is being coordinated by DWD. When the intersection of East Cypress Road and Jersey Island Road is reconstructed to function as the flood control levee, any utilities passing through/over the engineered levee must have minimum invert elevations of 8 mean sea level. The main is being installed within the existing road, with top of roadway at approximately elevation 6.5 mean sea level. The 24-inch water main is being installed with appro-

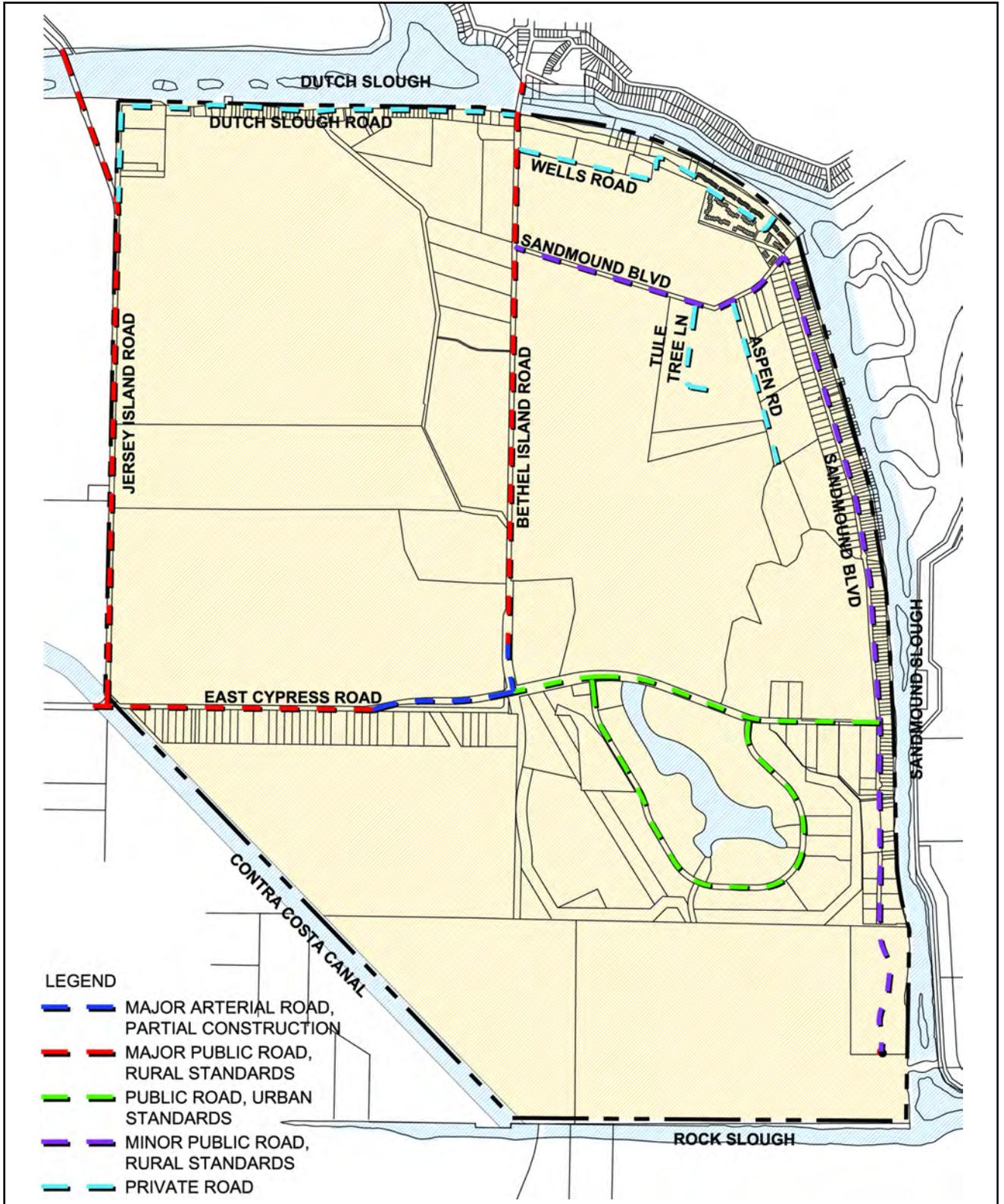


Exhibit 12
Existing Circulation



appropriate valving to allow for the main within the proposed levee to be bypassed, taken out of service, and reconstructed at the appropriate grade to pass over elevation 8 mean sea level as it crosses the levee. This operation will be required when the proposed engineered levee is constructed.

Existing water facilities within the Specific Plan Area are illustrated in Exhibit 13, “Existing Water Facilities.”

3.7.2 SEWER

Sanitary sewer services within the boundaries of the Specific Plan Area are provided by ISD. With some isolated exceptions, generally all of the existing residences in the vicinity of Sandmound Slough and in the vicinity of Dutch Slough are connected to sanitary sewer mains owned and operated by ISD. This system consists of a series of gravity mains flowing to local lift stations. The local lift stations pump to a regional station referred to as WEB 14 by ISD just southerly of the Bethel Island Bridge. From WEB 14 sewage is then pumped 23,000 lineal feet along Bethel Island Road and East Cypress Road via a 14” diameter cement lined ductile iron sewer force main to gravity facilities near Main Street, at the westerly terminus of East Cypress Road.

Existing residences along East Cypress Road are not connected to any ISD facilities. Sewage disposal is handled by septic tank and leach fields and is regulated by the Contra Costa County Environmental Health Services Department.

The Summer Lake South sewer system is comprised of a system of gravity lines and two lift stations located within the project. An additional 10” force main is stubbed to the east in East Cypress Road. This 10” force main will connect to the 14” force main at the intersection of East Cypress Road and Bethel Island Road.

All sewage within Summer Lake South ultimately flows to the lift station. From there it is pumped to the 14” force main. With the Summer Lake South development, the 14” force main is being relocated to the north side of East Cypress Road and the east side of Bethel Island Road. The new 14” force main is tied into the existing 14” force main at its northerly end and at its westerly end. The force main was relocated to ensure that it was not too deep with respect to the new intersection.

There are three existing emergency overflow ponds situated at the north end of the Specific Plan Area westerly of Bethel Island Road. These ponds are the remnants of the old Bethel Island sewer treatment facility. The largest of the three ponds is unlined and is currently unutilized. The southeasterly pond is clay-lined and used for emergency storage for the residences along Dutch and Sandmound Sloughs. The south-westerly pond has been membrane-lined by Shea Homes and is used for emergency storage for both the north and south Summer Lake developments. ISD has established a minimum storage capacity of three days of full flow as a standard. The two existing ponds substantially exceed this criterion. The remaining pond will be membrane-lined by the Delta Coves development on Bethel Island.

The existing sanitary sewer facilities are illustrated in Exhibit 14, “Existing Sanitary Sewer Facilities.”

3.7.3 STORM DRAINAGE

Existing drainage facilities within the Specific Plan Area consist primarily of swales and culverts within roadways. The overall drainage pattern of the Specific Plan Area is from the southwest to the northeast. These surface facilities convey the runoff to the five existing storm drainage pump facilities, which are owned and maintained by RD 799.

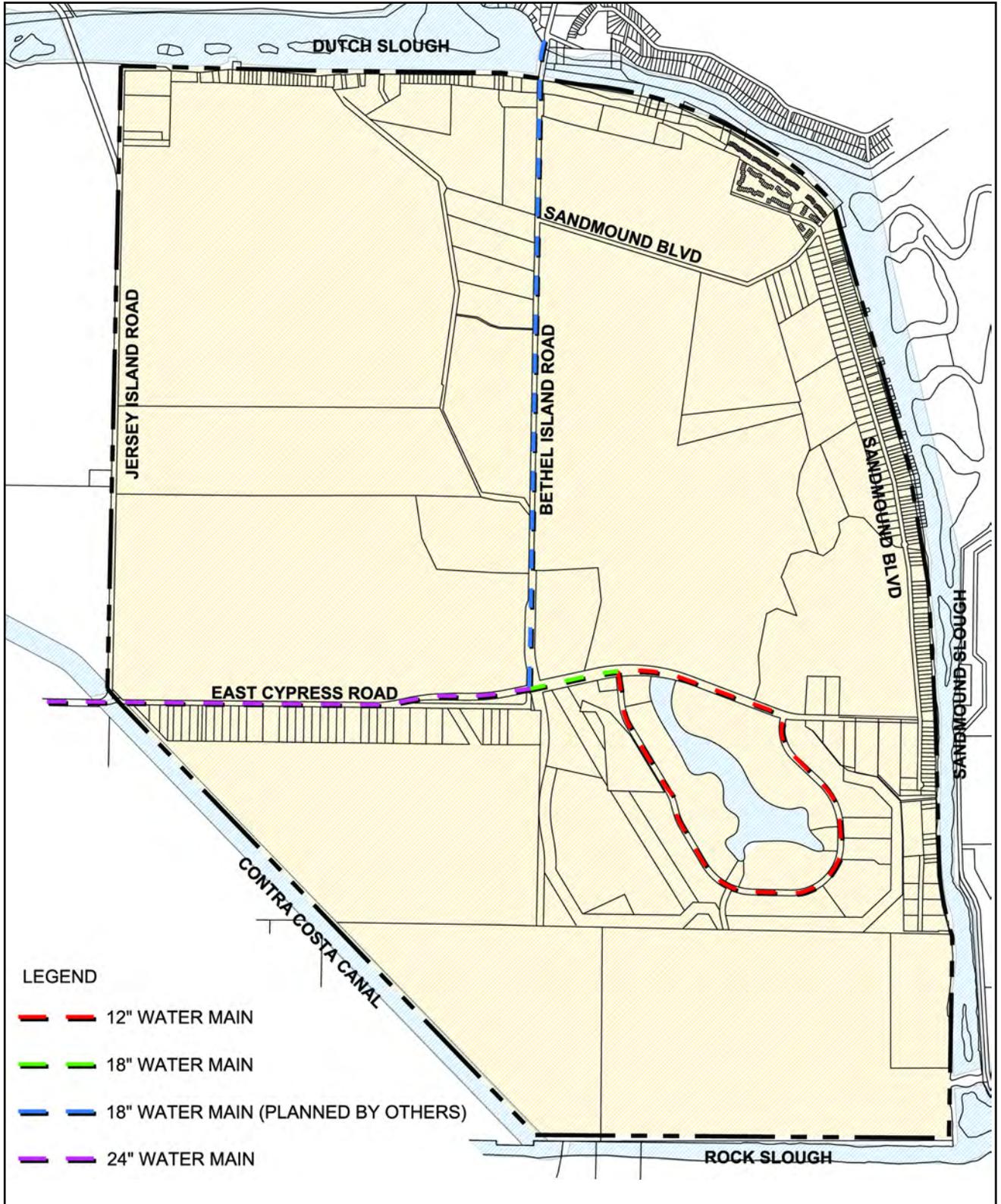


Exhibit 13
Existing Water Facilities



The Summer Lake South development is installing a system of underground storm drain pipelines to collect and convey the runoff from the site. Storm drainage also sheet flows to the lakes, through the lake system filters. The system of storm drain pipelines connects to a central lake. This man-made lake will collect and detain the runoff from the site. The lake system is constructed with several means of filtration, including both mechanical and biofiltration systems to clean the project runoff. The clean runoff will then be pumped from the lake to the existing facilities owned and maintained by RD 799.

3.7.4 UTILITIES

3.7.4.1 Electrical Transmission Facilities

Three tower-mounted, high-voltage transmission lines cross the Specific Plan Area. The two westerly electrical transmission lines are owned and operate by PG&E. These lines are 500 kv power lines. The easterly 500 kv overhead electrical transmission line is owned and operated by WAPA.

Very limited information is available for the electrical facilities due to security concerns. These overhead electrical facilities will not be modified with development in the Specific Plan Area, however appropriate setbacks from power lines and uses allowed within power line easements and rights-of-way are identified within the Specific Plan Area.

When planning, designing, and constructing within the existing transmission line easements and rights-of-way, safety of the public is kept in mind. Any use of the easement that may endanger the public is not allowed. For example, no kite flying is permitted in the vicinity of the electrical transmission facilities. All metal facilities within the easement are required to be grounded. Vegetation is kept to a minimum

height, in accordance with the owner's requirements. These design guidelines protect the public, as well as the owner and its employees.

The final design for all areas within the rights-of-way and easements will be reviewed by the appropriate owner individually.

3.7.4.2 Residential Electrical Facilities

The existing residents along Dutch Slough, Sandmound Blvd, and Sandmound Slough each receive power from overhead facilities originating on Bethel Island, across the Bethel Island Bridge. The residents along East Cypress Road receive power from overhead facilities that cross the Contra Costa Water District (CCWD) canal and Jersey Island Road. The overhead electrical lines that supply other existing residents are to remain. Each developer will be responsible for bringing power supply from the nearest facilities to serve the development. The developer of Summer Lake South is upgrading the existing power lines that cross into the project from the Bethel Island Bridge south along Bethel Island Road to serve the development.

3.7.4.3. Natural Gas Facilities

Several high-pressure gas mains exist within the Specific Plan Area. A 10" high-pressure gas main owned by PG&E located in Jersey Island Road crosses into the Specific Plan Area at the northwest corner across the Jersey Island Bridge, heads south on the west side of Jersey Island Road, and turns to the west at the intersection of Jersey Island Road and East Cypress Road.

A 42" high-pressure gas main, also owned by PG&E, exists on the east side of the overhead power lines. This gas main crosses into the Specific Plan Area across Rock Slough, heads to the northwest along the overhead power lines, and leaves the Specific Plan Area across Dutch

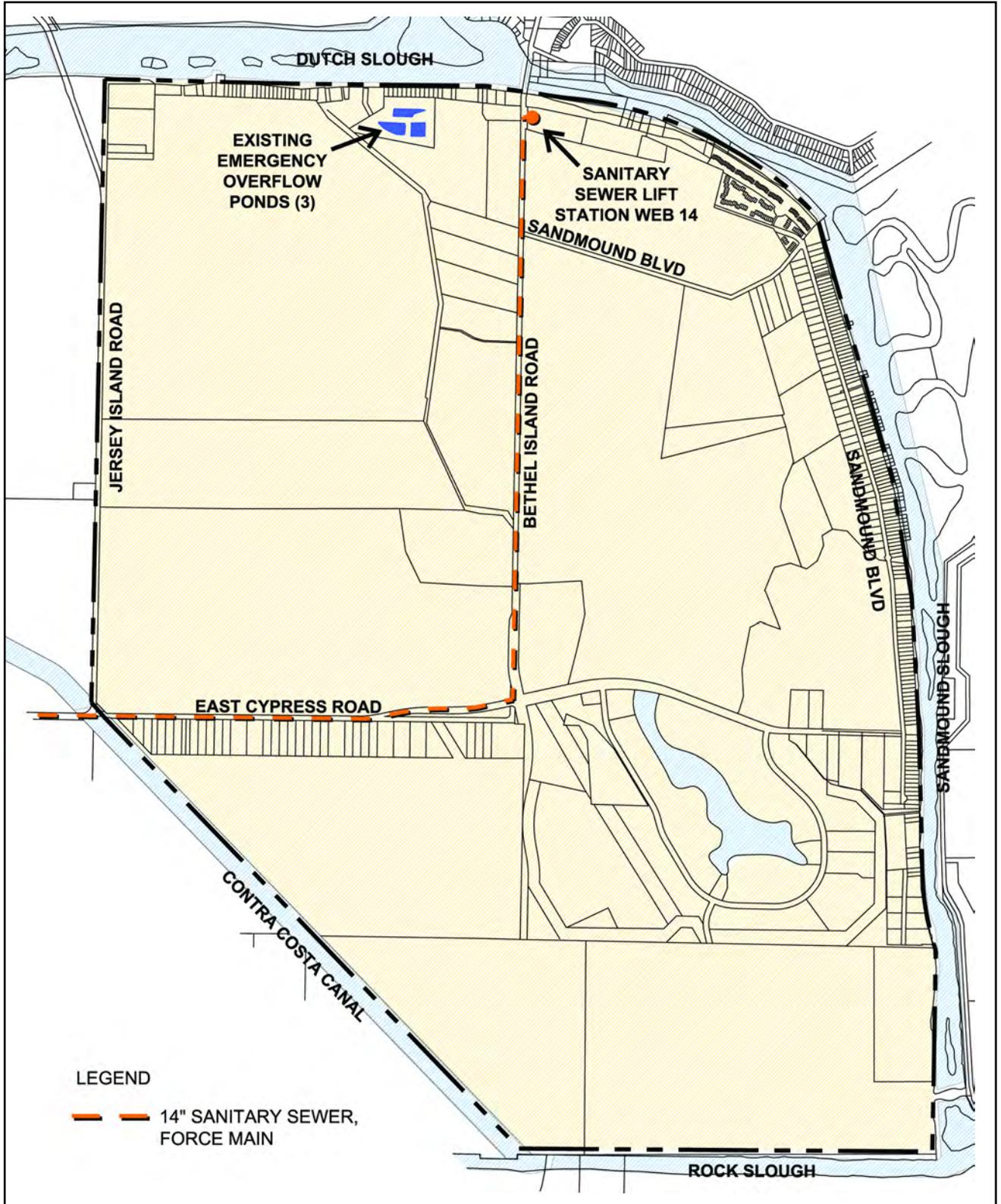


Exhibit 14
Existing Sanitary Sewer Facilities



Slough. Both lines are generally installed 4'-6' below grade. Six gas wells exist within the Specific Plan Area as illustrated in the Exhibit 15, "Existing Natural Gas Facilities."

In addition, Calpine owns a 4" gas main located in the northern half of the Specific Plan Area. Supplied by gathering lines, the 4" gas main traverses Planning Area 1, crosses Bethel Island Road at Sandmound Boulevard and continues east into Planning Area 6. It connects to a 6-8" regional gas main located in Jersey Island Road.

3.7.4.4 Telephone Facilities

Telephone facilities for existing residents are provided by SBC. The residents along East Cypress Road have overhead facilities that cross the Contra Costa Canal and Jersey Island Road. The residents along Dutch Slough Road and Sandmound Blvd have facilities that originate on Bethel Island, cross the Bethel Island Bridge, and continue south on Bethel Island Road. The Summer Lake South development will be served by telephone facilities from Bethel Island Road.

3.7.4.5 Cable Facilities

Cable facilities for this area are provided by Comcast cable through a franchise agreement with the City of Oakley. New development will be required to upgrade existing facilities as deemed necessary by Comcast.

3.8 SCHOOLS

The Specific Plan Area is located within two school districts providing for the K-8 students. Oakley Union Elementary School District boundaries include Planning Areas 1, 2, 3 and 5. Knightsen School District boundaries include all of Planning Area 4 and a portion of Planning Area 6. Existing school district boundaries are illustrated in Exhibit 16, "School District Boundaries." The entire

Specific Plan Area is within the Liberty Union High School District.

3.9 HYDROLOGY

Except for limited areas of high ground at elevations greater than seven feet, the entire Specific Plan Area is within a one percent chance floodplain designated by the FEMA. The source of flooding for this floodplain is the adjacent Sacramento-San Joaquin Delta. The base flood elevation of 7.0 feet is the current estimate of the one percent chance flood event in the sloughs adjacent to the Specific Plan Area.

The Specific Plan Area lies within the protection of an existing non-engineered levee system. The existing levee system is operated and maintained by RD 799. The base floor of the Specific Plan Area varies in elevation from nearly ten feet below mean sea level (MSL) at the lowest point to three above mean sea level. The existing non-engineered levee system is depicted in Exhibit 17, "Existing Levee System." Approximately 3,000 lineal feet of non-engineered levee, along the northern and eastern perimeter of the Specific Plan Area, extending from the Jersey Island Bridge easterly along Dutch Slough and southeasterly along Sandmound Slough was constructed in approximately 1910 for flood protection. The non-engineered levee system along the southerly boundary of the Specific Plan Area is comprised of the embankment constructed when Rock Slough, a historic natural slough, was realigned and straightened at some unknown date. The southwesterly boundary of the Specific Plan Area was constructed as a result of the construction of the Contra Costa Canal built by the Federal Bureau of Reclamation in the mid 1950's.

The existing levees are constructed as non-engineered fills. Earth was "stacked" directly on top of the native ground with no efforts to prepare and pre-compact the underlying soils. This method of construction is typical for much of

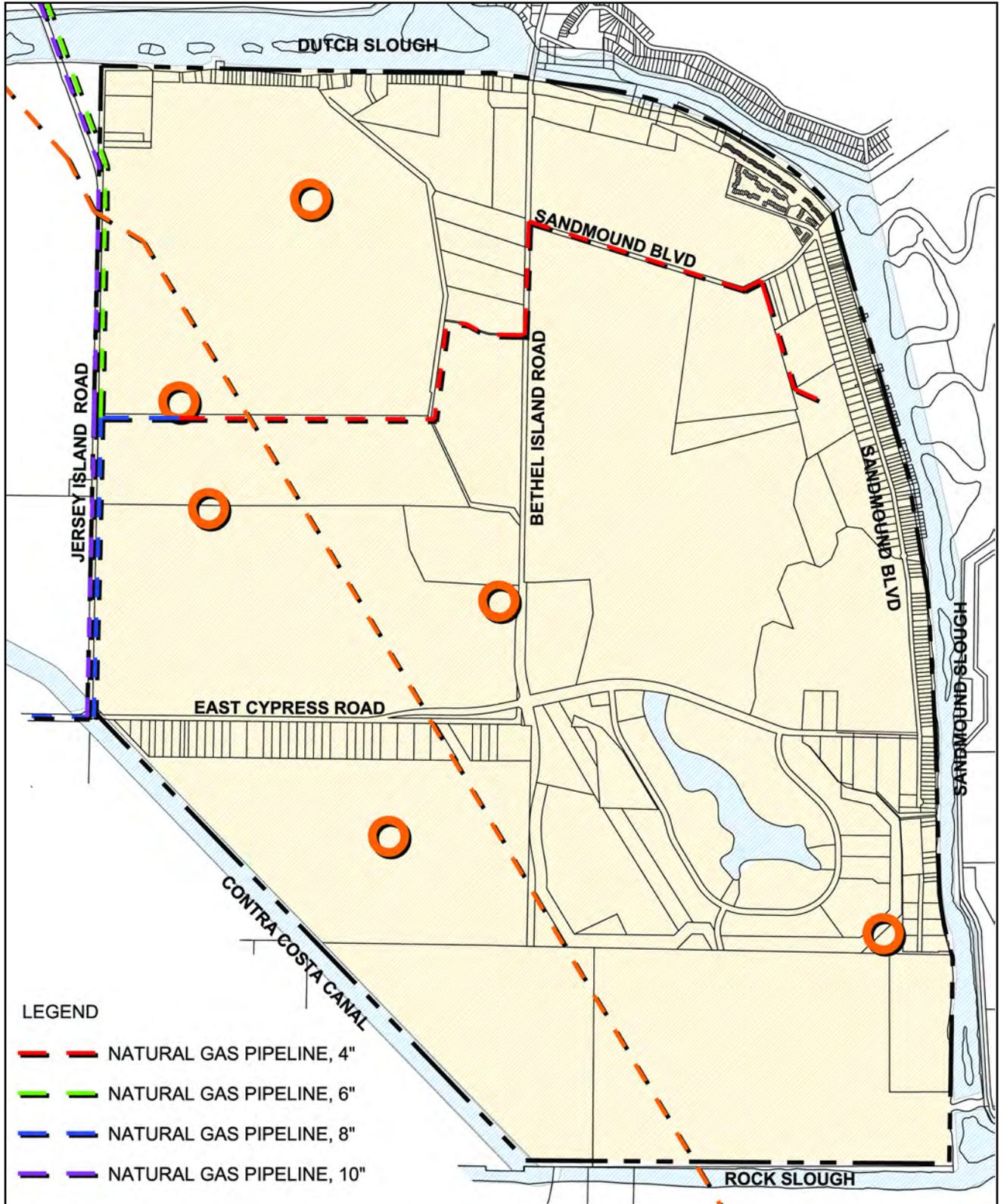


Exhibit 15
Existing Natural Gas Facilities





Exhibit 16
School District Boundaries

the Delta levee system. The levees are subject to damage in the event of a major earthquake. Because the levees were constructed as non-engineered fills over an un-compacted base, it is unknown exactly how they will respond to a major earthquake event. The non-engineered levees provide flood protection within their perimeter. These levees generally do not meet FEMA standards for an Urban Standard Levee. For this reason, the entire area inside of the levees is within a designated flood zone with a base flood elevation equivalent to that of the outside flooding source of 7 mean sea level as established by FEMA. Isolated sand dunes within the Specific Plan Area are located in Zone C outside the flood plain.

The existing levee system maintained by RD 799 is approximately 8.9 miles long. The portion of non-engineered levee running along Dutch Slough and Sandmound Sloughs has extensive residential and commercial develop-

ments on it and immediately adjacent to it. Homes, retaining walls, decks, docks, and other structures have been constructed on both the landside and waterside of the existing levee. The remaining portion of the non-engineered levee system, along Rock Slough, the Contra Costa Canal, and those portions outside of the Specific Plan Area, has no structures on it or immediately adjacent to it as it is in undeveloped areas.

The existing levee top elevation varies from elevation 7.9 mean sea level to elevation 11.1 mean sea level. The 100-year flood plain elevation for the area has been established at 7 mean sea level by FEMA. Requirements for freeboard above the 100-year flood plain elevation as well as methods for engineered levee construction do not allow the existing non-engineered levee to be upgraded to a FEMA urban levee standard. For this reason, the existing non-engineered levees cannot be upgraded to remove the Specific Plan Area

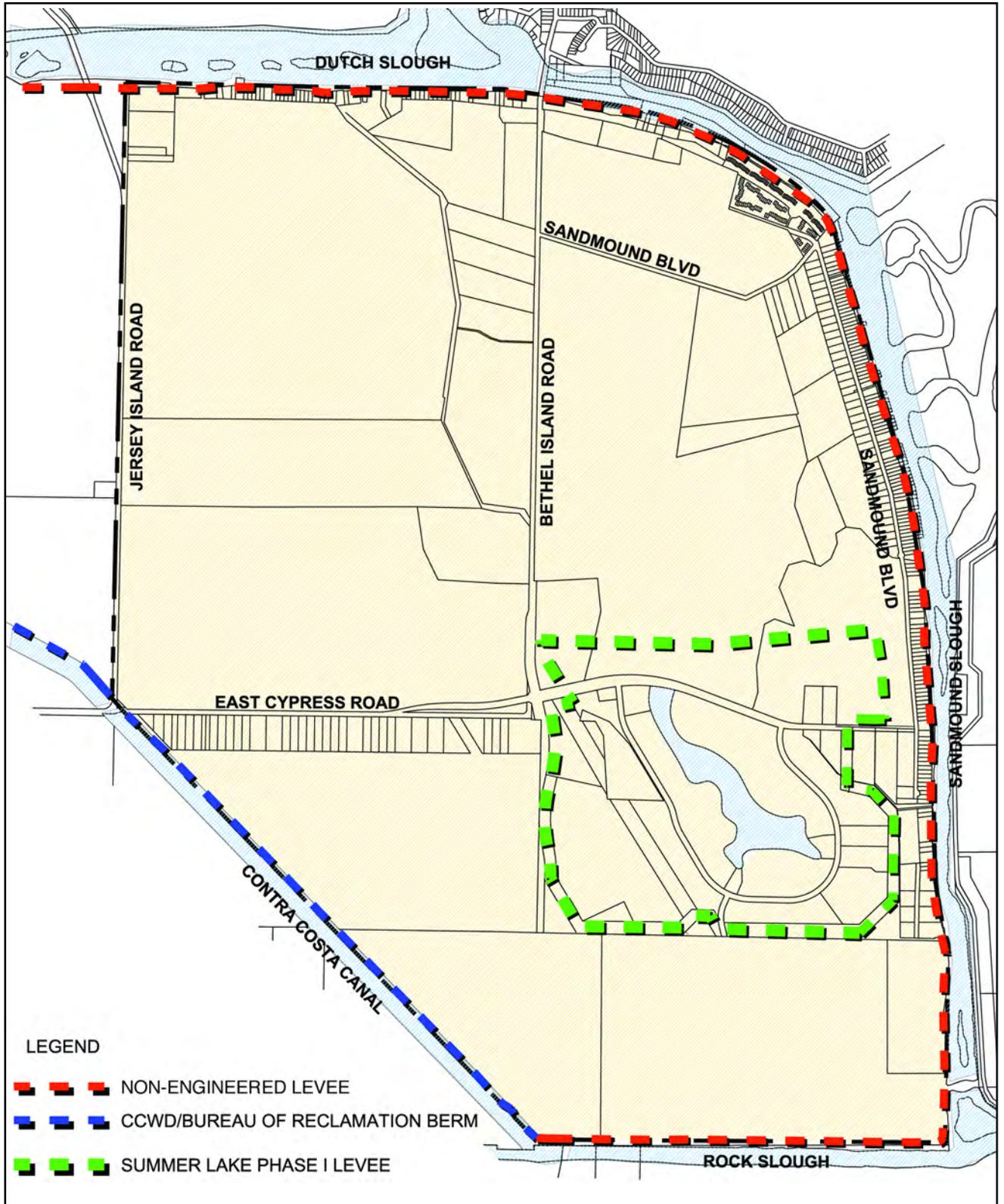


Exhibit 17
Existing Levee System



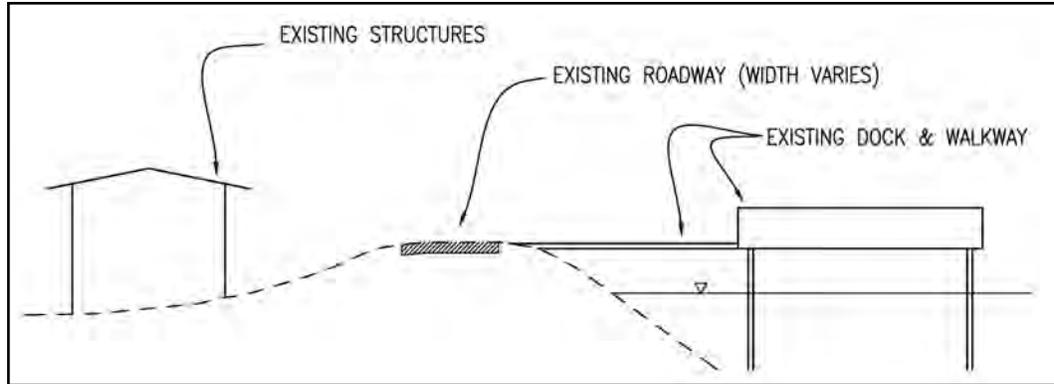


Exhibit 18
Existing Non-Engineered Typical Levee Section

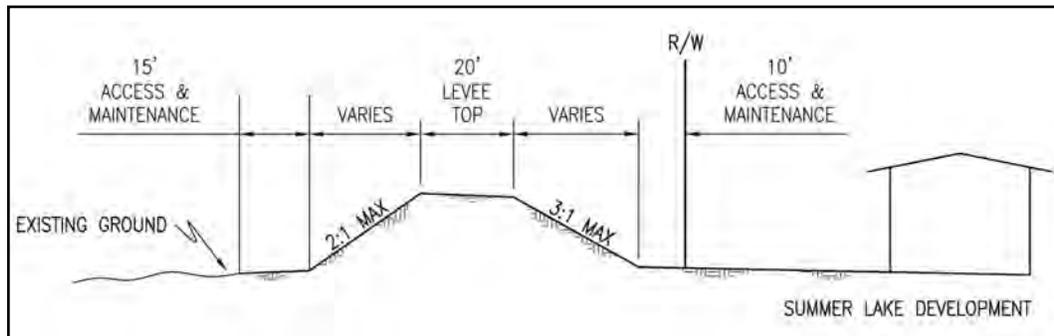


Exhibit 19
Summer Lake South Typical Levee Section

from the 100-year flood plain. A typical section of the existing non-engineered levee is shown in Exhibit 18, “Existing Non-Engineered Typical Levee Section.”

As part of the Summer Lake South development an engineered levee system independent of the non-engineered levee system is under construction. This levee was designed and constructed in accordance with FEMA standards for an urban levee. The purpose of the new proposed engineered levee is to remove the entire Summer Lake South development area from the 100-year flood plain and allow construction of homes. The Summer Lake South levee will be maintained by RD 799 pursuant to an agreement entered into between the developer and RD 799. The engineered levee

section for Summer Lake South is illustrated in Exhibit 19, “Summer Lake South Typical Levee Section.”

3.10 GEOLOGY AND SOILS

Soils studies performed within the Specific Plan Area have not encountered conditions that preclude development of the parcels. In general, the data obtained from these studies indicate that the southern portion of the Specific Plan Area is underlain by moderately stiff to stiff clay to depths ranging from about 1 to 13 feet with 3 to 5 feet being typical. The clay tends to be moderate to highly plastic, low to moderately compressible, and moderately strong. The clay tends to be underlain by loose to medium-dense silty, clayey, and “clean” sand to depths ranging

from about 15 to 25 feet below ground surface (bgs) and then becomes dense to very dense to depths over 130 feet bgs. The sand is occasionally interbedded with relatively thin (1 to 2 foot thick) strata of stiff to very stiff clay. The subsurface conditions underlying the northern portion of the SPA are similar, with the exception that the near surface clay is absent.

Organic silt and clay was encountered at the surface, above the clay and sand, in about one-half of the exploration data reviewed. In general, the low plasticity, organic soil appears to coincide with shallow depressions, swales, or wetlands that occur randomly throughout the Specific Plan Area. In general, the organic soil appears to range from less than 1/2 foot to 3 feet in thickness, with 1 to 2 feet being typical, generally has an organic content ranging from 10 to 20 percent loss on ignition, and is highly compressible and weak. Investigations performed along the Sandmound and Dutch Slough levees, the Rock Island Marina development and other properties indicate the existing non-engineered levees are generally underlain by about 2 to 8 feet of peat and peaty mud. In many places, this peat and mud appear to extend inland about 10 to 50 feet from the landside toe of the levees and could extend farther along old buried channels and ditches that once connected to the sloughs. Many of the residences adjacent the non-engineered levees are likely supported over these highly compressible, weak, and organic soils. Peat was also encountered in a few areas adjacent to or within areas designated as wetlands. The organic content of the peaty soil typically ranges from about 40 to 70 percent loss on ignition.

Groundwater was encountered at depths as shallow as about 2 feet bgs in low lying areas to as deep as about 25 feet bgs in the vicinity of the small hills or knolls located in the north central portion of the Shea property. Overall, groundwater within the Specific Plan Area

tends to be encountered at relatively shallow depths at about 3 to 5 feet bgs. Groundwater level data obtained from two isolated observation wells suggest that the groundwater levels are likely influenced by stage variations and duration of stage in the Sandmound and Dutch Sloughs, as well as flows in the many irrigation and drainage ditches that traverse the Specific Plan Area. Based on the limited data reviewed, groundwater fluctuation tends to range from less than 1 foot to 2 or 3 feet, with the highest groundwater levels recorded in the mid winter to early spring months of the year.

3.11 BIOLOGY

The Specific Plan Area contains several biologically sensitive areas as illustrated on Exhibit 11, "Site Constraints." The most predominant biologically sensitive area occurs within the northwest corner of the Specific Plan Area south of Dutch Slough and west of the WAPA power line easement. This area will be preserved as a natural habitat area as part of the East Cypress Corridor Specific Plan.

Several different habitat types are found within the Specific Plan Area which support a diversity of plant and wildlife species. These habitats include irrigated pasture, alkali meadow, non-native annual grasslands, disturbed and ruderal areas, riparian scrub and forests, and aquatic environments within drainage and irrigation ditches, as well as natural freshwater marshes and the open waters outside of the levees along Rock Slough on the south, Dutch Slough on the north, and Sandmound Slough on the east.

In places, the Specific Plan Area includes sand mounds comprised of loose, sandy soils that have some potential to support certain wildlife species or plants characteristic of an interior dune community. Interior dune community habitats have been identified in a few locations within the Specific Plan Area. These various

habitats are valuable to wildlife, especially species endemic to the region.

Three sensitive natural communities, alkali meadow and grassland, Great Valley riparian forest/willow scrub, and Valley freshwater marsh, occur within the Specific Plan Area. Alkali meadow, Great Valley riparian forest/willow scrub, and Valley freshwater marsh are designated as sensitive natural communities by the California Natural Diversity Database maintained by the California Department of Fish and Game. Additionally, the aquatic communities Great Valley riparian forest/willow scrub and Valley freshwater marsh may fall under state and/or federal jurisdiction as wetlands or other waters.

On-site habitats have the potential to support special-status plant and wildlife species, species that receive protection under federal or state laws or are considered by the California Department of Fish and Game and/or the U.S. Fish and Wildlife Service to be species of concern. Potentially suitable habitat for special-status plant species occurring in the region is present within sand mounds, interior dune community, freshwater marshes, Great Valley riparian forest and willow scrub, mesic grasslands, and alkali meadows. Some of the plant species which have potential to occur within the Specific Plan Area include Delta button-celery (*Eryngium racemosum*), state-listed Endangered, crownscale (*Atriplex coronata* var. *coronata*), California Native Plant Society List 4, showy madia (*Madia radiata*), California Native Plant Society List 1B, large-flowered fiddleneck (*Amsinckia grandiflora*), federally and state-listed Endangered, and San Joaquin spearscale (*Atriplex joaquiniana*), California Native Plant Society 1B, among others.

Special-status wildlife species known to occur on site include several species of birds which forage and nest in the diverse habitats present

within the Specific Plan Area. Swainson's hawks (*Buteo swainsoni*), state-listed Threatened, are known to nest in the vicinity and utilize the irrigated pasture and grassland areas for foraging. Burrowing owls (*Athene cunicularia hypugea*), a California Species of Special Concern, inhabit ground squirrel burrows within grassland areas and elevated land features such as sand mounds and berms along irrigation and drainage canals. In addition, the sandy substrates have potential to support a distinct invertebrate fauna and native reptile species, including the silvery legless lizard (*Anniella pulchra pulchra*), a California Species of Special Concern. Western pond turtles (*Clemmys marmorata*), a California Species of Special Concern, occur within irrigation and drainage ditches when water is present and are also known to occur in the adjacent sloughs. These aquatic habitats also provide potential foraging, dispersal, and retreat opportunities for the giant garter snake (*Thamnophis gigas*), federally-listed Threatened and state-listed Threatened.

Dutch Slough, the Contra Costa Canal, Sandmound Slough, and associated irrigation and drainage canals are potential movement corridors, which may facilitate the movement of animals to and from the Specific Plan Area and may provide safe refuge for species that may forage within the Specific Plan Area during various times of the year, including the giant garter snake, if present, and western pond turtle.

SECTION 4. LAND USE

4.0 OVERVIEW

The 2,546 acre East Cypress Corridor Specific Plan is a master plan for the development of a new community offering a combination of traditional neighborhoods established within an open space setting. The Specific Plan comprehensively plans for the development of new land uses while preserving natural habitat in the form of wetlands and dune areas, and provides for an appropriate interface with existing residential uses within the Specific Plan Area. Approximately 28% of the total Specific Plan Area consists of open space, community and neighborhood parks, lakes, trails system and wetlands/dunes area forming a spine of recreational and open space connecting the various elements of the community.

The design of new development for the East Cypress Corridor Specific Plan features residential neighborhoods within walking and biking distance to parks, open space, schools and commercial uses. Pedestrian and bicycle connectivity is provided through the community via a network of trails to be developed within open space areas and on top of engineered levees ultimately linking to public sidewalks and on-street and off-street bicycle trails. Public open space and recreational amenities are provided to include lakes, community and neighborhood parks, and equestrian facilities to complement the natural setting afforded the Specific Plan Area. The East Cypress Corridor Specific Plan provides sites for public community facilities including three elementary schools, a middle school, and a fire station. Commercial development is planned to provide for the retail and service needs of the community.

Planned residential development is designed to address a variety of lifestyles such as singles, families, executive lifestyles, and “empty nesters.” Single-family detached housing types will include detached homes on lot sizes ranging from 2,100 square feet to 10,000 square feet. Lots of one-third acre up to one acre in size will be developed for semi-custom homes. Both detached and attached housing will be developed to fulfill the needs of first time homebuyers and provide needed rental housing.

The development of a Village Center at the key intersection of Bethel Island Road and East Cypress Road in Planning Area 1 is designed to capture local and regional retail sales while providing needed commercial services within walking or biking distance to residential neighborhoods. Local serving commercial uses will also be provided north of the intersection of Bethel Island Road and Sandmound Boulevard in Planning Area 6 as well as on a site adjacent to East Cypress Road in Planning Area 2.

The East Cypress Corridor Specific Plan offers a strong identity for residents and visitors through community elements which create a sense of place and a unique community identity. The East Cypress Corridor Specific Plan also provides for the necessary infrastructure and public facilities to support project development.

“Land Use Plan,” Exhibit 6, depicts the overall distribution of land use for the Specific Plan Area.

4.1 LAND USE SUMMARY

The Land Use Plan for the East Cypress Corridor Specific Plan is designed in a manner to reflect the traditional characteristics of older California neighborhoods. The design of the community envisions the development of active and passive recreational amenities for the community of Oakley. Homes will be sited to relate to parks, lakes, and open space wherever possible to further enhance the open space feel of the community and to promote a human scale of development.

Casual social interaction is encouraged among residents within informal gathering places located in parks and along sidewalks and trails. The pedestrian and bicycle circulation system is designed within a network of open space and greenbelts connecting to levee trails and to local and arterial streets. This system brings people from residential neighborhoods to the community parks, neighborhood parks, lakes, and natural habitat areas affording opportunities for both residents and visitors to enjoy these open space amenities.

4.1.1 RESIDENTIAL USES

Residential land uses within the Specific Plan Area account for approximately 1,542 acres, or 61% of the Specific Plan Area. Planning Areas 1-4 comprise approximately 913 acres of the total residential area and represent the area proposed for new development as part of the East Cypress Corridor Specific Plan. Approximately 149 acres in Planning Area 5 are designated for residential land use as the Summer Lake South development. Approved by Contra Costa County in 1993, this project is currently under construction. Existing and future residential land use comprises approximately 481 acres of Planning Area 6 including existing residential and agricultural land use areas not proposed for development as part of the Specific Plan.

4.1.2 RESIDENTIAL DISTRIBUTION

The East Cypress Corridor Specific Plan permits an ultimate development capacity of 5,759 residential dwelling units in a variety of single family detached and multi-family residential products. A total of 4,036 new residential units are proposed for development within Planning Areas 1-4. Within Planning Area 1, up to 150 dwellings could be developed within a portion of the Village Commercial Center pursuant to the provisions established in Section 8.10.2.2 “Implementation.” A maximum of 628 residential units are approved for Planning Area 5, which is under construction. Within Planning Area 6 approximately 1,095 residential dwelling units are possible in the future based upon existing residential units (544) and the potential build-out of residential land uses on vacant properties.

The maximum dwelling unit allocation of 5,759 for the Specific Plan Area may be exceeded under the following conditions:

- Second units, as defined by the City of Oakley Zoning Ordinance, are permitted pursuant to the special development regulations for second units as established in Chapter 5, Section A of the City of Oakley Zoning Ordinance.
- The provision of affordable housing pursuant to the City’s Housing Element and Zoning Ordinance, Chapter 4, Sections A.5 and A.6.

The following describes the types of residential development permitted within the Specific Plan Area:

AGRICULTURE LIMITED RESIDENTIAL
43,000 square foot lots or greater

The Specific Plan provides for up to 255 single family lots in the Agriculture Limited

Residential land use district within Planning Areas 2 and 6. Up to 12 new lots of this type are proposed within this residential type within Planning Area 2. Within Planning Area 6 a maximum of 243 lots in the “Agriculture Limited Residential” type will be permitted.

SINGLE FAMILY RESIDENTIAL LOW DENSITY
15,000 – 39,999 square foot lots

The Specific Plan allows for the development of up to 198 single family detached dwelling units on lots ranging from 15,000 square feet up to 39,999 square feet in size. Approximately 23 new “Single Family Residential Low” density lots will be developed in Planning Area 3. Within Planning Area 6 up to 175 “Single Family Residential Low” low density lots will be permitted.

SINGLE FAMILY RESIDENTIAL MEDIUM
8,000 – 14,999 square foot lots

The Specific Plan allows for the development of up to 958 new single family detached dwelling units on lot sizes between 8,000 and 14,999 square feet at an average density of 3.1 dwelling units per acre. Up to 900 dwelling units in the “Single Family Residential Medium” land use district are proposed for development in Planning Areas 1, 2, and 3. Up to 58 dwelling units within this land use district are permitted within Planning Area 6.

SINGLE FAMILY RESIDENTIAL HIGH
6,000 – 7,999 square foot lots

The Specific Plan allows for the development of up to 1,747 new single family detached dwelling units within Planning Areas 1-4 on lot sizes between 6,000 and 7,999 square feet at an average density of 4.2 dwelling units per acre. Within Planning Area 5, up to 272 “Single Family Residential High” dwelling units are approved for construction. Within Planning Area 6, up to 405 dwelling units in the “Single

Family Residential High” land use district are permitted.

MULTI-FAMILY RESIDENTIAL LOW
(DETACHED)

2,800–5,999 square foot lots

The Specific Plan allows for development of up to 1,156 new dwelling units with minimum lot sizes ranging from 2,800 square feet up to 5,999 square feet in size at an average density of 5.0 dwelling units per acre. New “Multi-Family Residential Low Detached” housing types will be developed in Planning Areas 1, 2, 4 and 5.

MULTI-FAMILY RESIDENTIAL LOW
(ATTACHED)

The Specific Plan allows for up to 163 units of “Multi-Family Residential Low Attached” housing at a density of 8.2 dwelling units per acre within Planning Area 6.

MULTI-FAMILY RESIDENTIAL MEDIUM

The Specific Plan allows for development of up to 404 units of “Multi-Family Residential Medium” housing within Planning Area 4. This housing type will be attached and detached product developed at a maximum 12.0 units per acre.

MOBILE HOME

The Specific Plan permits a total of 33 units in the “Mobile Home” land use district within Planning Area 6 at a maximum density of 9.6 dwelling units per acre corresponding to an existing land use.

4.1.2 TRADITIONAL NEIGHBORHOOD DESIGN

The East Cypress Corridor Specific Plan offers a strong identity for residents and visitors through a traditional approach to street design, architecture, and landscape design elements. A traditional design for local streets within residential neighborhoods includes gentle curves adding visual interest enhancing the pedestrian orientation of neighborhoods. On loop and collector streets, sidewalks will be separated from the street by a landscaped parkway promoting pedestrian mobility, encouraging neighbors to meet and greet each other along the street.

The architecture of new residences within the Specific Plan Area will be designed to focus on human-scale details to enhance the pedestrian friendly character of the residential neighborhoods. Such features may include the use of front porches, railings, enhanced entries, a mix of materials and textures, and authentic detailing on elements such as windows, doors, columns, balconies, and lighting.

Innovative garage designs will be utilized in order to de-emphasize the visual impact of garage doors on the streetscene. Such design techniques may include garage setback requirements, split-garages, turn-in garages or other similar devices that de-emphasize the view of garage doors from the street. Together, such design features enliven the streetscene and promote the friendly interaction of neighbors.

4.2 PARKS, RECREATION, AND OPEN SPACE

The City of Oakley requires the dedication of parks and open space within new developments in the City. Parks are required at a ratio of 6 acres per 1,000 residents to be distributed as 2 acres per 1,000 residents in community parkland, 3 acres per 1,000 residents in neighbor-

hood parkland and 1 acre per 1,000 residents in open space and greenbelts. The East Cypress Corridor Specific Plan includes approximately 588 acres as parks, lakes, and active open space. New parks consist of public community parks, and neighborhood parks. Open space consists of the preservation of utility corridors and canals improved for multi-use trails and passive recreational use. Public and private lakes, including two private beach clubs, provide open space amenities and recreational opportunities for residents. Beach clubs are for the exclusive use of residents within their respective Planning Areas. They may provide amenities such as pools, spas, party and fitness rooms, and business centers. The parks and open space system for the Specific Plan Area is illustrated in Section 7, Exhibit 70, “Open Space System Diagram.”

Approximately 68 acres of new neighborhood parks will be developed within Planning Areas 1-5. New community parks totaling approximately 41 acres will be developed within Planning Areas 3, 5 and 6. Community parks will be improved pursuant to the requirements of the City of Oakley and dedicated to the City for public use to include picnic areas, sports fields, unique play areas, trails, and open play fields. Support facilities such as restrooms and parking will also be provided. Neighborhood parks are planned within walking distance to residential neighborhoods and will be designed to fulfill the need for informal gathering spots and age appropriate play areas.

A key open space amenity provided in all Planning Areas will be the development of lakes and water related amenities for residents and visitors to the community. The Specific Plan Area provides approximately 159 acres of lakes and 6 acres in private beach clubs located within easy walking distance from residential neighborhoods.

4.3 COMMERCIAL/COMMERCIAL RECREATION USE

Approximately 93 acres, or 4% of the Specific Plan Area are permitted for development of new commercial uses within Planning Areas 1, 2, and 6. Approximately 37 acres of commercial recreation uses are located within Planning Area 6. A new Village Center of approximately 40 net acres is proposed within Planning Area 1. This Village Center is located adjacent to an open space corridor making it accessible by bicycle and is also located within walking distance to many residential areas. The central location of the Village Center within the Specific Plan Area makes access to commercial goods and services more convenient for all residents. The Specific Plan requires that a minimum of 20 acres of the Village Center be reserved for development of commercial uses and provides an option for development of the balance of the Village Center site for up to 150 residential dwellings under conditions as outlined in Sections 4 and 8 of the Specific Plan.

An approximately 1.7 acre local serving retail center is proposed in Planning Area 2 to address the convenience retail needs of the community. Approximately 44 acres within Planning Area 6 are designated for commercial use at the corner of Bethel Island Road and Sandmound Boulevard.

4.4 NATURAL HABITAT PRESERVATION

The Specific Plan land use plan identifies approximately 119 acres of wetlands/dune area to be reserved as a natural habitat area. Approximately 105 acres of the wetlands/dune area are located within Planning Area 1. Approximately 10 acres of wetlands are also located within Planning Area 2, and approximately 4 acres are reserved as part of the development plan approved for Planning Area 5, the Summer Lake South development. These areas

will be preserved in a natural condition as part of the project. As development of Planning Area 6 occurs in the future, additional wetlands/dune areas will be set aside as habitat area.

The 105 acre wetlands/dune area to be reserved within Planning Area 1 as natural habitat is located within the San Joaquin River flood plain. Wetlands/dune areas adjacent to the Dutch Slough will be left in a natural condition. Areas historically used for agriculture will be enhanced and returned to a naturalized condition. Public access to this area will be limited and the area will be managed for the purpose of habitat mitigation according to a Mitigation Monitoring Plan (MMP) adopted as part of the certification of the East Cypress Corridor Specific Plan EIR. The MMP will provide guidance on managing and monitoring the mitigation habitat. The wetlands/dune area will be preserved and enhanced in perpetuity for the following species and habitats:

- Wetlands and waters
- Riparian habitat and trees
- Alkali meadow and grassland
- Special status plants
- Special status invertebrates: dune insects and vernal pool fairy shrimp
- Special status reptiles: giant garter snake, western pond turtle and silvery legless lizard
- Special status birds: burrowing owl, Swainson's hawk and other nesting raptors
- Special status mammals: bats

Potentially, an overlook point will be provided along a multi-use trail on top of the engineered levee to be constructed adjacent to the wetlands/dune area.

4.5 TRAILS

A network of multi-use trails is planned within the open space corridors which span the length of the Specific Plan Area, and on the extensive proposed levee system. These trails will connect to trails and bike lanes within roadway rights-of-way, creating an inviting environment to encourage pedestrian and bicycle mobility. Existing utility corridors will be reserved and improved as open space to provide for the development of a major component of the trail system. The trail system will also be developed within the rights-of-way of arterial and collector streets and on the top of the engineered levee system. The comprehensive trail system planned for the Specific Plan Area will provide connectivity throughout the new planned community and provide access to the wetlands/dune habitat areas, commercial areas, commercial recreation areas adjacent to Dutch Slough and Bethel Island. The trail system will allow residents to bike or walk to and from their residences to schools, parks, open space, commercial centers and natural habitat areas.

The City of Oakley General Plan identifies a proposed multi-use trail which travels eastward on East Cypress Road and northward on Bethel Island Road, terminating at Bethel Island. This trail, which connects the Specific Plan Area to the rest of the City, will be constructed within the landscape setbacks along East Cypress Road and Bethel Island Road, along with on-street Class II bikeways on this route.

Connections to the City's trail network will include extensions of the multi-use trail and bikeways to be developed southward along Bethel Island Road, eastward along East Cypress Road, and within the rights-of-way of Jersey Island Road and major collector streets within Planning Areas 1-5. Multi-use trails to be developed within the open space areas and on the top of the proposed engineered levee

system will also connect to the City's multi-use trail system. Trail connections will be developed from the open space areas into and through parks and onto the levee trail system completing the network of connectivity to schools, parks and open space.

Exhibit 71 "Trails Diagram," in Section 7, illustrates the trail network proposed for the Specific Plan Area.

4.6 ARCHAEOLOGICAL SITES

Archaeological sites within the Specific Plan Area will be avoided by development to the extent possible. Where archaeological sites cannot be avoided, they will be incorporated into parks and open space. Where archaeological sites cannot be avoided or incorporated into parks, green space or open space, these sites will be treated with a layer of chemically stable soil prior to the construction of facilities on top of them. Types of facilities which will be permitted to be constructed on top of archaeological sites include tennis courts, parking lots, or similar facilities, and school playgrounds.

4.7 GAS WELLS AND DRILL SITES

The Specific Plan Area contains six active gas wells located throughout Planning Areas 1, 2, 3 and 5. These gas well sites will continue in operation as part of the development of the Specific Plan Area and will be screened and incorporated into parks wherever possible. For planning purposes, an area of approximately 1 to 2.5 acres has been reserved for gas well sites within the Specific Plan Area. In instances where residential areas will be developed around a gas well site, a minimum setback will be maintained between the gas well head and habitable residential structures. Future drilling of new gas wells within the Specific Plan Area is anticipated to be permitted by the City of Oakley with approval of a conditional use per-

mit and consistent with the development standards and design guidelines established in the ordinance adopted to regulate the development of new gas wells within the Specific Plan Area.

Gas well sites may be enlarged or shifted based upon the site specific needs and findings as part of subsequent conditional use permits that are reviewed and approved pursuant to the Oil and Gas Chapter of the Zoning Code and the Specific Plan. In the event of a conflict between the Specific Plan and the provisions of the Zoning Code that regulate Oil and Gas Drilling, the Zoning Code provisions shall control.

4.8 PLANNING AREAS

4.8.1 PLANNING AREA 1

Planning Area 1 includes approximately 704 acres proposed for development by KB Home. Planning Area 1 is proposed for development of up to 1,700 residential dwelling units offering a variety of detached housing types on lot sizes ranging from 4,500 square feet to over 10,000 square feet. An approximately 40 net acre Village Center is proposed at the northwest corner of East Cypress Road and Bethel Island Road suitable for a variety of commercial uses including a supermarket/drug store, restaurants, and specialty stores. An approximately 12 acre site will be improved for development of an elementary school. Planning Area 1 will also reserve an approximately one acre site for development of community facilities anticipated to be used for sewer lift stations. Parks, recreational facilities, and open space within Planning Area 1 includes provision of approximately 22 acres of neighborhood parks, 30 acres of lakes with a 3 acre private beach club, and 56 acres of active open space. Approximately 105 acres of wetlands/dune area will be preserved as natural habitat area. Planning Area 1 contains three gas well sites on a total of 3.9 acres. The land use plan for Planning Area 1 is illustrated in

Exhibit 20 “Planning Area 1 - Land Use.” A conceptual site plan for Planning Area 1 is included in Appendix A to the Specific Plan and is provided for illustrative purposes only.

4.8.2 PLANNING AREA 2

Planning Area 2, also known as Summer Lake North, includes approximately 409 acres proposed for development by Shea Homes. Planning Area 2 will be developed with up to 816 detached residential dwelling units. An approximately 1.7 acre local serving commercial center will be developed on the north side of East Cypress Road near the intersection of Bethel Island Road. Approximately 20 acres will be reserved for development of a public middle school, and a 1.2 acre site will be reserved for development of a fire station and a lift station. Approximately 6 acres are designated for light industrial land use to accommodate an existing boat storage area and a future public facility anticipated for development by RD 799 to house levee maintenance equipment. Parks, recreational amenities and open space amenities to be provided within Planning Area 2 include development of approximately 18 acres of neighborhood park, approximately 71 acres for lakes, and approximately 7 acres as open space. Approximately 10 acres will be reserved for wetlands/dunes as natural habitat area. Planning Area 2 also includes two gas well sites on a total of 5.5 acres. The land use plan for Planning Area 2 is illustrated in Exhibit 21 “Planning Area 2 - Land Use.” A conceptual site plan for Planning Area 2 is included in Appendix A to the Specific Plan and is provided for illustrative purposes only.

4.8.3 PLANNING AREA 3

Planning Area 3 includes approximately 183 acres proposed for development by D.R.Horton. Planning Area 3 will be developed with up to 400 single family detached residen-

tial dwelling units on lots ranging between 6,000 to 20,000 square feet in size. Development of Planning Area 3 will also include approximately 11 acres of neighborhood parks, approximately 12 acres to be dedicated toward the development of a community park, 10 acres of lake amenity and 23 acres of active open space area. Planning Area 3 also includes one gas well site on 2.4 acres. The land use plan for Planning Area 3 is illustrated in Exhibit 22 “Planning Area 3 - Land Use.” A conceptual site plan for Planning Area 3 is included in Appendix A to the Specific Plan and is provided for illustrative purposes only.

4.8.4 PLANNING AREA 4

Planning Area 4 includes approximately 351 acres proposed for development by Bethel Island, LLC. Planning Area 4 will be developed with up to 1,120 residential dwelling units offering a variety of products including single family detached dwelling units as well as multi-family attached residential dwelling units of up to 12.0 dwelling units per acre. Development of Planning Area 4 will also include development of approximately 14 acres of neighborhood park, reservation of approximately 10 acres for development of an elementary school, approximately 23 acres of lake amenity and approximately 45 acres of active open space area. Planning Area 4 also includes three gas well sites on 6.3 acres. The land use plan for Planning Area 4 is illustrated in Exhibit 23 “Planning Area 4 - Land Use.” A conceptual site plan for Planning Area 4 is included in Appendix A to the Specific Plan and is provided for illustrative purposes only.

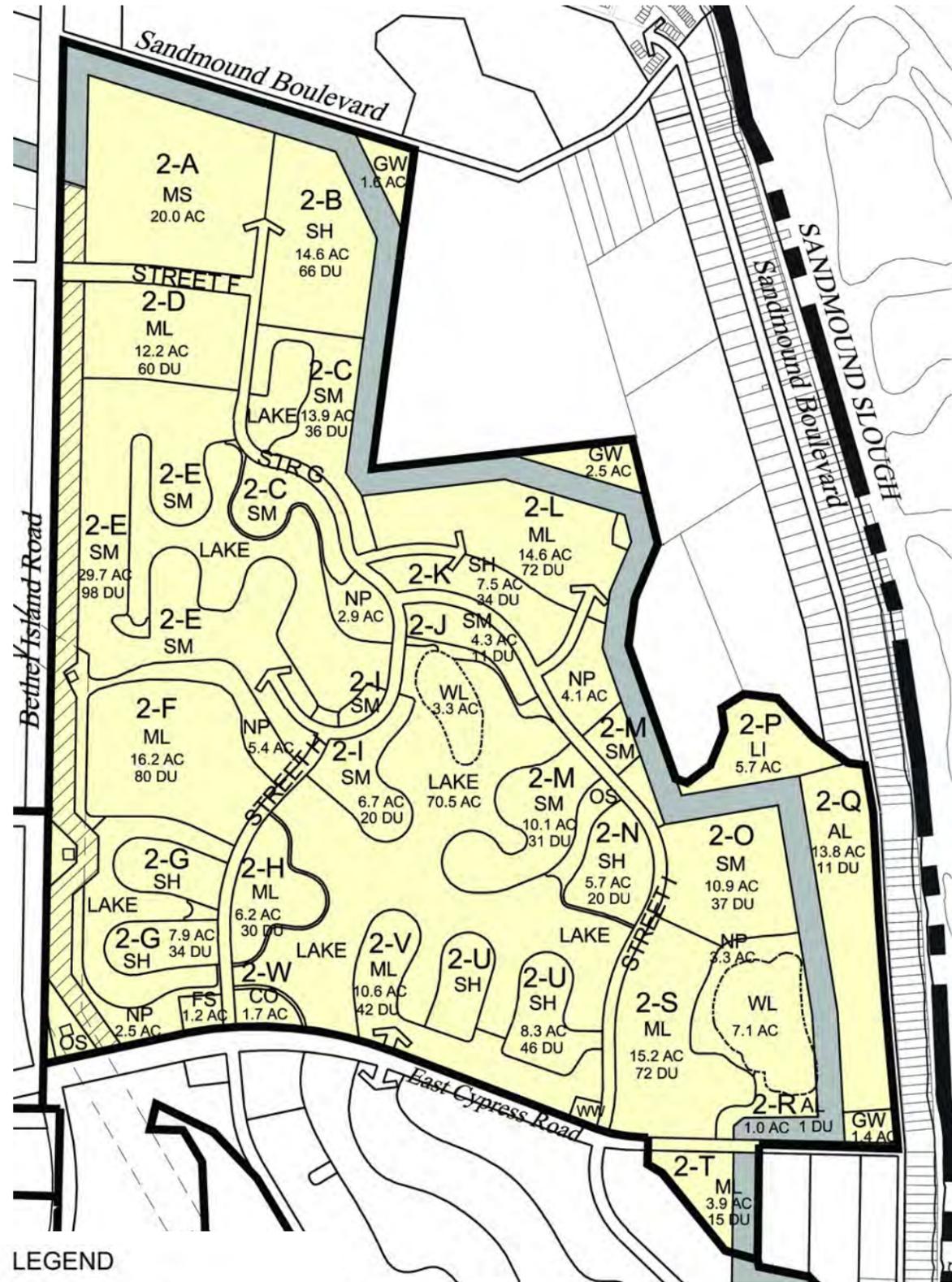
4.8.5 PLANNING AREA 5

Planning Area 5 includes approximately 269 acres of the approved Summer Lake South planned community being developed by Shea Homes. Planning Area 5 is approved for the

development of up to 628 single family detached residential dwelling units on lots ranging between 5,000 to 8,000 square feet in size. Development of Planning Area 5 includes a 10 acre elementary school, approximately 20 acres of community park area, approximately 3 acres of neighborhood park, approximately 25 acres of lake amenity with a 3 acre private beach club, approximately 13 acres of active open space area, and approximately 4 acres of wetlands/dune area to be preserved as natural habitat area. Planning Area 5 also includes one gas well site on 2.1 acres. The approved site plan for Planning Area 5 is illustrated in Exhibit 24 “Planning Area 5 - Land Use.”

4.8.6 PLANNING AREA 6

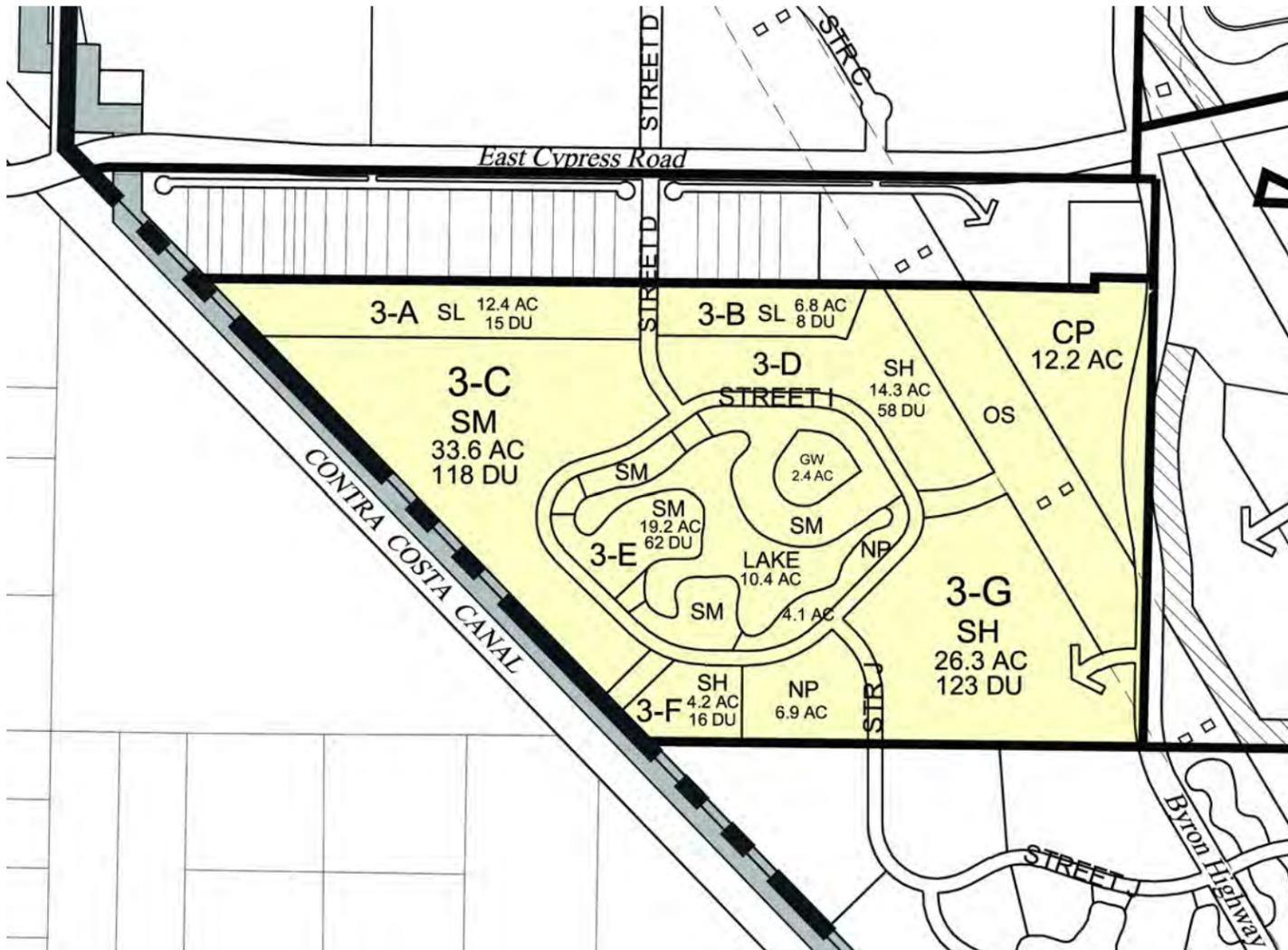
Planning Area 6 includes the approximately 631 acres within the Specific Plan Area containing existing residential, commercial and agricultural land uses. Planning Area 6 has the potential for up to 1,095 residential dwelling units, which includes both existing residences and areas for potential future development. Approximately 44 acres at the intersection of Sandmound Boulevard and Bethel Island Road are designated for development of retail commercial uses. New development within Planning Area 6 is not proposed as part of the East Cypress Corridor Specific Plan. Exhibit 25 “Planning Area 6 - Land Use” illustrates the land use designations for Planning Area 6.



Planning Area	Land Use	Description	Acres	Density Range	Target Units	Square Footage
2-A	MS	Middle School	20.0			
2-B	SH	Single Family Residential - High Density	14.6	3.8 - 5.5	66	
2-C	SM	Single Family Residential - Medium Density	13.9	2.3 - 3.8	36	
2-D	ML	Multi-Family Residential - Low Density (Detached)	12.2	5.5 - 9.6	60	
2-E	SM	Single Family Residential - Medium Density	29.7	2.3 - 3.8	98	
2-F	ML	Multi-Family Residential - Low Density (Detached)	16.2	5.5 - 9.6	80	
2-G	SH	Single Family Residential - High Density	7.9	3.8 - 5.5	34	
2-H	ML	Multi-Family Residential - Low Density (Detached)	6.2	5.5 - 9.6	30	
2-I	SM	Single Family Residential - Medium Density	6.7	2.3 - 3.8	20	
2-J	SM	Single Family Residential - Medium Density	4.3	2.3 - 3.8	11	
2-K	SH	Single Family Residential - High Density	7.5	3.8 - 5.5	34	
2-L	ML	Multi-Family Residential - Low Density (Detached)	14.6	5.5 - 9.6	72	
2-M	SM	Single Family Residential - Medium Density	10.1	2.3 - 3.8	31	
2-N	SH	Single Family Residential - High Density	5.7	3.8 - 5.5	20	
2-O	SM	Single Family Residential - Medium Density	10.9	2.3 - 5.5	37	
2-P	LI	Light Industrial	5.7			
2-Q	AL	Agriculture Limited	13.8	0.1 - 1.0	11	
2-R	AL	Agriculture Limited	1.0	0.1 - 1.0	1	
2-S	ML	Multi-Family Residential - Low Density (Detached)	15.2	5.5 - 9.6	72	
2-T	ML	Multi-Family Residential - Low Density (Detached)	3.9	5.5 - 9.6	15	
2-U	SH	Single Family Residential - High Density	8.3	3.8 - 5.5	46	
2-V	ML	Multi-Family Residential - Low Density (Detached)	10.6	5.5 - 9.6	42	
2-W	CO	Commercial(F.A.R. = 0.14)	1.7			10,000
	FS	Fire Station	1.2			
	NP	Neighborhood Park	18.2			
		Lakes	70.5			
		Levee	36.5			
		Interim Levee	14.9			
	OS	Open Space / Easement	6.9			
	WL	Wetlands / Dune Area	10.4			
	WW	Water Well	0.6			
	GW	Gas Well Sites	5.5			
		Roads (Bethel Island, Jersey Island, and E. C)	3.6			
TOTAL			409.0		816	10,000

Note: All acreages are based on gross area.

Exhibit 21
Summer Lake North
Planning Area 2 - Land Use

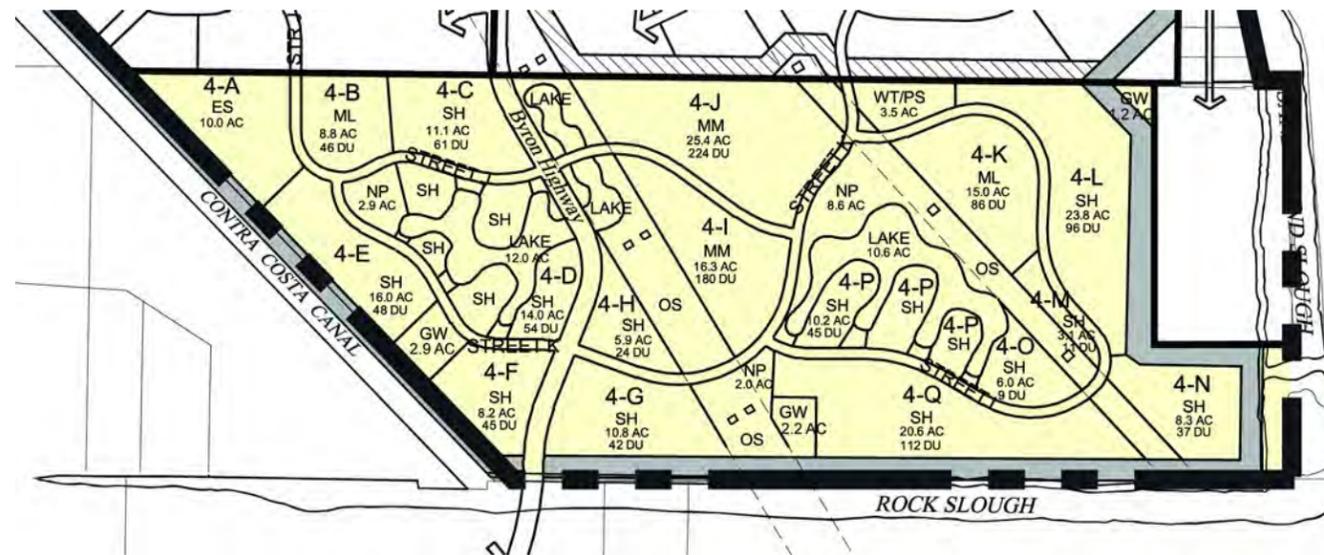


Planning Area	Land Use	Description	Acres	Density Range	Target Units	Square Footage
3-A	SL	Single Family Residential - Low Density	12.4	0.8 - 2.3	15	
3-B	SL	Single Family Residential - Low Density	6.8	0.8 - 2.3	8	
3-C	SM	Single Family Residential - Medium Density	33.6	2.3 - 3.8	118	
3-D	SH	Single Family Residential - High Density	14.3	3.8 - 5.5	58	
3-E	SM	Single Family Residential - Medium Density	19.2	2.3 - 3.8	62	
3-F	SH	Single Family Residential - High Density	4.2	3.8 - 5.5	16	
3-G	SH	Single Family Residential - High Density	26.3	3.8 - 5.5	123	
	CP	Community Parks	12.2			
	NP	Neighborhood Parks	11.0			
		Lakes	10.4			
		Levee	3.2			
	OS	Open Space / Easement	22.7			
	GW	Gas Well Sites	2.4			
		Roads (Bethel Island, Jersey Island, and E. Cypress)	3.8			
TOTAL			182.5		400	

Note: All acreages are based on gross area.

LEGEND

-  LEVEE
-  INTERIM LEVEE
-  PLANNING AREA
-  SPECIFIC PLAN BOUNDARY
-  ELECTRICAL TOWER

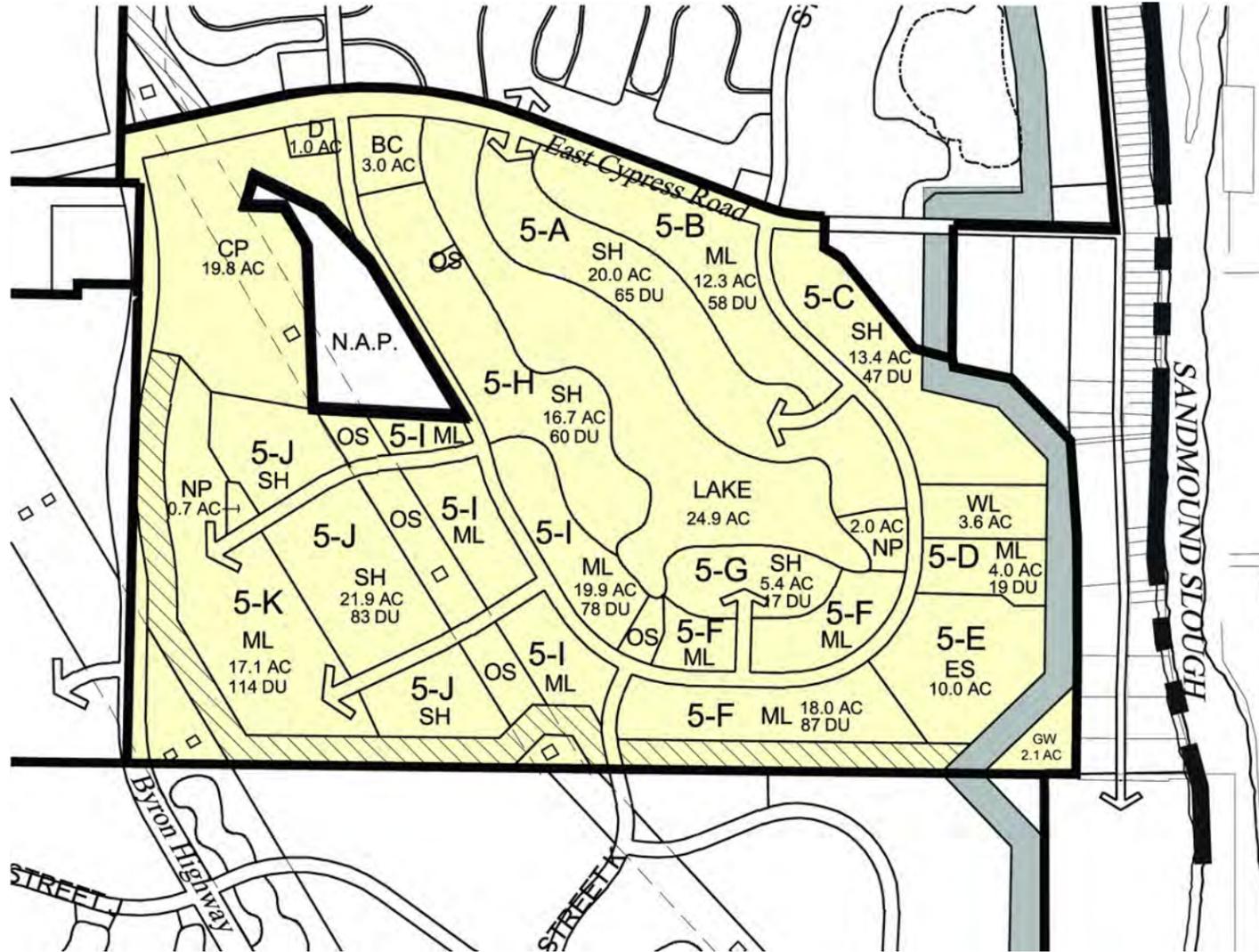


Planning Area	Land Use	Description	Acres	Density Range	Target Units	Square Footage
4-A	ES	Elementary School	10.0			
4-B	ML	Multi-Family Residential - Low Density (Detached)	8.8	5.5 - 9.6	46	
4-C	SH	Single Family Residential - High Density	11.1	3.8 - 5.5	61	
4-D	SH	Single Family Residential - High Density	14.0	3.8 - 5.5	54	
4-E	SH	Single Family Residential - High Density	16.0	3.8 - 5.5	48	
4-F	SH	Single Family Residential - High Density	8.2	3.8 - 5.5	45	
4-G	SH	Single Family Residential - High Density	10.8	3.8 - 5.5	42	
4-H	SH	Single Family Residential - High Density	5.9	3.8 - 5.5	24	
4-I	MM	Multi-Family Residential - Medium Density	16.3	9.0 - 12.0	180	
4-J	MM	Multi-Family Residential - Medium Density	25.4	9.0 - 12.0	224	
4-K	ML	Multi-Family Residential - Low Density (Detached)	15.0	5.5 - 9.6	86	
4-L	SH	Single Family Residential - High Density	23.8	3.8 - 5.5	96	
4-M	SH	Single Family Residential - High Density	3.1	3.8 - 5.5	11	
4-N	SH	Single Family Residential - High Density	8.3	3.8 - 5.5	37	
4-O	SH	Single Family Residential - High Density	6.0	3.8 - 5.5	9	
4-P	SH	Single Family Residential - High Density	10.2	3.8 - 5.5	45	
4-Q	SH	Single Family Residential - High Density	20.6	3.8 - 5.5	112	
	NP	Neighborhood Parks	13.5			
		Lakes	22.6			
		Levee	38.4			
	OS	Open Space / Easement	44.6			
	GW	Gas Well Sites	6.3			
	WT/PS	Water Tanks / Pump Station	3.5			
		Roads (Bethel Island, Jersey Island, and E. Cypress)	8.6			
TOTAL			351.0		1,120	

Note: All acreages are based on gross area.

LEGEND

-  LEVEE
-  INTERIM LEVEE
-  PLANNING AREA
-  SPECIFIC PLAN BOUNDARY
-  ELECTRICAL TOWER



Planning Area	Land Use	Description	Acres	Density Range	Target Units	Square Footage
5-A	SH	Single Family Residential - High Density	20.0	3.8 - 5.5	65	
5-B	ML	Multi-Family Residential - Low Density (Detached)	12.3	5.5 - 9.6	58	
5-C	SH	Single Family Residential - High Density	13.4	3.8 - 5.5	47	
5-D	ML	Multi-Family Residential - Low Density (Detached)	4.0	5.5 - 9.6	19	
5-E	ES	Elementary School	10.0			
5-F	ML	Multi-Family Residential - Low Density (Detached)	18.0	5.5 - 9.6	87	
5-G	SH	Single Family Residential - High Density	5.4	3.8 - 5.5	17	
5-H	SH	Single Family Residential - High Density	16.7	3.8 - 5.5	60	
5-I	ML	Multi-Family Residential - Low Density (Detached)	19.9	5.5 - 9.6	78	
5-J	SH	Single Family Residential - High Density	21.9	3.8 - 5.5	83	
5-K	ML	Multi-Family Residential - Low Density (Detached)	17.1	5.5 - 9.6	114	
D		Day Care	1.0			
CP		Community Park	19.8			
NP		Neighborhood Park	2.7			
BC		Beach Club	3.0			
		Lakes	24.9			
		Levee	9.6			
		Interim Levee	18.5			
OS		Open Space / Easement	13.0			
WL		Wetlands / Dune Area	3.6			
GW		Gas Well Sites	2.1			
		Roads (Bethel Island, Jersey Island, and E)	12.1			
TOTAL			269.0		628	

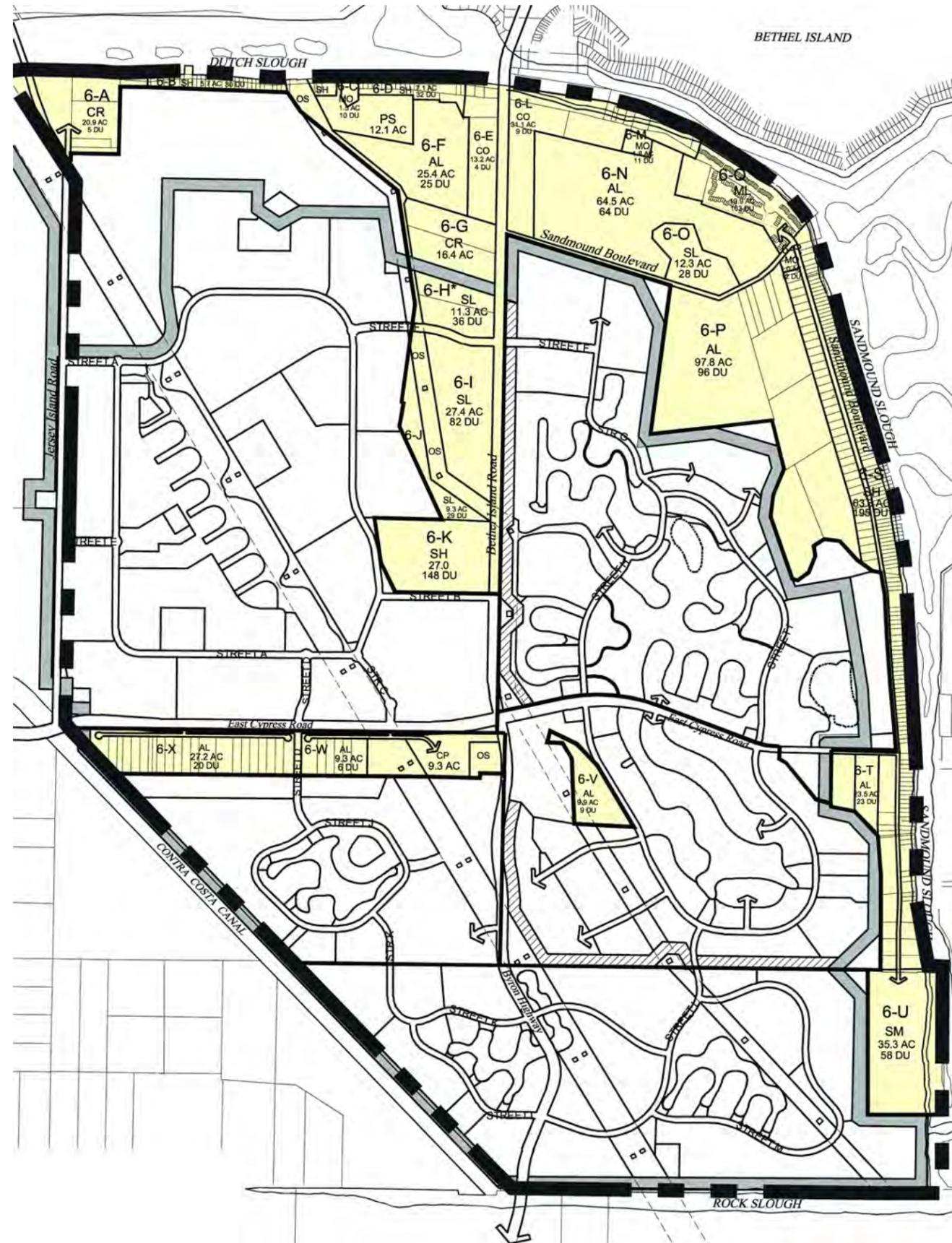
Note: All acreages are based on gross area.

LEGEND

-  LEVEE
-  INTERIM LEVEE
-  PLANNING AREA
-  SPECIFIC PLAN BOUNDARY
-  ELECTRICAL TOWER

Exhibit 24
Summer Lake South
Planning Area 5 - Land Use

SECTION 4. LAND USE



Planning Area	Land Use	Description	Acres	Density Range	Target Units	Square Footage
6-A	CR	Commercial Recreation (F.A.R. = 0.1)	20.9		5	91,000
6-B	SH	Single Family Residential - High Density	5.7	3.8 - 5.5	30	
6-C	MO	Mobile Home	1.5		10	
6-D	SH	Single Family Residential - High Density	7.1	3.8 - 5.5	32	
6-E	CO	Commercial (F.A.R. = 0.1)	13.2		4	57,500
6-F	AL	Agriculture - Limited	25.4	0.1 - 1.0	25	
6-G	CR	Commercial Recreation (F.A.R. = 0.1)	16.4			71,500
6-H*	SL	Single Family Residential - Low Density	11.3	0.8 - 2.3	36	
6-I	SL	Single Family Residential - Low Density	27.4	0.8 - 2.3	82	
6-J	SL	Single Family Residential - Low Density	9.3	0.8 - 2.3	29	
6-K	SH	Single Family Residential - High Density	27.0	3.8 - 5.5	148	
6-L	CO	Commercial (F.A.R. = 0.01)	31.1		9	135,500
6-M	MO	Mobile Home	1.8		11	
6-N	AL	Agriculture - Limited	64.5	0.1 - 1.0	64	
6-O	SL	Single Family Residential - Low Density	12.3	0.8 - 2.3	28	
6-P	AL	Agriculture - Limited	97.8	0.1 - 1.0	96	
6-Q	ML	Multi-Family Residential - Low Density	19.9	5.5 - 9.6	163	
6-R	MO	Mobile Home	1.0		12	
6-S	SH	Single Family Residential - High Density	63.8	3.8 - 5.5	195	
6-T	AL	Agriculture - Limited	23.5	0.1 - 1.0	23	
6-U	SM	Single Family Residential - Medium Density	35.3	2.3 - 3.8	58	
6-V	AL	Agriculture - Limited	9.9	0.1 - 1.0	9	
6-W	AL	Agriculture - Limited	9.3	0.1 - 1.0	6	
6-X	AL	Agriculture - Limited	27.2	0.1 - 1.0	20	
		PS	12.1			
		CP	9.3			
		Levee	4.5			
		OS	20.6			
		Roads (Bethel Island, Jersey Island, and E. Cypress)	21.6			
TOTAL			630.7		1,095	355,500

Note: All acreages are based on gross area.
 Actual commercial square footage is anticipated to be substantially less than 355,500 sf.
 * If the levee system is relocated to provide adequate protection, Planning Area 6-H may be considered for a Multi-Family Residential – High Density Affordable Housing Overlay District, at a maximum density of 20 units per acre.

LEGEND

- LEVEE
- INTERIM LEVEE
- PLANNING AREA
- SPECIFIC PLAN BOUNDARY
- ELECTRICAL TOWER

Exhibit 25
 Planning Area 6 - Land Use

SECTION 5. INFRASTRUCTURE AND COMMUNITY FACILITIES

The infrastructure, utilities, and community facilities to be provided in support of the development of the East Cypress Corridor Specific Plan are discussed in this section.

5.1 CIRCULATION

The circulation plan for East Cypress Corridor Specific Plan reinforces the concept of traditional neighborhood design. In addition to providing safe and efficient movement of vehicular traffic through the project, it also provides a safe environment for pedestrian movement and bicycle traffic reducing the reliance on the automobile as a means of travel. The “Roadway Hierarchy Diagram,” Exhibit 32 in Section 7, establishes the hierarchy and general location of roadways within the Specific Plan Area. The precise alignment of roads may vary to accommodate sloping terrain, site constraints, and existing development.

5.1.1 ARTERIAL AND COLLECTOR STREETS

The East Cypress Corridor Specific Plan includes two General Plan designated major arterial streets. Major arterial streets within the Specific Plan Area include East Cypress Road and Bethel Island Road extending between East Cypress Road and the northern boundary of the Specific Plan Area. The Specific Plan Area is bounded on the west by Jersey Island Road, a General Plan designated collector roadway. These arterial and collector roadways form the backbone circulation system to and from the Specific Plan Area.

5.1.1.1 East Cypress Road

East Cypress Road is a designated Major Arterial and traverses the Specific Plan Area in an east/west direction. East Cypress Road has a total designated right-of-way of 150 feet with 32 feet of pavement for east bound travel and 32 feet of pavement for west bound travel separated by a 16 foot wide landscaped median. On-street bicycle lanes, 6 feet in width, are provided. The north side of the street will include development of a 40 foot wide parkway with a 10 foot wide separated multi-use trail and a landscaped strip of varying width between the street and the trail. A varying width landscape buffer zone between the residential property line and the multi-use trail will be developed. The south side of the street will include development of a 30 foot wide parkway with a 6 foot wide pedestrian trail separated from the street by a landscaped strip of varying width. A landscape buffer of varying width will be developed within this 30 foot wide area between the back of pedestrian trail and the right-of-way.

Existing residences along East Cypress Road will continue to obtain access via the existing two-lane rural East Cypress Road. Safety considerations make it unadvisable for the existing residences to access directly onto the new major arterial roadway. As part of the development of the Specific Plan Area the frontage road will be improved with two points of access to the new major arterial roadway with the easterly and westerly ends terminating in typical cul-de-sac bulb configurations. This frontage road, as indicated on Exhibit 6 and Exhibit 22, will provide the residents proper access to their properties without interfering with the traffic pattern on the new East Cypress Road. Public utilities,

including sanitary sewer, potable water, and storm drainage will be installed in the existing public right of way to serve the residents along existing East Cypress Road. Residents will be provided with service stubs ready for connection in the future.

The new East Cypress Road will be constructed as a four-lane divided major arterial roadway easterly of Jersey Island Road extending east to connect with the Bethel Island Road intersection improvements. As part of the construction of the new East Cypress Road, the intersection at Jersey Island Road and the Contra Costa Canal will be reconfigured to coordinate with the proposed engineered levee to be developed at this point and the new Contra Costa Canal crossing. As the roadway approaches the intersection from the east it will ramp up to an elevation sufficient to allow for the inverts of all underground utilities to be above elevation 8 mean sea level as they pass over the proposed levee easterly of the Contra Costa Canal. The roadway will also be widened to six lanes west of Jersey Island Road, as it passes over the Canal. The Canal crossing will require modification of the existing siphon to allow for the upgrade to and extension of it. The roadway embankment for the intersection will function as a section of the proposed engineered levee system. East of the intersection with Jersey Island Road, East Cypress Road will ramp back down to existing grade in order to provide access to the frontage road along new East Cypress Road to the south. At the approach to the intersection of Bethel Island Road, East Cypress Road will ramp back up to meet the same roadway segment constructed as part of Summer Lake South. The intersection will remain at levee height in order to ensure access to the Summer Lake South development.

The proposed new East Cypress Road is illustrated in Section 7, on Exhibits 33-36, “East Cypress Section,” “East Cypress Approaching Bethel

Island Intersection,” “East Cypress Plan,” and “East Cypress/Bethel Island Intersection Plan.”

As part of the Summer Lake South development within Planning Area 5, the intersection of East Cypress Road and Bethel Island Road was raised to levee height. The intersection was improved to widen the roadway and raise the road to provide access to the Summer Lake South development. With the development of the remainder of the Specific Plan Area, East Cypress Road will need to be widened to full width, with curb and gutter installation. The developers of the Specific Plan Area will be responsible for the completion of the improvements of realigned East Cypress Road between Jersey Island Road and its terminus at Bethel Island Road.

5.1.1.2 Bethel Island Road

Bethel Island Road is designated in the City’s General Plan as a major arterial roadway between the intersection of East Cypress Road and the northern boundary of the Specific Plan Area. It is designated as a Minor Arterial between the intersection of East Cypress Road and the southerly boundary of the Specific Plan Area. North of the intersection with East Cypress Road, Bethel Island Road will be reconstructed as a major arterial roadway, with a total right-of-way of 150 feet. This includes a 16 foot wide raised median, 32 feet of paved area on both sides, including a 6 foot wide on-street bicycle lane. The west side of the street will include development of a 40 foot wide parkway with a 10 foot wide street separated multi-use trail and a landscaped strip of varying width between the street and the trail and a varying width landscape buffer zone between the property line and the multi-use trail. The east side of the street will include development of a 30 foot wide parkway with a 6 foot wide pedestrian trail separated from the street by a landscaped strip of varying width. A landscape

buffer of varying width will be developed within this 30 foot wide area between the back of pedestrian trail and the right-of-way.

North of the intersection with East Cypress Road, Bethel Island Road will ramp back down to existing grade. It will then follow existing grade until a point southerly of Sandmound Boulevard. From there it will rise to pass over the proposed northern levee planned within the Specific Plan Area. Once passing over the proposed northern levee Bethel Island Road will again go back to existing grade until it reaches the Contra Costa County Public Works Department Bethel Island Bridge improvement project.

The CCCPWD is designing a replacement to the Bethel Island Bridge. Federal funds are being utilized to finance the design and construction work. As part of this project, the bridge will remain as a two-lane bridge. CCCPWD is at the 65% phase in the process of preparing final plans for the replacement bridge and access roadways at this time. Access to the roadways adjacent to the bridge improvements will be modified. Access to Dutch Slough Road will be obtained via reconstructed Wells Road. The intersection of Wells Road and Bethel Island Road will be raised approximately 4'-5'. Wells Road will then ramp back down to existing grade. Approximately 160 feet east of the intersection, a levee access road is provided to the north. This access road will guide residents to the existing road on the existing non-engineered levee, and onto Dutch Slough Road.

Bethel Island Road south of its intersection with the new East Cypress Road will be improved as a minor arterial with a total right-of-way of 135 feet to include a 16 foot wide raised median, and 32 feet of paved travel area on each side of the street to include a 6 foot wide bicycle lane. On the east side of the street

a 25 foot wide parkway will be provided to include a 6 foot wide multi-use trail, a landscaped strip of varying width between the back of curb and the multi-use trail, and a landscaped buffer area of varying width between the back of the multi-use trail and the right-of-way. On the west side of the street a 30 foot wide parkway will be provided to include a 10 foot wide pedestrian trail, a landscaped strip of varying width between the back of curb and the pedestrian trail, and a landscaped buffer area of varying width between the back of the pedestrian trail and the right-of-way.

Bethel Island Road is planned to extend southerly to tie into the northerly extension of Byron Highway per the Contra Costa County General Plan, 1990-2005 Transportation and Circulation Element. Particular care is required in the design of Bethel Island Road as it extends southerly from the new East Cypress Road. The current intersection realignment under construction shifts the roadway easterly and in close proximity to the area's most prominent archaeological site, the Hotchkiss Mound. As Bethel Island Road nears the southern border of the Specific Plan Area it must begin to elevate to ultimately pass over Rock Slough and connect with the future Byron Highway extension. Design and construction of the improvements to the crossing of Rock Slough will be phased as part of the development of the Specific Plan Area as required by the City Engineer.

The proposed improvements for Bethel Island Road are illustrated in Section 7 on Exhibits 36-41, "East Cypress/Bethel Island Intersection Plan," "Bethel Island Road North of East Cypress Intersection," "Bethel Island Road North Section," "Bethel Island Road at Planning Area 4 Section," "Bethel Island Road at Planning Area 4 Plan" and "Bethel Island Road Approaching Rock Slough Section."

5.1.1.3 Jersey Island Road

Jersey Island Road is a General Plan designated collector roadway. Jersey Island Road will be improved to a total right-of-way of 68 feet. Between East Cypress Road and the northerly entry to Planning Area 1, the roadway will include 36 feet of paved area including a 6 foot wide bicycle trail on each side of the street. On the west side of the street a 12 foot wide landscaped area will be provided between the back of the curb and the right-of-way. West of the right-of-way, a multi-use trail is planned on top of the proposed levee which will parallel Jersey Island Road. On the east side of the street a 20 foot wide landscaped area will be provided to include a 6 foot wide sidewalk separated from the street by a 7 foot wide landscaped parkway and a 7 foot wide landscaped buffer area between the back of sidewalk and the right-of-way. At its intersection with East Cypress Road, this roadway will be designed at an elevation to allow for the inverts of all underground utilities to be above elevation 8 MSL as they pass over the proposed engineered levee. The roadway surface must be at least elevation 11 MSL to function as the flood control levee. Jersey Island Road will then ramp down to existing grade as it heads north. It will ramp back up to cross over the new levee at the northern levee crossing. It will ramp back down to existing grade to continue northerly to the existing Jersey Island Bridge.

The proposed improvements for Jersey Island Road are illustrated in Section 7 on Exhibit 42-45, “Jersey Island Road at Residential Section,” “Jersey Island Road at Wetlands Section,” “Jersey Island Road at Residential Plan” and “Jersey Island Road at Wetlands Plan.”

5.1.1.4 Project Collectors

Major project entry roadways for the East Cypress Corridor Specific Plan are designated as

project collector streets designed to provide access to and from Planning Areas. Project collector streets will have a total right of way of 100 feet to include a 16 foot wide median, 20 feet of paved travel area to include an 8 foot wide on street bike lane, and 22 foot wide parkways on each side of the street. Within the parkways an 8 foot wide landscaped area will separate the street from a 6 foot wide sidewalk. An additional 8 feet of landscaped area will be provided between the back of sidewalk and residential property lines. Project collector streets are illustrated in Section 7 on Exhibits 46 and 47, “Project Collector Road Section” and “Project Collector Road Plan.”

5.1.2 RESIDENTIAL STREETS

Residential streets proposed for Planning Areas 1-4 within the Specific Plan Area are described below and illustrated in Section 7.

5.1.2.1 Loop Roads

Loop roads are the primary circulation routes connecting each neighborhood within the Planning Areas. The right-of-way of the loop roads varies depending upon the orientation of the home to the street. Where homes do not front on the loop road the right-of-way will be 76 feet with 36 feet of paved travel area including a 7 foot parking lane on each side of the street. Parkways on one side of the loop road will be 17 feet in width improved with a 7 foot landscaped parkway, a 5 foot street separated sidewalk and a 5 foot planting area. On the other side of the loop road, the parkway will be 23 feet to include a 10 foot multi-use trail including decomposed granite shoulders. This wider trail and parkway should be provided on the side of the loop road with the most non-residential frontage where adjacent to parks, open space or lakefront areas.

When homes front on to the loop road, the right-of-way will be 60 feet with 36 feet of paved travel area including a 7 foot parking lane on each side of the street. Parkways will be 12 feet wide with a 7 foot wide landscaped area adjacent to the curb, and a 5 foot sidewalk behind the landscaped area. Appropriate traffic calming measures will be incorporated into the design of front-loaded loop roads.

Loop roads planned for the Specific Plan Area are illustrated in Section 7 on Exhibits 48-51, “Loop Road Section,” “Loop Road Plan,” “Front Loaded Loop Road Section,” and “Front Loaded Loop Road Plan.”

5.1.2.2 Local Streets

Public local streets within the Specific Plan Area are designed to distribute vehicular traffic from the project collector streets and loop roads into and through residential neighborhoods. Local streets will have varied rights-of-way as described below:

The typical local street will have a right-of-way of 46 feet with 36 feet of paved area and a 5 foot sidewalk next to the street on each side. A 5 foot public utility easement will be located behind the sidewalk. Local streets proposed for the new residential development areas within the Specific Plan Area are illustrated in Section 7 on Exhibits 52-55, “Local Street Section,” “Local Street Plan,” “Minor Local Street/Cul de Sac Section,” and “Local Single Loaded Street Section.”

5.1.2.3 Alleys

Where private alleys are utilized in rear loaded single family detached and attached product areas, the alley right-of-way will be 20 feet of paved area with an additional area provided as a planting area occurring in the setback area up to the sides of the garage apron. Alleys planned for the Specific Plan

Area are illustrated in Section 7 on Exhibits 56-57, “Alley Section” and “Alley Plan.”

5.1.3 TRAFFIC CALMING

The East Cypress Corridor Specific Plan provides for traffic calming within residential neighborhoods to slow traffic and reduce traffic noise on streets contributing to safe and livable neighborhoods in which to walk, bike, and drive.

Traffic calming within the Specific Plan Area is designed to address the following:

- Reduction in traffic speeds.
- Reduction in traffic related noise.
- A safe and pedestrian-friendly circulation system to encourage walking.
- Non-restricted access for emergency services vehicles such as police, fire, and ambulances.

Factors affecting traffic speeds are those that influence the driver’s perception of the roadway such as:

- Type of adjacent development and distance of development from the roadway.
- Frequency of access points onto the roadway.
- Roadway alignment and curvature.
- Type and massing of landscaping adjacent to the roadway.
- Frequency of traffic control devices along the roadway.
- Narrowness of travel lanes.

At a minimum, the following traffic calming techniques will be implemented in design of the roadways within the Specific Plan Area.

STREET DESIGN

Residential Collector Streets and Loop streets within the Specific Plan Area are designed with

landscaping on either side within parkways to add interest in the street, encouraging drivers to slow their travel speed and observe their surroundings. On front-loaded loop roads, traffic calming measures will be included as appropriate to ensure safety. Specific measures will be addressed at the tentative map stage.

LANDSCAPING

Landscaping adjacent to streets within the Specific Plan Area will combine the use of shade trees, shrubs, and groundcover adjacent to sidewalks to create a more intimate streetscape encouraging drivers to slow down and observe their surroundings. The landscape concept for the Specific Plan Area is designed to contribute to a sense of the street system as a pedestrian protected area to promote slower traffic speeds.

ROUNDBABOUTS

Roundabouts will be incorporated into street design wherever feasible as a traffic calming and traffic noise reduction device. The use of roundabouts rather than conventional all-way stop intersection maintains traffic flow by requiring traffic entering the roundabout to yield to traffic traveling within the roundabout. The lack of stop signs allows entering traffic to merge with the flow of traffic.

5.1.4 PEDESTRIAN AND BICYCLE CIRCULATION

Pedestrian and bicycle mobility are key elements to the land use plan for new development areas within the Specific Plan Area. Two landscaped open space corridors will be developed in a north-south alignment through the Specific Plan Area developed with multi-use trails offering pedestrian and bicycle accessibility between local parks, the school sites, and residential neighborhoods.

The open space corridor will connect to a multi-use trail to be developed along East

Cypress and Bethel Island Roads. Development of the trail will provide a link in the City of Oakley Trail System. The open space trail will provide connectivity between residential areas, schools, parks, and commercial uses and connectivity to the Dutch Slough Restoration Area. Multi-use trails will also be developed on the engineered levee tops, providing additional links throughout the Specific Plan Area. Off-street pedestrian circulation will be provided throughout the Specific Plan Area through a network of sidewalks separated from vehicular travel lanes by a landscaped parkway. Pedestrian access points from sidewalks will be provided to the open space corridor through the local street system and lake edge trails. The pedestrian and bicycle trail system for East Cypress is illustrated in Section 7 on Exhibit 71 “Trails Diagram.”

5.2 WATER MASTER PLAN

The Specific Plan Area will be served by DWD. Planning Areas 1,2, 5 and a portion of Planning Area 6 are currently in the DWD service boundary. The planning areas not currently in the DWD service boundary will be required to annex into the District as part of the City’s planned boundary reorganization applications.

DWD purchases raw untreated water from the CCWD. A prerequisite to receiving water from DWD is annexation to the CCWD service area, as CCWD boundaries are coterminous with DWD boundaries within the specific plan area. The boundary reorganization will concurrently adjust both district boundaries. CCWD obtains raw water from two sources: 1) the Central Valley Project (CVP) and 2) the Los Vaqueros Project (LVP). In conjunction with the annexation to CCWD is the requirement to request inclusion into both the CVP and LVP service areas.

Planning Areas 1-4 and Planning Area 6 are currently outside of the CVP service area.

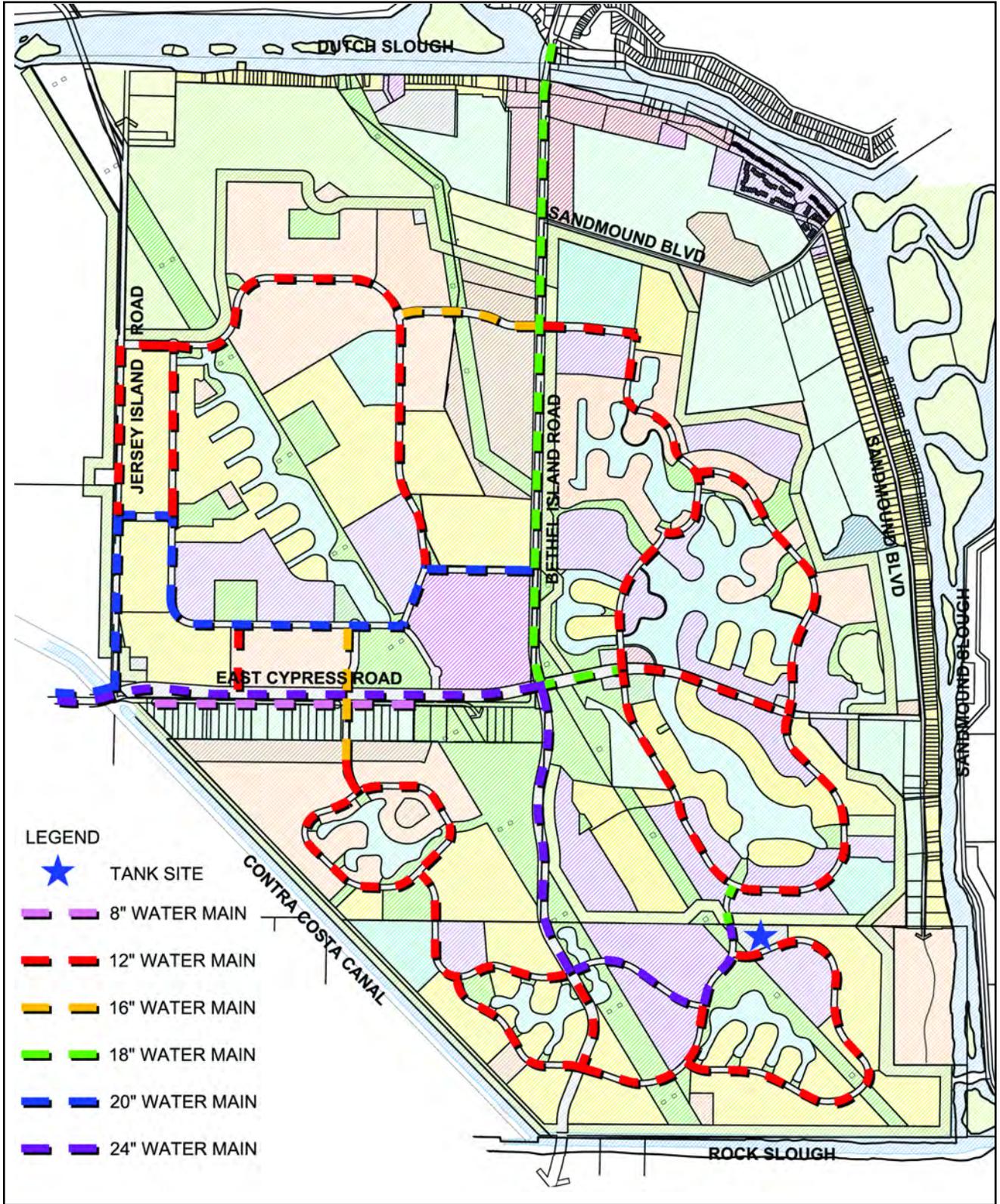


Exhibit 26
Water Master Plan



Inclusion into the CVP service area is at the discretion of the United States Bureau of Reclamation. Inclusion approval requires review by the United States Fish and Wildlife Service. This process is currently underway, and will be completed following the boundary reorganization.

Planning Areas 1-4 and 6 are outside the Los Vaqueros Project Area. These areas will be required to be placed within the LVP area. Planning Area 5 is currently being annexed to the LVP and will also be included in the CVP.

DWD recently completed a study to evaluate the District's water supply facilities in the vicinity of the project. The study was performed by DWD's consultant, Camp Dresser & McKee. The study determined that, with the development of the Specific Plan Area, additional water facilities are required. The facilities generally consist of new above-ground storage, looping of the water system within the Specific Plan Area, and looping of facilities beyond the boundaries of the Specific Plan Area. The future water facilities are shown in Exhibit 26, "Water Master Plan."

5.2.1 RESERVOIR

The required above ground storage capacity for the Specific Plan Area is six million gallons (Mg). This must be configured in a multiple tank scenario. Options for the multiple tank scenario are either two-3 Mg tanks or two-2.5 Mg tanks and one-1 Mg tank. Multiple tanks allow for maintenance operations to completely remove one tank from the system for cleaning and repairs while keeping the other tank(s) in operation and on line. Multiple tanks are also used to minimize water quality impacts in the event that water stands too long in one large reservoir. Tanks will be designed not to exceed 30 feet in height. A screening plan for the water tanks is illustrated in Section 7 on Exhibit 87, "Water Tank Screening Plan."

A pump system is associated with the reservoirs to supply maximum day demand flows and fire flows. As the ground elevation of the site and tanks are substantially the same, the opportunity for a gravity fed pressure system is not available. Requirements for the tank site include a large pump capable of meeting fire flow demands and several smaller domestic pumps to meet the total maximum day demand of the service area. One or more of the domestic pumps will need variable frequency drive to provide more flexibility for meeting lower flows. Standby pumps will be needed for both the emergency and domestic pumps. A permanent emergency backup generator of either natural gas fired or diesel, will be installed in the event of a power failure to the pumps. The land use plan for the site has provided a 3.5 acre site for the reservoir facilities. This includes space for all pumps, generator, telemetry control system, maintenance access around the tanks, landscape buffer, and storage space. DWD requires the site(s) to be fenced off, with no public access, for security and safety reasons. Screening of the facilities will be incorporated into the site design. Chlorination facilities may also be required.

In the event that the southern portion of the Specific Plan Area does not develop in a timely manner, DWD plans that an alternate site be provided in the northwest quadrant of the Specific Plan Area. This alternate site would only be needed in the event that the southern portion of the Specific Plan Area does not develop before the reservoirs are required. If the alternate site is used the waterlines in the northwest quadrant of the Specific Plan Area will require upsizing.

5.2.2 LOOP SYSTEM

The water system within the Specific Plan Area will be looped to provide necessary volume, redundancy and pressures. The DWD Major Distribution System Improvements in Eastern

Sphere of Influence from 1998 Facilities Plan and Summer Lake South Analysis figure shows the plan of the District for major water transmission lines within the Tract. Several loops are shown throughout the Specific Plan Area.

Available flows from the DWD system are sufficient to supply the needs for most of the Summer Lake South development and the Delta Coves project on Bethel Island. With construction of the Delta Coves project on Bethel Island, additional storage facilities will be installed at that site. With development of the Specific Plan Area, additional facilities will be needed to provide required water flow rates. The capacity constraint results from smaller line sizes and gaps in the DWD system. The additional lines are indicated on the DWD Major Distribution System Improvements in Eastern Sphere of Influence from 1998 Facilities Plan and Summer Lake South Analysis Plan and consist of, at a minimum, 8,000 linear feet of 24" water line in Sellers Avenue and 5,300 linear feet of 24" water main in Delta Road. These lines will be necessary to connect westerly to existing and/or proposed DWD major distribution mains. Additional lines may be required depending on the then current DWD system.

The Contra Costa Canal is located along the southwest boundary of the Specific Plan Area. The canal is owned by the United States Bureau of Reclamation and operated and maintained by CCWD. The canal supports water service to over 450,000 water users throughout Contra Costa County.

In response to the development of the entire Cypress Corridor, including the Specific Plan Area and those areas west of the Specific Plan Area, CCWD is planning to modify the unlined canal. CCWD is planning to replace four miles of the existing open-channel, unlined canal with a below ground pipeline.

CCWD has implemented an impact fee for all new development in the area of the canal at the rate of \$2,500 per single family detached unit, adjusted annually. This fee will be allocated to the undergrounding of the Contra Costa Canal, planned by CCWD. This fee is consistent with impact fees previously collected for development projects in the general vicinity of the Specific Plan Area, and will be collected by CCWD per their agreement.

The intersection of East Cypress Road and Jersey Island Road is planned to be reconstructed to be at levee height in order to provide access to the Specific Plan Area over the proposed levee. By widening and raising this intersection, the existing canal siphon under the existing intersection will be neither long enough nor have sufficient capacity to meet ultimate flows projected by CCWD. The siphon will need to be lengthened in order to provide area for roadway widening and slope. The roadway improvements shall be coordinated with CCWD to ensure that the canal crossing meets the needs and specifications of CCWD.

DWD has completed their calculations assuming full build-out within the Specific Plan area, the existing residents of Bethel Island, and the proposed Delta Coves development on Bethel Island. Because the majority of Jersey Island is owned by Ironhouse Sanitary District, with the exception of one isolated parcel, it is assumed that no further development will occur there. The existing uses have been included in the calculations. These calculations have found that the DWD has capacity to serve these areas.

5.3 SEWER MASTER PLAN

ISD has evaluated the proposed maximum development potential within the Specific Plan Area to determine if their existing facilities will accommodate all potential future development.

ISD has determined a peak flow rate of 7.2 million gallons per day (mgd) as the appropriate flow rate for District planning purposes. This figure includes full build out for Bethel Island pursuant to the 1990-2005 Contra Costa County General Plan maximum allowable residential densities, as well as the Specific Plan Area. The analysis completed by ISD used a peaking factor of 2.0.

The 14" force main in East Cypress Road currently conveys sewer flows from Bethel Island and sewered areas just south of Bethel Island to an 18" gravity main at the intersection of East Cypress Road and State Route 4. The 18" gravity main conveys flows northwest to the existing treatment plant.

ISD has determined that the existing 14" force main is inadequate to handle the proposed peak flow of 7.2 mgd from the Specific Plan Area and Bethel Island under sustained conditions. ISD has proposed a second 14" force main to assist in the handling of future flows from the area. Exact routing of this second force main has not been determined. One routing corridor that is being considered is within a proposed 50-foot easement south of the Contra Costa Canal running from Jersey Island Road and East Cypress Road (location of the proposed regional lift station) to the ISD wastewater facilities property.

When the intersection of East Cypress Road and Jersey Island Road is reconstructed to pass over the engineered flood control levee, the existing 14" force main will need to be modified. When the intersection passes over the flood control levee, all underground utilities that are to cross the engineered levee must have minimum invert elevations of 8 MSL. The existing intersection of East Cypress Road and Jersey Island Road is at elevation 6.5 MSL, necessitating the relocation of the existing 14" force main to a higher elevation. RD799

requires that all utilities that pass within/over the levee prism must be reviewed and approved by its governing board and engineers.

A new regional lift station is planned by ISD and is designated as Regional 1. The preferred site for Regional 1 is near the northeast corner of the intersection of Jersey Island Road and East Cypress Road. Regional 1 will have multiple pumps and permanent backup emergency generator. ISD estimates that with the development of Planning Area 5, the regional pump station will be required within two to three years. The existing 14" force main will be reconfigured to be routed through the Regional 1 pump station. The proposed second 14" force main will begin here and be routed out of Regional 1 along a corridor to be determined.

Each new development within the Specific Plan Area will be responsible for its own sewer collection facilities. Constraints of topography, soil conditions, depths of lines, and ground water elevation dictate that future sanitary sewer collection systems will be designed and constructed utilizing a design philosophy similar to that applied at Summer Lake South. A system of gravity lines draining to local lift stations which pump to a sub-regional lift station which ultimately pump to the proposed Regional 1 lift station will be employed. The sub-regional lift stations will have the capability of utilizing backup power from portable generators should electrical power be lost. Permanent emergency generators are not required at the sub-regional lift stations sites.

ISD owns and operates three raw sewage emergency storage ponds which were previously part of the now-abandoned Bethel Island wastewater treatment plan. The storage ponds are used to divert flow from the conveyance system when periodic maintenance and/or repair of the conveyance system is necessary. The most easterly and smallest of the existing ponds is clay lined.

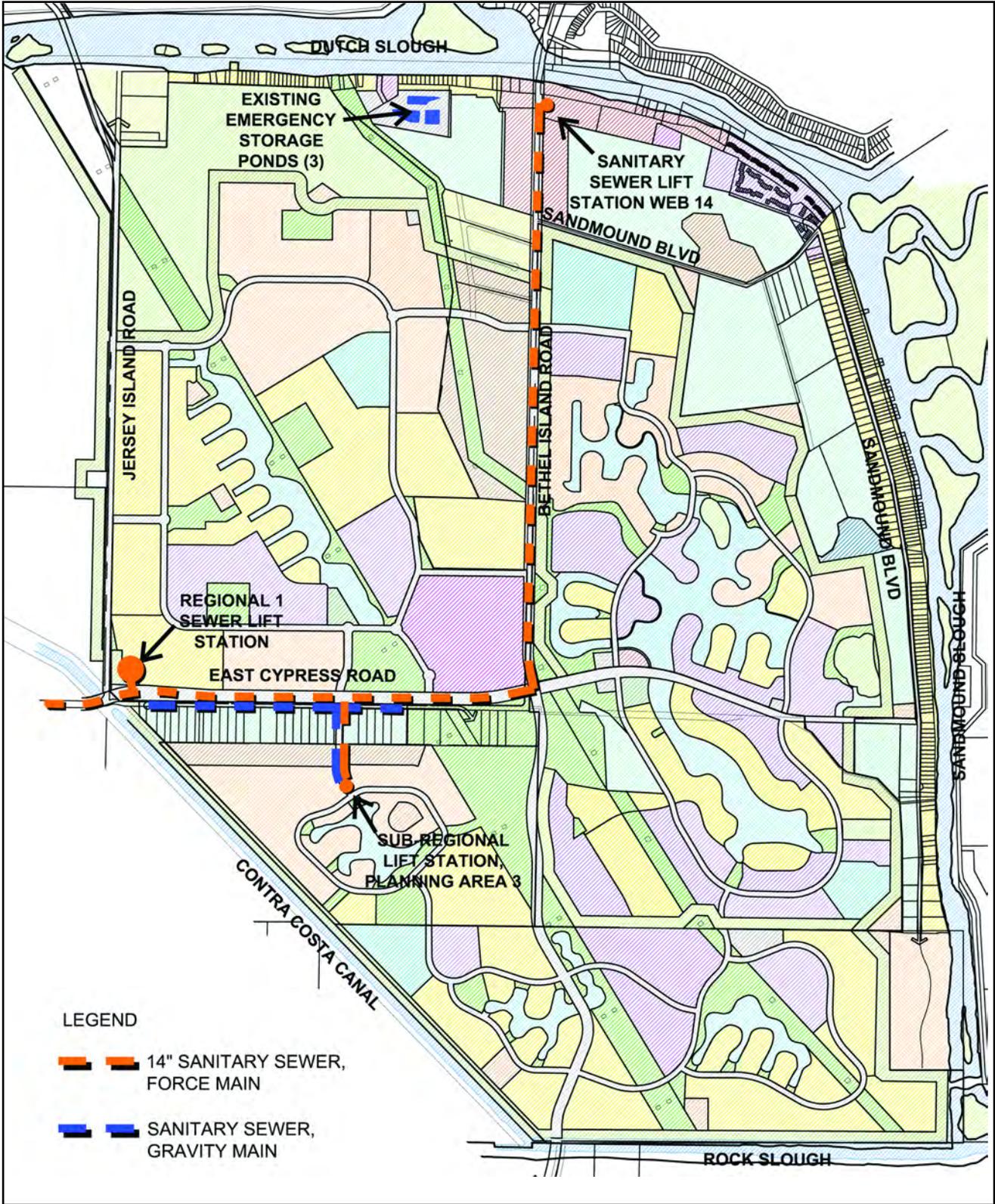


Exhibit 27
Sewer Master Plan



Shea Homes recently membrane-lined the most southerly pond to provide emergency storage capacity associated with the Summer Lake South development. The remaining unlined emergency storage must be membrane-lined to provide storage for the flow generated from the remaining Specific Plan Area and from growth areas on Bethel Island. ISD's design criteria is to provide three days storage at average dry weather flows. At an average flow of 3.7 mgd this equates to 11.1 million gallons of storage volume required. It is anticipated that additional storage over that provided by the three ponds at the Bethel Island Treatment Plant will be required. Location for the additional storage ponds has not been determined.

This existing 14" forcemain is inadequate to handle the proposed peak flow of 7.4 mgd from the Specific Plan Area and Bethel Island. ISD has proposed a second 14" forcemain to handle future flows. Exact routing of this second forcemain has not been determined. One routing corridor that is being considered is within a proposed 50-foot easement directly to the south of the Contra Costa Canal running from Jersey Island Road and East Cypress Road (location of Regional Pump Station 1) to the ISD wastewater facilities property.

At the time of the construction of widened East Cypress Road and the modifications to existing East Cypress Road, sanitary sewer facilities will be installed in existing East Cypress Road. The sewer mains will serve those residents adjacent to East Cypress Road. Sanitary sewer services will be provided to each property, and these properties will connect to these services in the future. A reimbursement agreement will be created for partial recovery of costs as properties in Planning Area 6 further develop.

ISD plans to construct a new treatment facility within the near future. The first phase of the new treatment plant is scheduled for planning,

design and construction, and is to be online in late 2009 or early 2010. At the current stage of development, the 18" main operates with no surcharge. Projected ultimate flows from the development of the Specific Plan Area and elsewhere in the City of Oakley necessitate that the 18" gravity main be upgraded to a 36" gravity sewer main. As development occurs within the boundaries of ISD, all new units, including those within the Specific Plan area, will be conditioned to pay a fee associated with the upgrade of the 18" main. The ISD plans to upgrade the sewer main in the same timeline as the construction of the new treatment plant.

The Sewer Master Plan for the Specific Plan Area is illustrated on Exhibit 27, "Sewer Master Plan."

5.3.1 EXISTING RESIDENCES

Existing residences in the urbanized areas of the Specific Plan Area in the Soundmound area and Dutch Slough Road area have access to public sewer facilities owned and operated by ISD. Existing residences along the south side of East Cypress Road currently do not have access to public sewer facilities. In conjunction with the construction of widened East Cypress Road, provisions must be made to accommodate the design of a gravity sewer system, potable water system, and storm drainage facilities to service the existing residences along the south side of existing East Cypress Road. These services will be provided within the existing right of way of East Cypress Road and available for use for each property in the future. Reimbursement agreements for these facilities will be created for partial recovery of costs as Planning Area 6 develops.

5.4 DRAINAGE

The developers of the Specific Plan Area will construct all required in-tract storm drain improvements necessary to serve the develop-

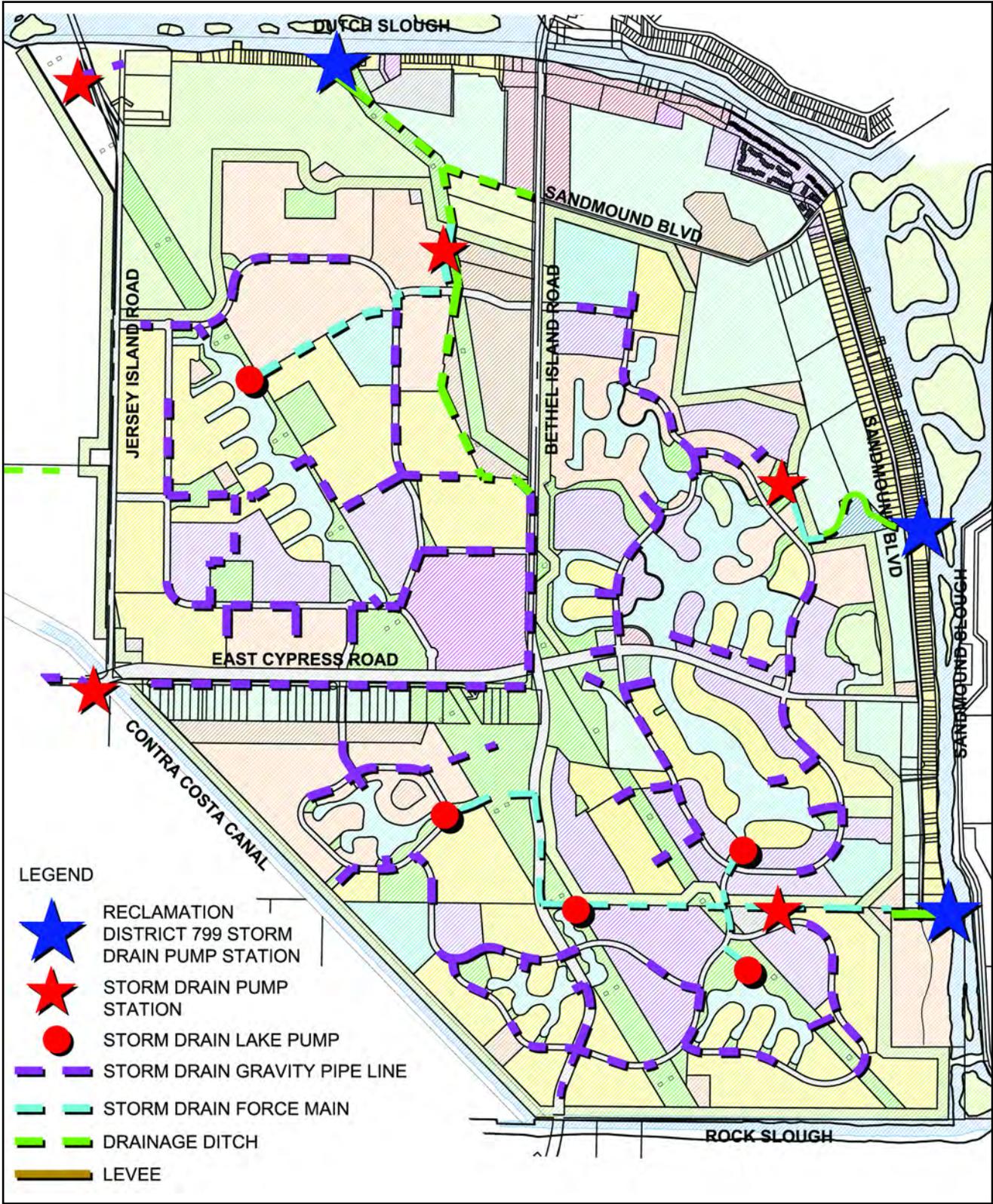


Exhibit 28
Drainage Master Plan



ment. The in-tract storm drainage will be designed to collect storm water runoff in a manner consistent with the requirements of the City of Oakley and Contra Costa County. The storm drain pipelines throughout each development will consist of pipe sizes ranging from 18” to 60” and will gravity flow to the proposed lakes for each development. The storm drainage can also be surface-drained to the lakes. Before entering the lakes, the storm drainage is directed through a constructed wetland, where it is filtered and cleaned before being discharged into the lake. The lake system is constructed with several means of filtration, including both mechanical and biofiltration systems to clean the project runoff. In the lakes, the storm water runoff will be detained, with portions of the runoff evaporating. This runoff may also be used for irrigation. Excess runoff that does not evaporate will then be pumped via individual lake pumps over the proposed engineered levee system. Discharge from the lake pumping system will be directed to RD 799 ditches and canals. The RD 799 ditches and canals drain to existing RD 799 pump stations. These pumps then lift the storm water runoff over the existing levee system and discharge into both Sandmound Slough and Dutch Slough. The Drainage Master Plan for the Specific Plan Area is illustrated in Exhibit 28, “Drainage Master Plan.”

5.5 FLOOD CONTROL AND LEVEES

Except for limited areas of high ground at elevations greater than seven feet, the entire Specific Plan Area is within a one percent chance floodplain designated by FEMA. The source of flooding for this floodplain is the adjacent Sacramento-San Joaquin Delta. The base flood elevation of 7.0 feet is the current estimate of the one percent chance flood event in the sloughs adjacent to the Specific Plan Area. The Specific Plan Area lies within the protection of an existing levee system. The levee

system is operated and maintained by Reclamation District No. 799 (RD 799).

In early 2004, the Board of Directors of RD 799 commissioned a feasibility study to determine the viability of upgrading the existing non-engineered levee system. The focus of the study was the existing levee system along Dutch Slough and Sandmound Slough referred to by RD 799 as the “urbanized levee.” The study investigated alternative methods of installing sheet piling as a means of reinforcing and raising the elevation of the existing levee system in order for it to be considered in compliance with FEMA standards. Addition of fill and expansion of the levee footprint was discounted as non-viable due to the impacts to the existing structures on and along the existing levee. The study, performed by Burns Engineering, Kjeldsen, Sinnock, & Neudeck, Inc., and Lowney Associates is available through the RD 799 offices.

The primary goal of the study was to determine a viable method of raising the top of the existing non-engineered levee to elevation 11 MSL and means to increase the stability of the levee system to resist earthquake induced loads. Several options were explored as part of the feasibility study. The top elevation of the levee must be raised to elevation 11 MSL to meet the FEMA requirements for 100-year flood protection. A basic method for raising the top of levee elevation is to add fill, however, when the delta levee system was constructed nearly 100 years ago, the non-engineered levee was constructed on native un-compacted soils and organic materials. Geotechnical investigations have found that the existing levees are constructed over a layer of organic material incapable of supporting additional fill or compaction.

Another concern of RD 799 is the stability of the existing non-engineered levee in response to an earthquake event. The feasibility study

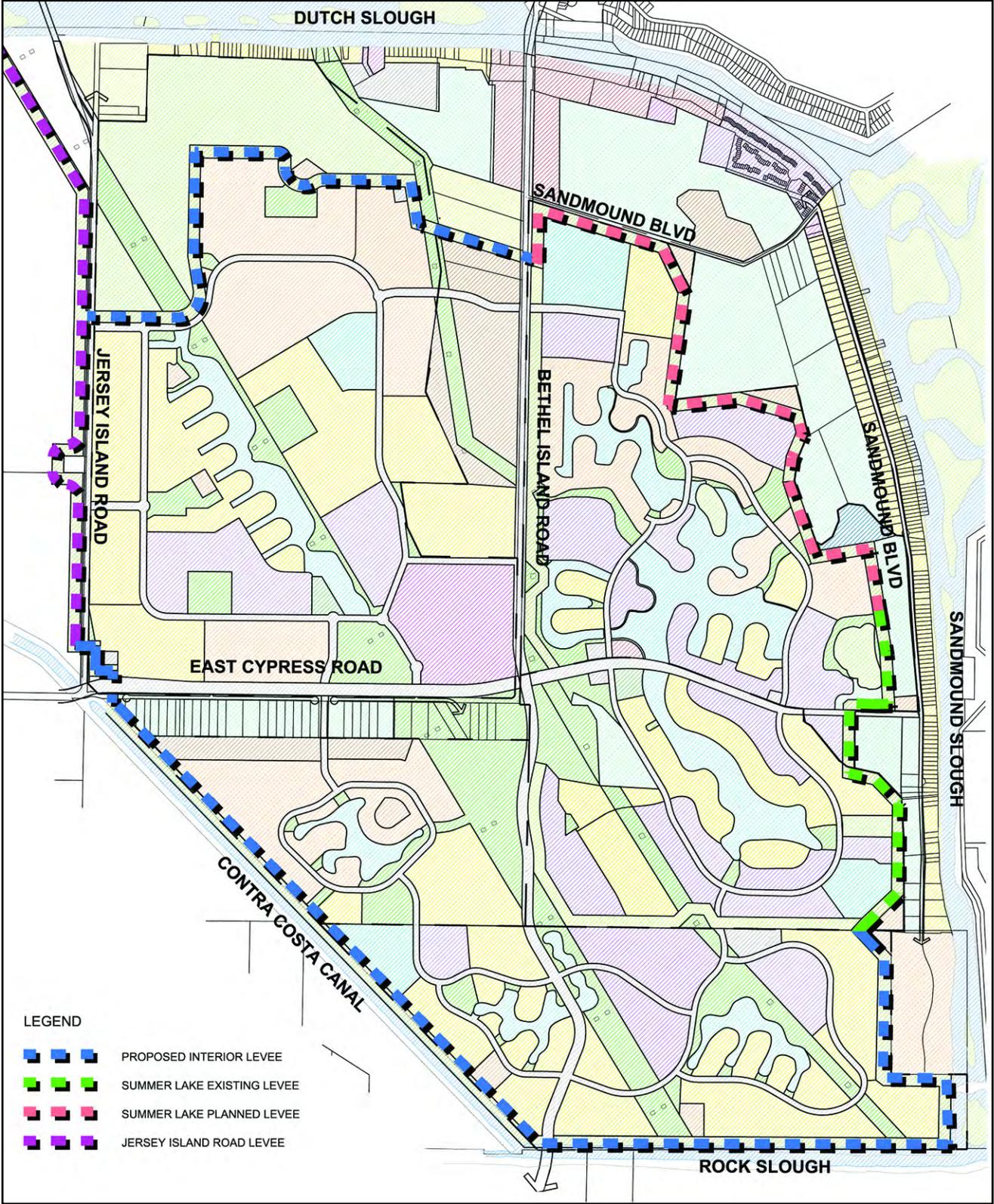


Exhibit 29
Levee Master Plan



examined the use of steel sheet piling to reinforce the existing levee and to raise the top of levee elevation to comply with FEMA requirements for 100-year flood protection. The study evaluated the design, constructability, and performance of the sheet piling in the event of an earthquake.

The study determined that the sheet pile wall would need to be 40 feet deep in order to anchor into the dense stiff soils underneath the levee. In order to install the sheet pile to this depth, the study evaluated the constructability of the sheet pile reinforcement.

Because the existing residences are built within close proximity of the existing non-engineered levee, construction becomes very difficult. Limited space is available for construction operations or equipment. Several options were examined for construction including vibration, impact hammer, and sheet press.

The vibration and impact hammer methods were both discounted due to the probable damage to adjacent structures and the likelihood of inducing a levee failure during construction. RD 799 evaluated the sheet press method for the purposes of the feasibility study. RD 799 and its consultants found a single method which mounts on top of the sheet pile and “presses” the next sheet pile section into the ground. The manufacturer recommended that for the depth required and the soil types in this area, that jetting would be necessary to install the sheet pile to the correct depth in several locations. The use of jetting was evaluated and found to reduce soil strength when utilized. The feasibility study concluded that a high probability existed of inducing a levee failure and creating significant cosmetic damage to several structures throughout the construction process. These risks were deemed unacceptable by RD 799.

The development of the Specific Plan Area includes a number of improvements that would markedly reduce flood hazards. The most important of these is the proposed engineered levee system, to be constructed by the developers of the Specific Plan Area. When completed to FEMA standards a Letter of Map Revision (LOMR) would be filed with FEMA to document the areas within the engineered levee system have indeed been removed from the one-percent chance floodplain. The LOMR would require extensive documentation, including an approved Levee Maintenance Plan, to meet FEMA’s approval standards.

The levee plan shown in Exhibit 29, “Levee Master Plan” shows a proposed engineered levee system. This levee system would be constructed either as a cooperative joint effort between the major property owners in the area or constructed in segments by each landowner. If the second scenario is utilized the construction activities would need to be coordinated to ensure the entire proposed engineered levee system is constructed and completed concurrently leaving no gaps. No homes may be built in Planning Areas 1-4 prior to the completion of the proposed engineered levee system. Provisions will be made for the proposed levee system to be constructed by others via a community facilities district, to proceed with development. The proposed engineered levee system would utilize the easterly existing engineered levee installed with the Summer Lake South development. The proposed levee will continue north following the easterly, northerly, and westerly edge of Planning Area 2 and cross Bethel Island Road approximately 500 feet north of Street F, the entrance to Planning Area 1 continuing westerly across Planning Area 1 to Jersey Island Road. The proposed levee will extend north and south on the west side of Jersey Island Road. The northern extension of the proposed levee will intersect the existing non-engineered levee west of Jersey

Island Road at Dutch Slough. The southern extension of the proposed levee will follow the west side of Jersey Island Road until it nears the Contra Costa Canal. The intersection of Jersey Island Road and East Cypress Road will be raised to levee height and tie into the proposed levee south of East Cypress Road. This portion of proposed engineered levee will head southeasterly and may be incorporated into the existing berm along the Contra Costa Canal. Where the existing berm will be incorporated into the proposed engineered levee system, the levee will be designed to meet FEMA standards for an urban levee, while utilizing the existing berm. Geotechnical evaluations of the existing berm will be performed to ensure that the existing berm has sufficient integrity. The proposed levee will continue southeasterly along the Contra Costa Canal and then turn easterly and be incorporated into the existing non-engineered levee along Rock Slough. From there it will continue easterly to the eastern boundary of the Specific Plan Area. It will then make a turn to the north, then to the west, then to the north, where it will tie into the existing Summer Lake South engineered levee.

The majority of the proposed levee system will be constructed as an engineered earthen levee, very similar to the levee constructed with the Summer Lake South improvements. The proposed levee will be approximately 150 feet at the base, with variations in base width to account for existing ground elevation variations.

The intersection of Jersey Island Road, East Cypress Road and the CCWD canal will be reconfigured to work with the proposed levee at this location. The canal will be modified with the levee improvements. With the widening of East Cypress Road and the raising of the intersection, the siphon under the intersection may be upsized and extended. As the levee heads southeast from this intersection, it may need to

be a self-supporting wall when adjacent to the existing residences.

Along the easterly side of the Bureau of Reclamation/CCWD canal, the top of the existing berm varies in height from 9.8 feet to 15.3 feet MSL. CCWD has discussed that during the construction of the proposed engineered levees along the canal/Specific Plan Area boundary, the existing berm may be incorporated into the proposed levee. A geotechnical analysis of the existing berm during the design phase of the proposed levee will support the design of this levee. RD 799 has an agreement with CCWD to upgrade the berm for flood control purposes.

With the development of Planning Area 5, RD 799 collected fees for the rehabilitation of the existing non-engineered levee system surrounding the specific plan area. These fees will be utilized for upgrades to the existing non-engineered levee system. The fee calculation has not been established at this time for the remaining specific plan area, but will be determined as a cooperative effort between RD 799 and the City. RD 799 will continue to maintain the levees within the specific plan area throughout development. The units within the specific plan area will pay an annual assessment to RD 799 for continued maintenance of the levee systems.

The City of Oakley will cooperate with RD 799, Contra Costa County and other pertinent agencies to update the emergency response plan for a failure of the non-engineered levee system. The updated emergency response plan will include consideration of the changes in land use and public facilities proposed for the Specific Plan Area. The emergency response plan will include a detailed levee failure analysis study to identify all areas of high risk, and select appropriate evacuation routes and staging areas accordingly. The emergency response

plan will be approved by the City, RD 799 and the County before the extension of the engineered levee system beyond the Summer Lake South project.

5.6 PUBLIC UTILITIES

5.6.1 NATURAL GAS

Several gas wells exist within the Specific Plan Area. The existing high-pressure gas mains owned by PG&E that traverse the Specific Plan Area are to remain in their existing location and condition. No modification is necessary to the mains due to development in the Specific Plan Area.

The existing 4" high pressure gas main owned by Calpine will be relocated during construction in Planning Area 1. The gas main will be located within the public street right-of-way, to be established with the filing of the tentative map for the planning area.

The existing gas wells are to remain in their current location or move as negotiated by each developer. The interconnecting pipes between each well will be relocated during construction to fit within the land plan for each development. Each developer will incorporate gas lines for public service during the design phase of each development.

5.6.2 ELECTRICITY

The overhead electrical lines that supply power to existing residents will remain. Each developer will be responsible for bringing power supply from the nearest facilities to serve the development. PG&E will evaluate necessary facilities at the design stage of each project.

The overhead power lines along East Cypress Road will be undergrounded in the future. This construction should take place during the realignment of East Cypress Road.

5.6.3 TECHNOLOGY WIRING

Residential units within the Specific Plan Area will be installed with current technology for internet access allowing residents to work and shop from home.

5.6.4 SOLID WASTE

Oakley Disposal provides solid waste collection services for the City and is anticipated to service the Specific Plan Area.

5.6.5 TELEPHONE FACILITIES

Telephone service and facilities for existing residents are provided by SBC. The residents along East Cypress Road have overhead facilities that cross the CCWD canal and Jersey Island Road. The residents along Dutch Slough, Sandmound Blvd, and Sandmound Slough have facilities that originate on Bethel Island, cross the Bethel Island Bridge, and continue south on Bethel Island Road. Summer Lake South is taking their telephone facilities from Bethel Island Road.

Each developer within the Specific Plan Area will be required to upgrade existing facilities and/or install new facilities to accommodate the new development. No regional upgrades have been determined at this time. SBC will evaluate specific requirements for each development at the design stage of each development.

5.6.6 CABLE FACILITIES

Cable facilities for this area are provided by Comcast cable through a franchise agreement with the City of Oakley. Each developer within the Specific Plan Area will be required to upgrade existing facilities to serve their respective development areas as deemed necessary by Comcast.

5.7 GRADING CONCEPT

The existing ground within the Specific Plan Area slopes gently to the northeast at an approximately 0.2% grade. The grading operation for Planning Areas 1-4 will generally consist of removal of any manure and organics remaining from dairy operations, clearing and grubbing, demolition of existing structures, and moving of surface soils to construct building pads and streets. The excavation of a lake on each property will provide surplus soils to construct the proposed levee adjacent to the project. These lakes will also be used to provide soil for the site grading where necessary.

Grading within the Specific Plan Area will provide for a balance of cut/fills for the site.

Grading plans for each tract within the Specific Plan Area will be reviewed and approved by the City of Oakley Public Work and Engineering Department prior to the issuance of grading permits. All grading plans and activities will conform to the City of Oakley grading ordinance and dust and erosion control requirements.

5.8 COMMUNITY FACILITIES AND SERVICES

5.8.1 SCHOOLS

School sites have been set aside by the developers within the Specific Plan Area. An approximately 12 acre site for an elementary school will be provided within Planning Area 1. Approximately 10 acres will be set aside within Planning Area 4 for development of an elementary school. The development plan approved for Planning Area 5 includes a 10 acre site for development of an elementary school. An approximately 20 acre site for development of a middle school to serve the Specific Plan Area is reserved within Planning Area 2. In the event a high school is required to be located within the Specific Plan Area, a 50 acre site will be made

available within Planning Area 2 in lieu of the middle school site and residential development area sufficient to accommodate the high school.

The developers of the Specific Plan Area will be required to pay school fees as required by State of California and will work with the school districts to identify the appropriate means to construct the necessary school facilities. School districts may consider community facilities district bonds or other property-based bonded indebtedness to finance school site acquisition and school facility construction.

5.8.2 FIRE PROTECTION FACILITIES

On August 30, 2002, LAFCO approved a merger of three East Contra Costa County fire districts, including East Diablo, Oakley-Knightsen, and Bethel Island. Prior to the merger, the Oakley-Knightsen Fire Protection District provided services to the City of Oakley. The new East County Fire Protection District (ECFPD) is located in the eastern portion of Contra Costa County and serves a population of approximately 82,000 residents. ECFPD covers all of Oakley and the Cypress Corridor Expansion Area, along with additional areas in the East County previously served by the East Diablo and Bethel Island Fire Protection Districts.

Fire services will be provided from a new station located within Planning Area 2. The development of the Specific Plan Area requires that an operational fire station be provided, including a signal, built to the specifications of the appropriate fire district on a minimum one-acre parcel of land. A type-one fire engine will also be provided as part of the development.

5.8.3 LIBRARY FACILITIES

The City's development impact fee program includes a main library as part of a community center facility. Development within the Specific Plan Area will contribute toward library facilities through the City's Public Facility Fee Program.

5.8.4 POLICE SERVICES

Police services will be provided by the City of Oakley. The Fire Station to be developed as part of the Specific Plan Area will include a dedicated room for City of Oakley Police service staff.

SECTION 6. DEVELOPMENT REGULATIONS

6.1 INTRODUCTION

The provisions contained herein will regulate design and development within the East Cypress Corridor Specific Plan. The regulations contained herein provide for the development of new land uses, the continued operations of existing land uses, and general landscaping regulations.

6.2 DEFINITION OF TERMS

The meaning and construction of words, phrases, titles, and terms shall be the same as provided in the City of Oakley Zoning Ordinance, Chapter 2, unless otherwise specifically provided herein.

6.3 APPLICABILITY

The development regulations contained herein provide specific standards for land use development within the East Cypress Corridor Specific Plan. Application of the following regulations is intended to encourage the most appropriate use of the land, ensure the highest quality of development, and protect the public health, safety, and general welfare. The adoption of the East Cypress Corridor Specific Plan establishes the zoning regulations for the SP-1 zoning district adopted for the Specific Plan Area, and as such, supersedes otherwise applicable City of Oakley zoning regulations unless stated herein to the contrary. Whenever the provisions and development standards contained herein conflict with those contained in the City of Oakley Zoning Ordinance, the provisions of the East Cypress Corridor Specific Plan shall take precedence. Where the Specific Plan is silent, City codes shall apply. These regulations shall reinforce specific site planning, architec-

tural design, and landscape design guidelines contained in Section 7, “Design Guidelines” of the Specific Plan.

6.4 ADMINISTRATION

6.4.1 GENERAL PLAN IMPLEMENTATION AND ZONING

The East Cypress Corridor Specific Plan will serve as the implementation tool for the General Plan as well as establish the development regulations for the SP-1 zoning district adopted for the Specific Plan Area. The East Cypress Corridor Specific Plan also establishes the general provisions, permitted uses, development standards, and design guidelines to be applied to development within the Specific Plan Area.

6.4.2 SUBSEQUENT APPROVALS REQUIRED

Approval of the East Cypress Corridor Specific Plan constitutes approval of a Preliminary Development Plan pursuant to the City of Oakley Zoning Ordinance requirements for development within the SP-1 zoning district. Following the annexation to the City of Oakley, approval of Final Development Plans and Tentative Maps are required prior to development within the East Cypress Corridor Specific Plan in accordance with the provisions of the City of Oakley Zoning Ordinance.

6.4.3 CONCEPTUAL SITE PLANS

The conceptual site plans included as Appendix A to the East Cypress Corridor Specific Plan are for illustrative purposes only. Approval of the East Cypress Corridor Specific Plan does

not constitute approval of the conceptual site plans included as Appendix A.

6.5 GENERAL SITE DEVELOPMENT CRITERIA

The following general site development criteria shall apply to all land development proposed in the Specific Plan Area.

6.5.1 GROSS ACRES

Gross acres for all development areas are measured to the centerline of streets except that the following streets have been excluded from gross acreage calculations:

- East Cypress Road
- Jersey Island Road
- Bethel Island Road

6.5.2 GRADING

Development within the Specific Plan Area shall utilize grading techniques as approved by the City of Oakley. Grading concepts shall respond to the design guidelines included in the Specific Plan.

6.5.3 BUILDING MODIFICATION

Building additions and/or alterations permitted by this Specific Plan shall match the architectural style of the primary unit and shall be constructed of the same materials and colors as the primary unit.

6.5.4 UTILITY LINES

All new public utility distribution lines of less than 66 kV shall be subsurface throughout the planned community.

6.5.5 TECHNOLOGY

All homes shall accommodate technology for computer internet access, phone, fax, and television.

6.5.6 DENSITY

The East Cypress Corridor Land Use Plan allocates a total number of residential dwelling units to each planning area as indicated in Table 1, “Overall Project Summary.” Variations in the number and type of residential dwelling units within each Planning Area may occur at the time of final design of the Planning Area provided the total number of dwelling units assigned to the Planning Area pursuant to the East Cypress Corridor Specific Plan is not exceeded, except as provided for in Section 6.5.7 below.

6.5.7 MAXIMUM NUMBER OF DWELLING UNITS

The maximum number of residential dwelling units permitted within the East Cypress Corridor Specific Plan shall be 5,759 dwelling units consistent with the Oakley General Plan. The maximum number of dwelling units within each Planning Area of the East Cypress Corridor Specific Plan is established in Table 1, “Overall Project Summary.” The maximum number of dwelling units may be exceeded pursuant to the provisions of Section A.5, “Affordable Housing Overlay District,” Chapter 4, Section A.6, “Residential Density Bonus and Incentives” and Chapter 5, Section A, “Second Dwelling Units,” of the City of Oakley Zoning Ordinance.

6.5.8 SCHOOLS AND PUBLIC FACILITIES

Schools and public facilities, including but not limited to, police sub-stations, fire sub-stations, libraries, community centers, and day care centers are permitted within any Planning Area.

6.6 RESIDENTIAL DETACHED DWELLING UNITS DEVELOPMENT STANDARDS: PLANNING AREAS 1-4 AND 6

6.6.1 GENERAL

This category includes the development of new single family detached dwelling units within the Specific Plan Area. The purpose of the residential standards for single family detached housing is to establish the minimum development criteria for the residential types within each Planning Area.

6.6.2 PERMITTED USES

6.6.2.1 Agriculture Limited (AL) Land Use Areas

Permitted uses within the Specific Plan Area designated for AL land use shall be those uses listed as permitted in the City of Oakley Zoning Ordinance, Chapter 4, Section A-1 Limited Agricultural District (AL).

6.6.2.2 Single Family Residential Land Use Areas

Permitted uses within the Specific Plan Area designated for Single Family Residential Low (SL), Single Family Residential Medium (SM), and Single Family Residential High (SH) shall be those uses listed as permitted in the City of Oakley Zoning Ordinance, Chapter 4, Section A-2 Single Family Residential Districts (R-6; R-7; R-10; R-12; R-20; R-40).

6.6.2.3 Second Units

Second units are permitted within all single family residential land use areas pursuant to the provisions of the City of Oakley Zoning Ordinance related to the development of second units, Chapter 5, Section A.

6.6.3 CONDITIONALLY PERMITTED USES

6.6.3.1 Agriculture Limited (AL) Land Use Areas

Conditionally permitted uses within areas of the Specific Plan Area designated for Agriculture Limited (AL) land use shall be those uses listed as conditionally permitted in the City of Oakley Zoning Ordinance, Chapter 4, Section A-1 Limited Agricultural District (AL).

6.6.3.2 Single Family Residential Land Use Areas

Conditionally permitted uses within the Specific Plan Area designated for Single Family Residential Low (SL), Single Family Residential Medium (SM), and Single Family Residential High (SH) shall be those uses listed as conditionally permitted in the City of Oakley Zoning Ordinance, Chapter 4, Section A-2 Single Family Residential Districts (R-6; R-7; R-10; R-12; R-20; R-40).

6.6.4 DEVELOPMENT STANDARDS

Development standards for single family detached residential land use are established for each residential land use in Table 2, "Residential Site Development Standards - Single Family Detached."

6.7 RESIDENTIAL ATTACHED DWELLING UNITS DEVELOPMENT STANDARDS: PLANNING AREAS 1-4 AND 6

6.7.1 GENERAL

This category includes the development of new multi-family attached dwelling units within the Specific Plan Area. The purpose of the residential standards for multi-family attached housing is to establish the minimum criteria for devel-

opment within the residential types within each Planning Area.

6.7.2 PERMITTED USES

6.7.2.1 Multi-Family Low (ML) and Multi-Family Medium (MM) Land Use Areas

Permitted uses within the Specific Plan Area designated for ML and MM shall be those uses listed as permitted in the City of Oakley Zoning Ordinance, Chapter 4, Section A-3 Multiple Family Residential Districts (M-9; M-12; M-17).

6.7.3 CONDITIONALLY PERMITTED USES

6.7.3.1 Multi-Family Low (ML) and Multi-Family Medium (MM) Land Use Areas

Conditionally permitted uses within the Specific Plan Area designated for ML and MM shall be those uses listed as conditionally permitted in the City of Oakley Zoning Ordinance, Chapter 4, Section A-3 Multiple Family Residential Districts (M-9; M-12; M-17).

6.7.4 DEVELOPMENT STANDARDS

Development standards for multi-family attached residential land use are established in Table 3, “Residential Site Development Standards - Multi-Family Attached.”

6.8 PARKING

Parking shall be provided for each residential land use in accordance with the parking requirements established in Chapter 8 of the City of Oakley Zoning Ordinance related to parking.

6.9 DEVELOPMENT STANDARDS PLANNING AREA 5

The development standards for Planning Area 5 shall be those adopted by the Contra Costa County Board of Supervisors on April 23, 2001 approving Development Plan 3032-90 and Rezoning 2918-RZ and as subsequently amended by the City of Oakley with the East Cypress Corridor Specific Plan and any future development application.

6.10 COMMERCIAL DEVELOPMENT STANDARDS PLANNING AREAS 1, 2 AND 6

6.10.1 GENERAL

This category includes the development of new commercial land uses within Planning Areas 1, 2 and 6. The purpose of the standards for commercial land uses is to establish the minimum criteria for development of community serving commercial uses planned for the Village Center in Planning Area 1, for commercial uses in Planning Area 2, and for the commercial land use area designated within Planning Area 6. All development shall follow the City’s Commercial Design Guidelines.

6.10.2 PERMITTED USES

Permitted uses within the land use area designated for Village Center (VC) and Commercial (CO) shall be those uses listed as permitted in the City of Oakley Zoning Ordinance, Chapter 4, Section 4-B-2.2 Retail Business District (RB).

A minimum of 20 acres shall be reserved for development of commercial uses within the Village Center site. Development of up to 150 residential dwelling units is permitted within the balance of the Village Center site, in the event the entire Village Center site is not leased or sold to one or more commercial entities

Table 2
Single Family Detached

Land Use	LOT STANDARDS				MINIMUM BUILDING SETBACKS						Maximum Height ^{8,9}	ACCESSORY STRUCTURES				
	Size (S.F.)	Min. Size (S.F.)	Min. Width	Min. Depth	FRONT ^{1,2,3}	SIDE	CORNER	REAR ⁶		Building Size (S.F.)		MINIMUM BUILDING SETBACKS			Maximum Height	
					Min.	Min./Total	Min.	Building Min.	Garage Min.			Front Min.	Side Min.	Rear Min.		
AL	43,000 or greater	43,000	140'	140'	30'	20'/40'	25'	15'	15'	30'	1,500 ¹⁰	65'	20'	15'	15'	
SV	40,000-64,999	40,000	140'	140'	30'	20'/40'	25'	15'	15'	30'	1,500 ¹⁰	65'	20'	15'	15'	
SL	20,000-39,999	20,000	120'	120'	30'	15'/35'	25'	15'	15'	30'	600	65'	15'	15'	15'	
SL	15,000- 19,999	15,000	100'	100'	25'	10'/25'	20'	15'	10'	30'	400	65'	15'	15'	15'	
SM	12,000- 14,999	12,000	100'	100'	25'	10'/25'	20'	15'	10'	30'	300	65'	10'	10'	12'	
SM	10,000- 11,999	10,000	80'	100'	25'	10'/20'	20'	15'	10'	30'	300	50'	10'	10'	12'	
SM	8,000- 9,999	8,000	70'	90'	25'	5'/15'	20'	15'	5'	30'	200	50'	10'	10'	12'	
SH	6,000- 7,999	6,000	60'	90'	25'	5'/15'	20'	15'	5'	30'	200	50'	5'	5'	8'	
SH	6,000- 6,999	6,000	55'	90'	25'	5'/15'	20'	15'	5'	30'	200	50'	5'	5'	8'	
ML	5,000- 5,999	5,000	50'	90'	20'	5'/10'	15'	15'/7' ⁷	5' ⁷	30'	120	50'	3'	3'	8'	
ML	4,000- 4,999	4,000	40'	90'	20'	0'/10' ⁴	15'	15'/7' ⁷	5' ⁷	35'	80	50'	3'	3'	8'	
ML	2,800- 3,999	2,800	35'	80'	20'	4'/8'	15'	7' ⁷	5' ⁷	35'	N/A	N/A	N/A	N/A	N/A	
MM	2,100-2,799	2,100	30'	45'	15'	4'/8' ⁵	15'	7' ⁷	5' ⁷	35'	N/A	N/A	N/A	N/A	N/A	

- NOTES
- Minimum front setbacks for swing-in garages shall be equal to 5 feet less than the minimum front setback.
 - One-story architectural features such as front porches, entries, and bay windows may encroach a maximum of 5 feet into the front setback, or side yard adjacent to a street.
 - Front facing garages shall be a minimum of 18 feet to the property line.
 - Minimum side yard setbacks for garages on 4,000-4,999 s.f. lots shall be 0 feet, with a 10-foot minimum separation between garage and buildings on the adjacent lot.
 - Minimum side setbacks for buildings located on the perimeter of a cluster that does not face a street shall be 10 feet.
 - Minimum rear and side setbacks for detached second units over street-accessed detached garages shall be the same as garage setbacks but not less than 10 feet for rear and 5 feet for side setbacks.
 - Where garage access is from rear alleys, minimum setbacks shall be 7 feet to building and 5 feet to garage as measured from the alley. Alleys shall have a minimum width of 20 feet.
 - Maximum building height shall not exceed 35 feet or 2.5 stories.
 - Maximum building height shall not exceed 30 feet or 2 stories.
 - Accessory buildings may exceed 1,500 s.f. by the same percentage that the square footage of the lot exceeds 40,000 s.f. Maximum size for an accessory building on a lot shall be 3,000 s.f.

Table 3
Multi-Family Attached⁵

Land Use	LOT STANDARDS				MINIMUM BUILDING SETBACKS				MINIMUM BUILDING STANDARDS				
	Density Range	Min. Lot Area (S.F.)	Min. Width	Min. Depth	FRONT	SIDE ¹	REAR		BUILDING SEPARATIONS			Max. Height	Max. Site Coverage ⁴
					Min.	Aggregate	Building Min.	Garage Min.	Front to Front	Side to Side	Bldg. to Garage		
ML	6-9 DU/AC	6,000	NA	NA	25'	20'	20'	10'	30'	20'	10'	35' ²	30%
MM	9-12 DU/AC	8,000	NA	NA	25'	20'	20'	10'	30'	20'	10'	35' ³	35%
MH	12-17 DU/AC	10,000	NA	NA	25'	20'	20'	10'	35'	25'	10'	40' ³	40%
MH	17-21 DU/AC	10,000	NA	NA	25'	20'	20'	10'	35'	25'	10'	40' ³	40%

- NOTES
- Side yard setbacks along public streets shall be a minimum 15 feet.
 - Maximum building height shall not exceed 35 feet or 2.5 stories.
 - Maximum building height shall not exceed 40 feet or 3 stories.
 - Maximum site coverage is calculated using the gross acreage of the lot.
 - Development standards may be modified pursuant to City of Oakley Zoning Code, Chapter 4, Section A.5, "Affordable Housing Overlay District," and Chapter 4, Section A.6, "Residential Density Bonus and Incentives."

Tables 2 and 3
Residential Site Development Standards

within seven years of the annexation of Planning Areas 1-5 or by the time of issuance of the 3,000th building permit for Planning Areas 1-5, whichever comes later.

6.10.3 CONDITIONALLY PERMITTED USES

Conditionally permitted uses within the Village Commercial (VC) and Commercial (CO) land use areas shall be those uses listed as conditionally permitted in the City of Oakley Zoning Ordinance, Chapter 4, Sections B-2.3 and B-3.3.

6.10.4 PARKING

Parking shall be provided for each commercial land use in accordance with the parking requirements established in the City of Oakley Zoning Ordinance, Chapter 8, related to parking.

6.10.5 VILLAGE COMMERCIAL (VC) DEVELOPMENT STANDARDS

The development standards for commercial uses in the Village Center of the East Cypress Corridor Specific Plan Area are described in Table 4, “Commercial Site Development Standards.”

6.10.6 COMMERCIAL (CO) DEVELOPMENT STANDARDS

Development of Commercial (CO) land uses within the East Cypress Corridor Specific Plan Area shall be governed by the provisions of the City of Oakley Zoning Ordinance, Chapter 4, Sections B-2.4 through B-2.7, “Retail Business District” (RB).

6.11 COMMERCIAL RECREATION DEVELOPMENT STANDARDS

6.11.1 GENERAL

Development of Commercial Recreation land uses within the East Cypress Corridor Specific Plan shall be governed by the provisions of the City of Oakley Zoning Ordinance, Chapter 4, Sections B-6 and B-7.

6.12 LIGHT INDUSTRIAL DEVELOPMENT STANDARDS

6.12.1 GENERAL

Development of Light Industrial land uses within the East Cypress Corridor Specific Plan shall be governed by the provisions of Chapter 4, Section C-1 of the City of Oakley Zoning Ordinance.

6.13 NON-CONFORMING STANDARDS

Existing uses determined to be non-conforming with the standards of the East Cypress Corridor Specific Plan shall be determined to be legal non-conforming and may be continued provided the legal non-conforming use is not expanded, intensified, reinstated, altered, restored after destruction, and/or reestablished after abandonment as defined in the City of Oakley Zoning Ordinance. Standards regulating legal non-conforming uses and structures within the Specific Plan Area shall be the standards for non-conforming uses and structures as established within the City of Oakley Zoning Ordinance, Chapter 9.

SECTION 6. DEVELOPMENT REGULATIONS

	VILLAGE CENTER
MINIMUM SITE AREA	40 acres
MAXIMUM DEVELOPMENT FLOOR AREA RATIO	0.50 ⁶
MAXIMUM LOT COVERAGE	50%
MINIMUM BUILDING SETBACKS ^{1, 2}	
From Public Street Property Line	20 ft.
From Internal Street Property Line	0 ft.
From Interior Property Line	0 ft.
PARKING AND DRIVE AISLE SETBACKS	
From Public Street Property Line	15 ft..
From Internal Street Property Line	10 ft.
From Interior Property Line ³	5 ft.
From Rear Property Line	5 ft.
MINIMUM SITE LANDSCAPING	15%
PARKING LOT LANDSCAPING	Minimum 1 tree per 6 parking stalls
MAXIMUM BUILDING HEIGHT	
Main Structure	45 ft.
Architectural Projections and Focal Elements such as Towers, Cupolas and other Appurtenances ⁴	55 ft.
Porte-Cocheres ⁵	1 story
LOADING AREAS	See "Screening" below
BUFFER TREATMENT ADJACENT TO RESIDENTIAL	Minimum building setback of 25 ft. Minimum landscape setback width of 10 ft. Masonry wall at property line, minimum height 8 ft.
MAXIMUM HEIGHT OF WALLS, FENCES AND HEDGES	8 ft. adjacent to residential, 6 ft. at other side or rear property lines behind front setback. 36 inches within front setback area.
PARKING	Per Chapter 8, City of Oakley Zoning Ordinance
SCREENING	All loading areas shall be screened from adjacent public streets, residential and open space uses through the use of landscaping, earthen berms, or decorative walls or fencing. All storage, including cartons, containers, materials or trash shall be shielded from view within a building or area enclosed by a solid fence or wall not less than six feet in height. Trash areas shall be enclosed and covered. All roof-mounted equipment, including but not limited to, mechanical equipment, satellite dishes, tanks, ducts and towers, and all equipment appurtenant thereto, shall be screened on all sides from public view from the street, adjoining properties, and neighboring residential units, by a parapet wall, decorative enclosure, or other architectural element. Equipment screening shall appear as an integral part of the building architecture.
LIGHTING	All interior and exterior lighting shall be directed away from residential and open space uses.

- Notes:
1. All setback areas shall be landscaped. Exceptions may be allowed for pedestrian plazas, storefront uses or arcades, subject to Planning Director approval.
 2. All setbacks are measured to habitable area, not architectural appurtenance or projection. An architectural projection is defined as an element that articulates the building elevation such as eaves, window and door pop-out surrounds, bay windows, pot shelves, chimneys, enhanced window sills, shutter details, window trim, and balconies and other similar elements. Such elements may project a maximum of 3 ft. into setback areas.
 3. Except where there are shared driveways or circulation between adjacent properties.
 4. Architectural element only not to be used for signage, subject to Planning Director approval.
 5. Porte-cocheres shall be open on three sides.
 6. Maximum allowable square footage shall not exceed 435,600 s.f.
 7. Up to 150 units may replace approximately 20 acres of commercial (217,800 S.F.)

Table 4
Commercial Site Development Standards
Village Center (VC)

6.14 LANDSCAPE STANDARDS

6.14.1 IDENTITY AND MONUMENTATION

All landscape plans, streetscape plans and graphic designs with regard to the identity of the East Cypress Corridor Specific Plan, neighborhood identity or entry monumentation shall conform to the design guidelines and regulations as set forth herein and shall be subject to review and approval by the City of Oakley.

6.14.2 LANDSCAPING

Landscaping within the East Cypress Corridor Specific Plan shall be provided in accordance with the Design Guidelines for Streetscapes and Entries and shall utilize plant materials specified on the Plant Palette included in Section 7.

6.14.3 LANDSCAPING IN PUBLIC RIGHTS-OF-WAY

Landscaping and irrigation systems, within the public rights-of-way of the East Cypress Corridor Specific Plan Area, shall be installed by the developer.

6.14.4 WALLS AND FENCES

Walls and fences shall be constructed in locations and of a design consistent with the “Walls and Fences Diagram,” Exhibit 90, as described in Section 7, “Design Guidelines.” Such walls and fences will be constructed concurrently with the neighborhoods of the Specific Plan.

6.14.4.1 Perimeter Walls and Fencing

All perimeter wall and fence materials throughout the East Cypress Corridor Specific Plan will be of uniform manufacture with colors specified for the overall design theme.

Free-standing perimeter walls and view fencing shall be provided within and at the perimeter of the Specific Plan Area as specified in “Walls and Fences Diagram,” Exhibit 90, Section 7, “Design Guidelines,” and shall not exceed six feet in height from finished grade. Perimeter walls may exceed six feet in height if required for sound attenuation as approved by the City of Oakley subject to the findings of an acoustical study. Perimeter walls shall be constructed of either masonry or other permanent, durable, low maintenance material. View fencing may be of a decorative wrought iron or other durable material approved by the City.

6.14.4.2 Side and Rear Walls and Fences

Residential side and rear yard walls and fencing shall not exceed six feet in height from highest adjacent finished grade unless required to exceed six feet in height pursuant to the recommendations of an acoustical study and approved by the City. Walls and fencing within the required residential front yard setback area shall not exceed three feet in height. Side yard and rear yard “good neighbor” fencing shall be constructed of wood with tubular steel posts and will be subject to approval by the City. Metal view fencing may be used at rear yards adjacent to lakes, open space, or levees.

6.14.4.3 Water Tank and Gas Well Screening Walls and Fences

All screening wall materials will be of uniform manufacture with colors specified for the overall design theme. Screening walls and fences shall be provided as specified in “Screening and Buffer Diagram,” Exhibit 85 in Section 7, and shall not exceed six feet in height measured from the interior finished grade unless required to exceed six feet pursuant to the recommendations of an acoustical study and approved by the City.

6.15 SIGNAGE

A Master Sign Program shall be submitted by the developer(s) of Planning Areas 1-4 as part of the application for a final development plan for any phase of development and approved by the City of Oakley as part of the approval of final development plans pursuant to the City's Zoning Ordinance. The Master Sign Program shall address community identity, Village Center monument signage, neighborhood entry and identification signs, and way finding signs within the Specific Plan Area. All other signs shall be subject to the approval of a sign permit pursuant to the City's Zoning Ordinance.

6.15.1 MASTER SIGN PROGRAM CONTENTS

All sign programs shall address, at a minimum, the following:

- Permitted signs.
- Prohibited signs.
- The hierarchy of signage.
- Definition of types of signs.
- Locations and dimensions for monument signs, neighborhood identification signs, and public facilities signs.
- Locations and dimensions of directional signage.
- Provisions for size, location, and duration of display of temporary signs.
- Permitted sign types, styles, construction materials, colors, and lettering styles.
- Requirements for a sign permit application.
- Procedures for obtaining approval of a sign permit.
- Procedures for amendments to the sign program.

6.16 LIGHTING

6.16.1 GLARE

No glare incidental to any use shall be visible beyond any boundary line of the parcel.

6.16.2 STREET LIGHTING

Street lighting within the Specific Plan Area will require lighting levels and uniformity ratios meeting the City of Oakley specifications. The minimum height of street lights shall be eighteen (18) feet.

6.16.3 WALKWAY LIGHTING

Low "pedestrian" level lighting shall be installed at selected intersections along walkways, trails, and recreation/open space areas.

- Bollard lighting and other pedestrian lighting is recommended in high pedestrian travel areas such as high density residential development, parks, schools, commercial centers and trails.
- Fixtures shall be uniform and unobtrusive.
- Shielded fixtures are required to prevent up-lighting and to shield lighting source from adjacent residential areas.

6.16.4 ALLEY LIGHTING

Alleys shall be well lit at nighttime. Alley lighting fixtures shall be on sensors for automatic nighttime lighting.

6.17 GAS WELLS

All new gas well sites shall be developed in accordance with the City of Oakley Gas Drilling Ordinance.

SECTION 7. DESIGN GUIDELINES

The East Cypress Corridor Specific Plan Design Guidelines have been prepared to provide the framework for high quality design within the project site and to consider project wide as well as site specific issues. The guidelines express the desired character of future development and address site planning, architecture, and landscaping. The East Cypress Corridor Specific Plan is designed to be an integrated planned community. Common development features such as signage, landscape and hardscape, walls and fences, and architecture are controlled throughout the development to provide unity and express a strong collective design statement.

The Design Guidelines establish development criteria to ensure a unified environment within the master plan. This document is not intended to limit innovative design, but rather to provide clear direction and design criteria. The site planning and architecture guidelines focus on multi-family, small lot (less than 6,000 SF), alley and cluster product types which are not currently referenced in the “Residential Design Guidelines” (Dec. 2003). Whenever the guidelines and standards contained herein conflict with City of Oakley zoning regulations or the “Residential Design Guidelines,” the provisions of the East Cypress Corridor Specific Plan shall take precedence. Where these guidelines are silent, the City of Oakley’s adopted Design Guidelines shall apply.

The goals of the East Cypress Corridor Specific Plan Design Guidelines are as follows:

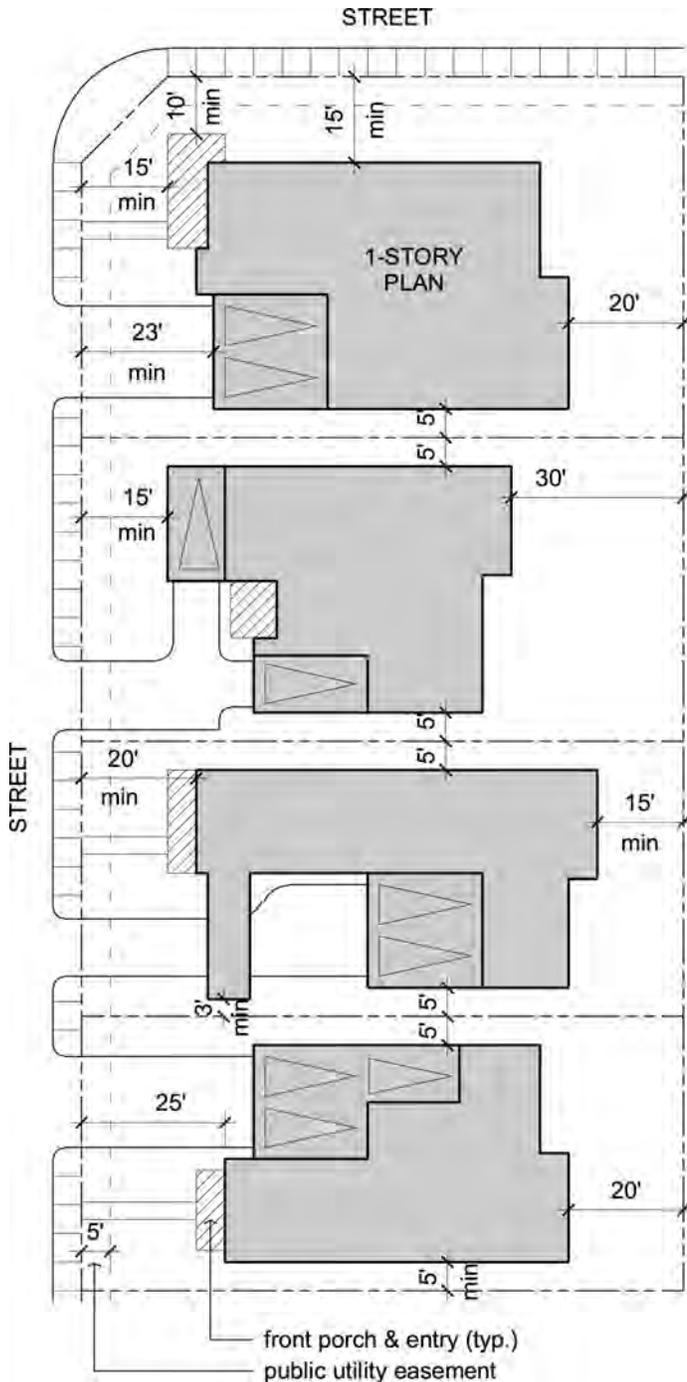
- To provide the City of Oakley with the necessary assurances that development within the master planned community will attain the desired level of high quality design;
- To serve as design criteria for use by planners, architects, landscape architects, engineers, builders and future property owners;
- To provide guidance to City Staff, the Planning Commission and the City Council when reviewing future development within the Specific Plan area;
- To provide a viable framework of expectations and clear direction without limiting the creativity of the designer;
- To ensure consistency between the City’s adopted Design Guidelines and those of this Specific Plan.

7.1 SITE PLANNING AND ARCHITECTURE

7.1.1 SMALL LOT DESIGN GUIDELINES

7.1.1.1 Introduction

While the predominant lot size found in the East Cypress Corridor Specific Plan area is over 6,000 square feet, providing small lot detached housing as a product alternative with overall lot size of less than 6,000 square feet, creates a broader spectrum of high quality housing opportunities at more affordable levels. Small lot detached housing generally falls into one of three types depending upon whether the garage is accessed from the street (front accessed), an alley (rear accessed), or a motor court (cluster). Motor courts are enclosed areas predominantly used for garage access that are frequently enhanced with accent paving and landscape features. Cluster housing with garages off of motor courts should have their formal front doors accessed by way of greenways between clusters (greenway clusters). This type of housing allows for higher densities and smaller private spaces



Typical 5,000-5,999 SF Setbacks (front accessed)

but offers increased public open space and active recreation facilities. With appropriate street setbacks, clusters can have streetscapes similar to traditional single family detached neighborhoods.

The challenge of new development on the one hand is to create a statement of uniqueness and identity while also contributing and blending with the existing context. Guidelines will therefore be flexible and open to interpretation and intended to foster fresh and creative solutions. Standards will be requirements that must be followed to ensure the smooth integration of new development within the existing City fabric and way of life. The overall goals and purposes already described within the City of Oakley's "Residential Design Guidelines" will therefore apply to these guidelines.

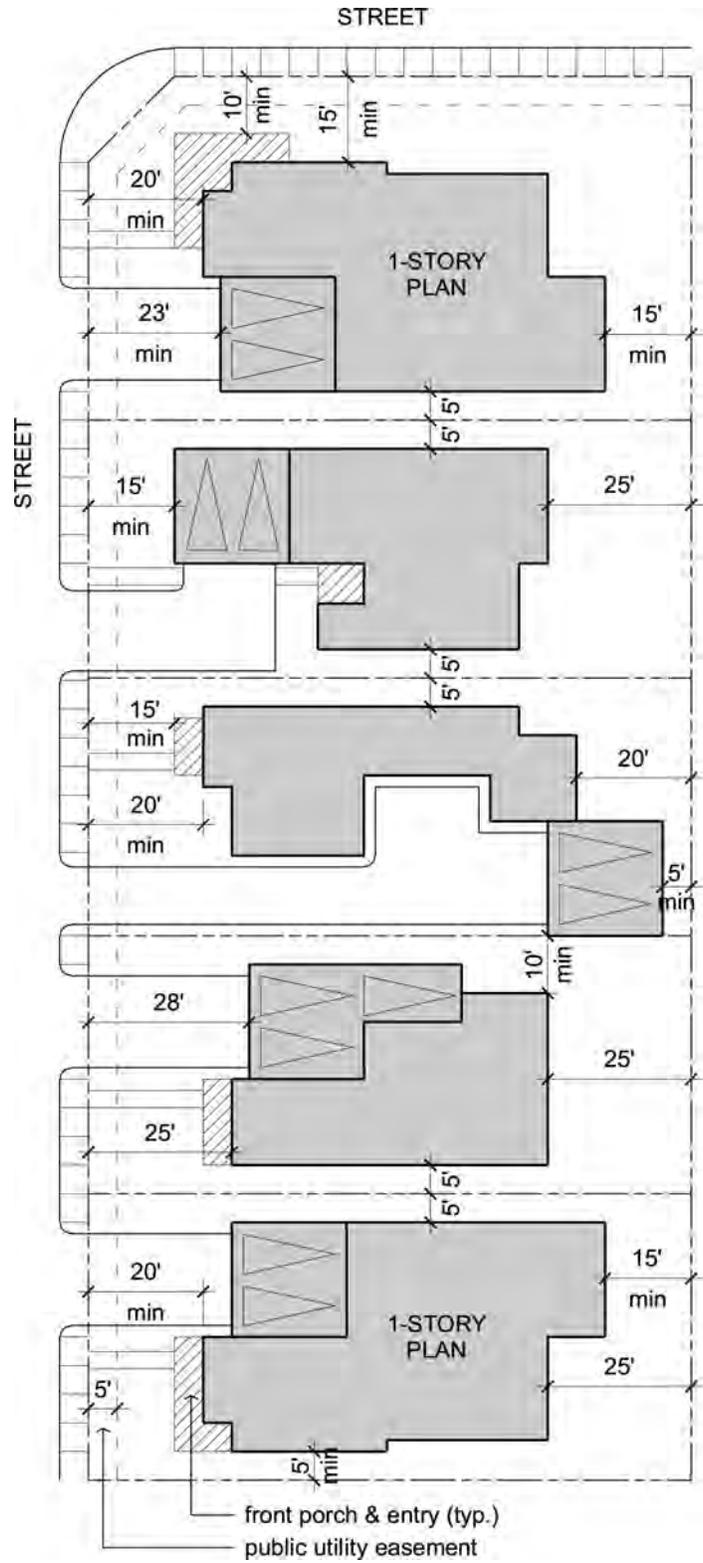
7.1.2 SMALL LOT SITE PLANNING

As with larger lot residential, the important goal is still "to create a functional residential streetscape that provides variety in appearance as well as individuality for each home." However, some adjustments such as tighter setbacks will have to be made to accommodate the smaller lots. Location of the garage along with the size of building setbacks and separations will strongly effect the function and feel of a small lot neighborhood.

7.1.2.1 Site Planning for Front Accessed Housing

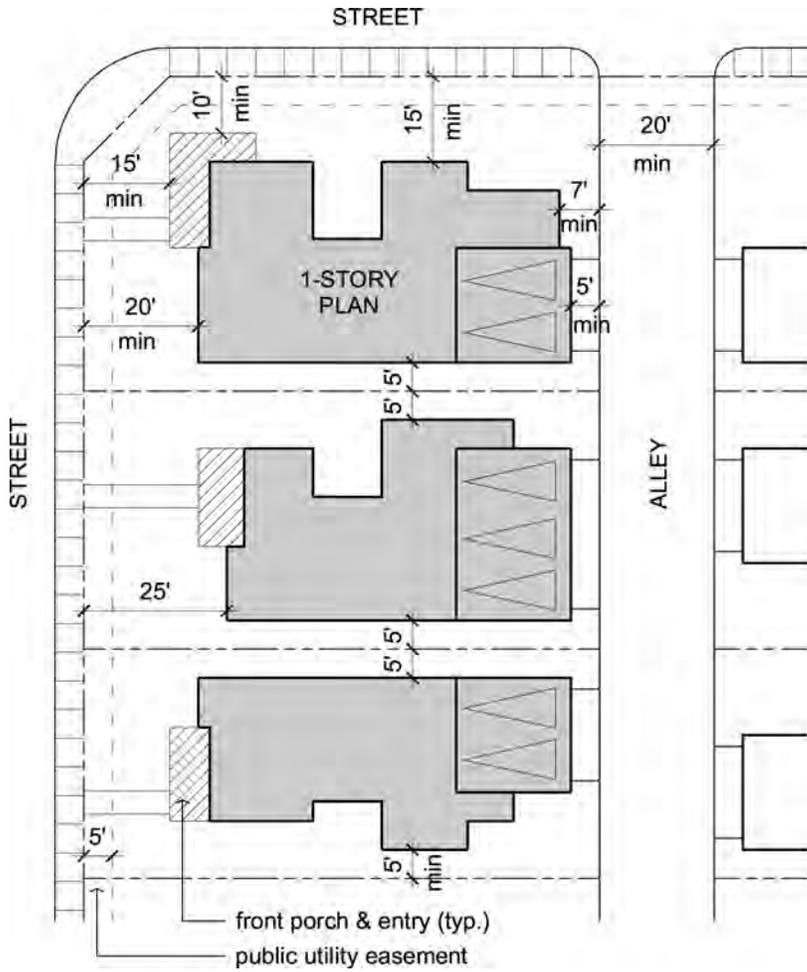
1. Front yard setbacks to living space will vary with lot size but shall be a minimum of 20 feet.
 - Front yard setbacks to any part of the structure shall be a minimum of 15 feet.
 - Setbacks shall be measured from foundation of the structure to the property line or adjacent foundation.

- The first 5' of setback area represents a P.U.E. where only landscaping (or fencing) may be located.
 - One-story features such as front porches and entries, but not including living space, may encroach into front setbacks a maximum of 5 feet.
 - For each group of three adjacent homes, at least one should contain a front yard setback which varies from its neighbors by a minimum of 5 feet. Minimum setbacks may not be reduced to accommodate the variation.
 - Rear garages may have a rear setback of 5 feet.
2. Rear setbacks shall be a minimum of 15 feet.
 - Vary rear setbacks by at least 5 feet where visible from major roadways or public areas. Minimum setbacks may not be reduced to accommodate the variation.
 3. Side yard setbacks shall be a minimum of 5 feet. Total aggregate side yard will vary depending on lot size.
 - The exterior side yard setback for corner lots shall be a minimum of 15 feet.
 - One-story features such as front porches and entries, but not including living space, may encroach into exterior side yard setbacks a maximum of 5 feet.
 - Offset windows for privacy along adjacent side yards.
 - In no case, shall any side yard setback be less than 5 feet, including any building elements such as entertainment systems and fireplaces. No encroachments shall be allowed into the 5-foot setback.
 - Rear garages may have a side yard setback of zero feet as long as no adjacent structure is closer than 10 feet. Second



Typical 4,000-4,999 SF Setbacks (front accessed)

SECTION 7. DESIGN GUIDELINES



Typical 4,000-5,999 SF Setbacks (alley accessed)

story living space over the garage must maintain the minimum required side and rear yard setbacks.

- Air conditioning equipment shall not be allowed into the minimum side yard setback.
4. Lots backing onto loop and local streets should be kept to a minimum.
 - Place open ended cul-de-sacs and single loaded streets adjacent to trails and open space areas where possible in order to avoid walls along project streets.



Lots Less than 45 Feet Wide Should be Rear Accessed

- Use wider lots for single story plans to create varied front and rear elevations.

7.1.2.2 Site Planning for Rear Accessed Housing

1. Front yard setbacks to living space shall be a minimum of 20 feet.
 - Front yard setbacks to any part of the structure shall be a minimum of 15 feet. One story porches and entries may encroach a maximum of 5 feet into the front setback.
 - Setbacks shall be measured from foundation of the structure to the property line or adjacent foundation.
 - For each group of three adjacent homes, at least one should contain a front yard setback which varies from its neighbors by a minimum of 5 feet. Minimum setbacks may not be reduced to accommodate the variation.
 - Wrap around porches are encouraged on corner lots. Ensure that porches are large enough to be functional.
2. Rear setbacks shall be a minimum of 7 feet to living space and 5 feet to garage as measured from the alley edge.

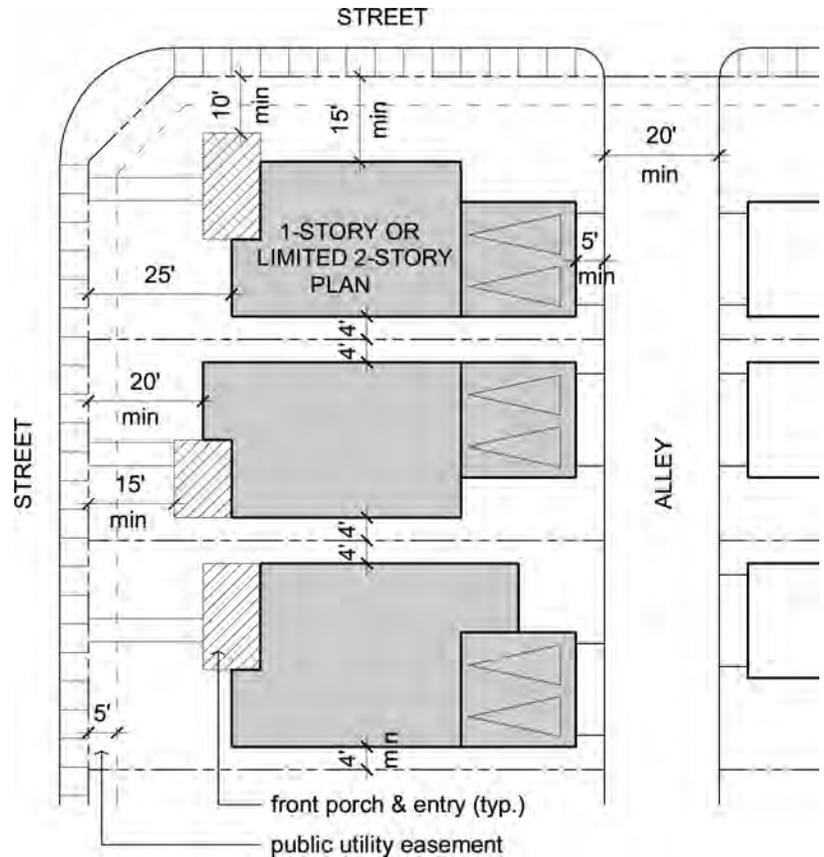
- Single loaded alleys should not be visible along public streets.
 - Vary second story setbacks over garages by at least 5 feet in order to avoid a tunnel effect when viewing down the alley.
3. Side yard setbacks will vary with lot size.
- The exterior side yard setback for corner lots shall be a minimum of 15 feet. However, one story porches and entries may encroach a maximum of 5 feet into this setback.
 - Offset windows for privacy.
 - In no case, shall any side yard setback be less than 5 feet, including any building elements such as entertainment systems and fireplaces. No encroachments shall be allowed into the 5-foot setback.
 - Air conditioning equipment shall not be allowed into the minimum required side yard setbacks.

7.1.2.3 Site Planning for Greenway Clusters

1. Front yard setbacks to living space shall be a minimum of 15 feet.
 - Front setbacks to any part of the structure shall be a minimum of 10 feet. One story porches and entries may encroach a maximum of 5 feet into the front setback.



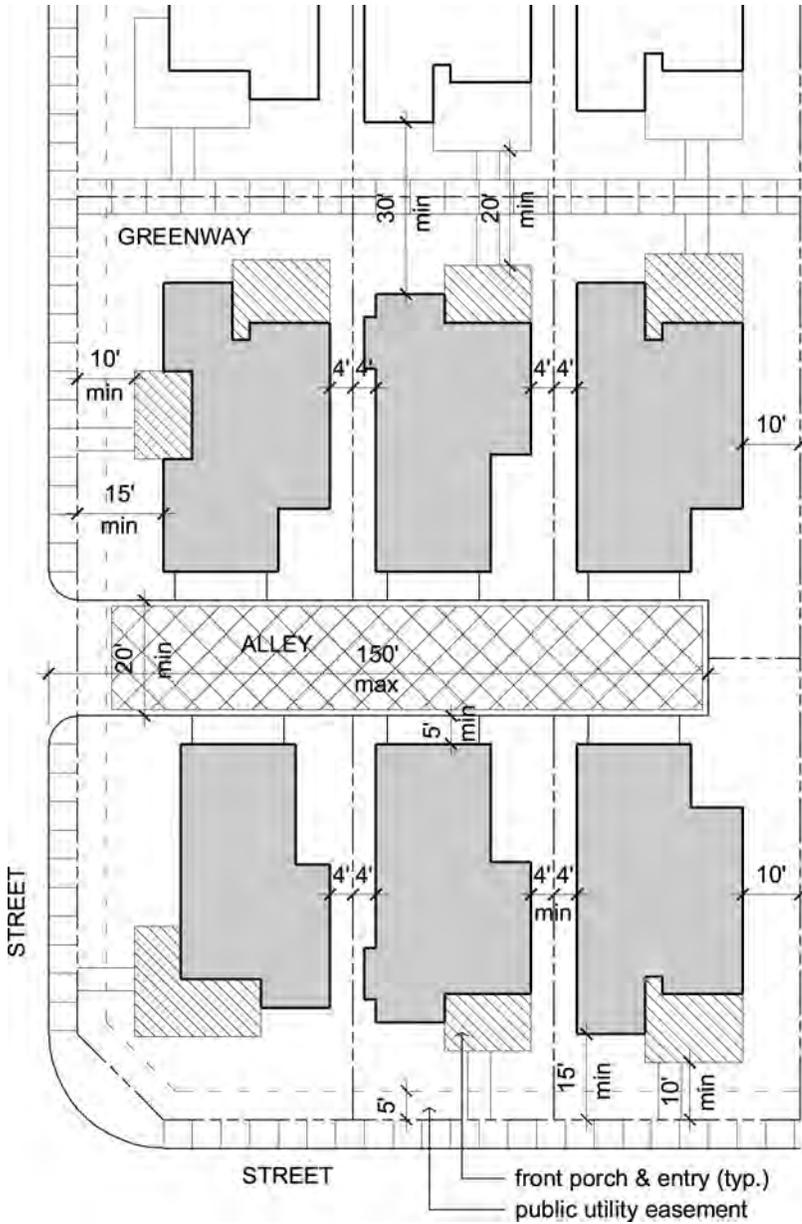
Paths along Greenways Lead to Clustered Homes



Typical 2,800-3,999 SF Setbacks (alley accessed)

- Setbacks shall be measured from foundation of the structure to the property line of adjacent foundation.
 - For each group of three adjacent homes, at least one should contain a front setback which varies from its neighbors by a minimum of 3 feet. Minimum setbacks may not be reduced to accommodate the variation.
 - Front to front building separations between greenway clusters shall be a minimum of 30 feet, as measured from living space.
2. Rear setbacks shall be a minimum of 7 feet to living space and 5 feet to garage as measured from the alley.

SECTION 7. DESIGN GUIDELINES



Typical Greenway Clusters

- Single loaded alleys should not be visible along public streets.
 - Vary second story setbacks over garages by at least 5' to avoid a tunnel effect when viewing down the alley.
3. Side yard setbacks shall be a minimum of 4 feet.
 - The exterior side yard setback of corner lots shall be a minimum of 15 feet. However, one-story architectural elements such as porches and entries may encroach a maximum of 5 feet into this setback.
 - Offset windows for privacy along adjacent side yards.
 - In no case, shall any side yard setback be less than 5 feet, including any building elements such as entertainment systems and fireplaces. No encroachments shall be allowed into the 5-foot setback.
 - Air conditioning equipment shall not be allowed into the minimum required side yard setback.
 - Side to side building separations between clusters shall be a minimum of 15 feet.
 4. Alley lengths for clusters shall be a maximum of 150 feet as measured from an adjacent street curb or alley.

7.1.3 SMALL LOT BUILDING DESIGN

The architecture of small lot housing must strike a difficult balance between the appearance and relationship of the house to the street with the requirements of natural lighting, interior circulation and room proportions. The garage frontage becomes more dominant as the lot width decreases. Lots less than 45 feet in width will normally access the garage by way of an alley located in the rear. Removing the garage from the street allows the residential elevation to fill the entire width of the lot. This can give a small lot home nearly as much street presence (relatively speaking) as a larger home with the garage accessed from the street.

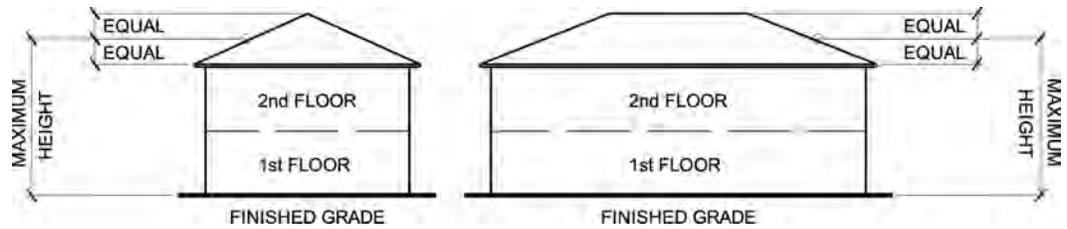
7.1.3.1 Building Design for Front Accessed Housing

1. At least 20% of the homes within a subdivision shall be single stories.
 - Create diversity by varying the bulk and mass of buildings.
 - Combine single and two-story building elements and forms.
2. At least 50% of the corner lots in a subdivision shall be one-story homes.
 - Single story or stepped, two-story homes are required on corner lots
 - One-story porches help to reduce a two-story elevation on the corner to a more pedestrian scale.
3. Design elevations to be viewed from multiple directions.
 - Avoid large facades with no windows or surface articulation.
 - Provide interesting rear building elevations especially when visible from major roadways or public spaces.
4. The building plan should highlight the entry providing visibility and identity from the street.
 - Front porches may encroach a maximum of 5 feet into the front or side (on a corner lot) setbacks.
 - Integrate features such as columns, porches and roof overhangs to create a protected entry area.
 - Wrap around porches are encouraged on corner lots.
 - Porches must have a minimum depth of 6'.
5. Roof forms should be appropriate to the architectural style.
 - Avoid monotonous and repetitive forms.
 - Flat roofs are not allowed, unless an exception is made for a specific architectural style.
6. For homes on lots under 5,000 SF, maximum building height shall not exceed 35 feet or 2 1/2 stories. Maximum height for homes on lots 5,000 SF or larger shall be 30 feet or 2 stories.



Use Wrap-around Porches on Corner Lots

SECTION 7. DESIGN GUIDELINES



Building Height Diagram

- Building height shall be measured from finish grade to the midpoint of the primary roof.
7. Color and material requirements shall conform to those listed in the “Residential Design Guidelines.”
 - Materials and colors should be compatible with adjacent uses.
 - A consistent material treatment should wrap around all sides of a building.
 8. Architectural style requirements shall conform to those listed in the “Residential Design Guidelines.”
 - The architectural character of new residences in the City of Oakley should be appropriate to the region.
 - Design individual homes using one consistent style.



Design Individual Homes Using One Consistent Style

- Vary color and materials on adjacent homes to avoid repetitious streetscapes.
- Provide design features such as trellises, shutters, balconies and planter boxes to provide interest and accents to homes.

7.1.3.2 Building Design for Rear Accessed Housing

1. For lots larger than 3,999 square feet, at least 20% of the homes within a subdivision shall be one story.
 - Create visually interesting streetscapes by varying the bulk and mass of buildings.
 - Combine single and two-story building elements and forms such as covered porches and entry towers.
2. At least 50% of the corner lots in a subdivision shall be either one story or combined with two story massing that steps back away from front and exterior side yard setbacks a minimum of 5 additional feet.
 - The use of one-story porches on two-story corner elevations is encouraged.
3. Design elevations to be viewed from multiple directions.
 - Avoid large facades with no windows or surface articulation especially when visible from a public street.
 - Rear elevations are secondary but should not appear monotonous.

4. Building plans should highlight the entry providing visibility and identity from the street.
 - For variety, allow some corner lots to enter from side streets.
 - Integrate features such as columns, porches and roof overhangs to create a protected entry area.
 - Primary entries shall not be accessed solely from alleyways.
5. Roof forms should be appropriate to the architectural style of the home.
 - Avoid monotonous and repetitive forms.
 - Break up roof forms with well proportioned changes in roof pitch or direction.
 - Flat roofs are not allowed, unless an exception is made for a specific architectural style.
6. For homes on lots under 5,000 SF, maximum building height shall not exceed 35 feet or 2-1/2 stories. Maximum height for homes on lots 5,000 SF or larger shall be 30 feet or 2-stories.
 - Building height shall be measured from finished grade to the midpoint of the primary roof.
7. Color and material requirements shall conform to those listed in the “Residential Design Guidelines.”
 - Materials and colors should be compatible with adjacent uses.
 - A consistent material treatment should wrap around all sides of a building.
8. Architectural style requirements shall conform to those listed in the “Residential Design Guidelines.”
 - Design individual homes using one consistent style.
 - Vary color and materials on adjacent homes to avoid repetitious streetscapes.

7.1.3.3 Building Design for Greenway Clusters

1. Create visually interesting streetscapes by varying the bulk and mass of buildings.
 - Combine single and two story building elements and forms such as covered porches and entry towers.
 - Wrap around porches are encouraged on corner lots. Ensure that porches are large enough to be functional.
2. At least 50% of the corner lots in a subdivision shall be either one story or combined with two story massing that steps back away from front and exterior side yard setbacks a minimum of 5 additional feet.
 - The use of one-story porches on two-story corner elevations is encouraged.



Place Garages in the Rear of the Lot



Use a Porte Cochere to Conceal the Garage



Set Garage Doors Behind the Building Facade



Alleyways Enhanced with Landscaping

3. Design elevations to be viewed from multiple directions.
 - Avoid large facades with no windows or surface articulation especially when visible from a public street.
 - Rear elevations are secondary but should not appear monotonous.
4. Building plans should highlight the entry providing visibility and identity from the street.
 - For variety, allow some corner lots to enter from side streets.
 - Integrate features such as columns, porches and roof overhangs to create a protected entry area.
5. Roof forms should be appropriate to the architectural style of the home.
 - Avoid monotonous and repetitive forms.
 - Break up roof forms with well proportioned changes in roof pitch or direction.
 - Flat roofs are not allowed, unless an exception is made for a specific architectural style.
6. For clusters, maximum building height shall not exceed 35 feet or 2-1/2 stories.
 - Building height shall be measured from finished grade to the midpoint of the primary roof.
7. Color and material requirements shall conform to those listed in the “Residential Design Guidelines.”
 - Materials and colors should be compatible with adjacent uses.
 - A consistent material treatment should wrap around all sides of a building.
8. Architectural style requirements shall conform to those listed in the “Residential Design Guidelines.”

- Design individual homes using one consistent style.
- Vary color and materials on adjacent homes to avoid repetitious streetscapes.

7.1.4 GARAGES

7.1.4.1 Front Accessed

1. Avoid garage dominated streetscapes.
 - Minimize visual impacts of garage doors by locating garages to the rear of the lot or providing additional garage setback from the street.
 - Front facing garage doors shall be recessed a minimum of 3 feet behind the front elevation of the house.
 - The use of decorative garage doors with details such as glass panels or pattern moldings shall be required.
 - Garages with a maximum of two garage doors shall be allowed to face the street.
 - Driveway aprons shall be a minimum of 18 feet long, as measured from garage face to property line.
2. No more than two adjacent homes shall use the identical garage configuration.

7.1.4.2 Rear Accessed

1. Garages with a maximum of three garage doors shall be allowed when accessed by an alley.
 - The use of decorative garage doors with details such as glass panels or pattern moldings shall be required.
2. On corner lots, blank garage walls along public streets should be avoided.
3. Landscaping, including trees, shall be provided along alleys. Accent paving at alley entries is encouraged.

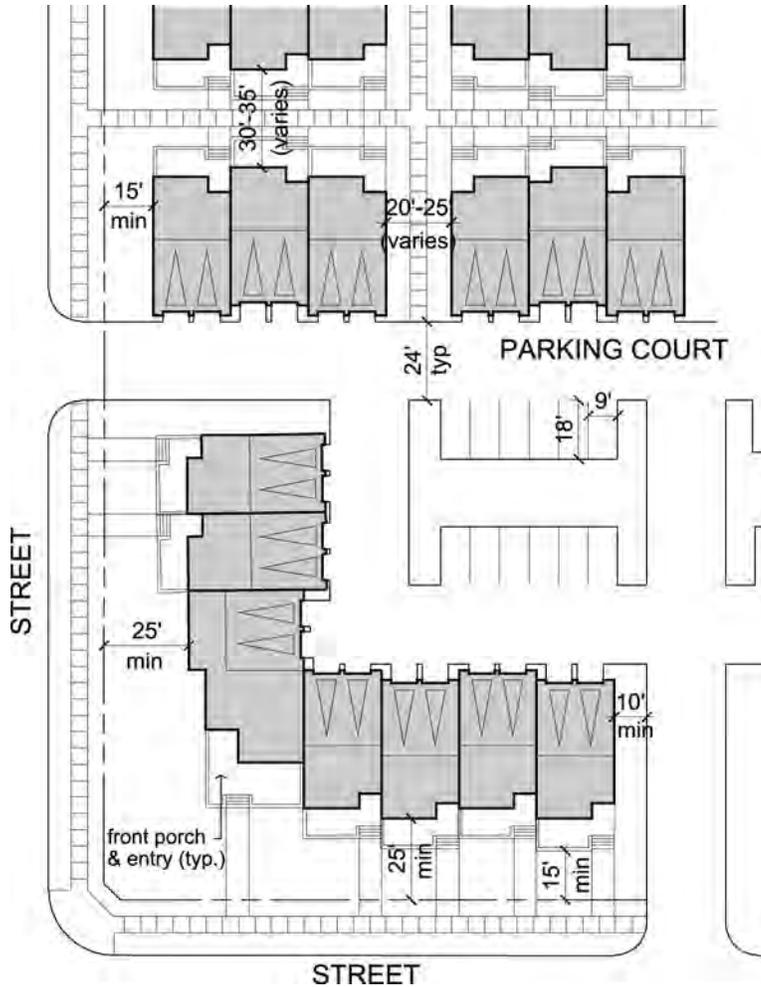


Entry Porches Address the Street



Buildings can Help Frame and Define Adjacent Open Space

SECTION 7. DESIGN GUIDELINES



Typical Multi-family Setbacks

7.1.4.3 Greenway Clusters

1. Garages shall have a maximum of two garage doors.
 - The use of decorative garage doors with details such as glass panels or pattern moldings shall be required.
2. On corner lots, blank garage walls along public streets should be avoided.
3. Accent paving for motor courts is required.
 - Motor courts shall be landscaped.

7.1.5 MULTI-FAMILY DESIGN GUIDELINES

7.1.5.1 Introduction

Multi-family housing makes diverse and affordable life style options accessible for people wishing to live in high quality communities. The challenge of new development on the one hand is to create a statement of uniqueness and identity while also contributing and blending with the existing context. Guidelines will therefore be flexible and open to interpretation and intended to foster fresh and creative solutions. Standards will be requirements that must be followed to ensure the smooth integration of new development within the existing City fabric and way of life. The overall goals and purposes already described within the City of Oakley's Residential Design Guidelines will therefore apply to these multi-family guidelines.

7.1.6 MULTI-FAMILY SITE PLANNING

Just as with single family lotting, the siting of multi-family buildings needs to be sensitive to the overall effects on adjacent streetscapes. Building orientation and setbacks to perimeter streets and the placement of open space and parking all impact the visual quality of the surrounding neighborhoods.



Buildings Should Resemble Clusters of Individual Homes



Upper Floor Balconies Reduce the Building Mass

1. Buildings should orient toward and reinforce the street pattern. Vary building length, separations or setbacks to create visual interest and variety.
2. Where possible, provide entry porches and front stoops along perimeter streets in order to link with adjacent public sidewalks or trails. Avoid the use of project walls on local or smaller collector streets and along public open space.
3. Buildings should frame project entries and parks to better define these outdoor spaces.
4. Provide long views into the project especially at entry points and places where the project abuts public open space.
5. Orienting buildings to best take advantage of prevailing winds, sun angle or important views is encouraged.
6. Position project open space and recreation facilities to benefit the maximum number of units. Link these amenities to public open spaces and trails where possible.
7. Provide at least 60 square feet of private or semi-private open space (such as a balcony, enclosed patio or front porch) for each unit.

7.1.7 MULTI-FAMILY BUILDING DESIGN

Multi-family buildings should be appropriate in scale and massing to their adjacent environment. Higher density projects can act as transition and buffers between lower density areas and more public or noisier uses. When located in predominately single family areas, multi-family housing should add a visual diversity that both makes an individual statement and utilizes some of the following elements to blend with lower density housing around it.

1. The scale and massing of multi-family buildings shall reflect that of its surrounding environment.

SECTION 7. DESIGN GUIDELINES



The Front Door can Create Identity from the Street



Articulate Rear Elevations

- Buildings should resemble clusters of individual homes rather than one entire unit.
 - Long building facades should be divided into segments compatible with adjacent uses.
 - These segments should be distinguished by architectural features such as front stoops or covered porches, distinctive color schemes and individual roof forms.
 - Elements such as upper floor setbacks, balconies, single-story front porches, changes in plate heights and individual roof forms should be used to reduce the building mass, and to transition between uses.
2. Building facades shall be articulated in order to create visually interesting elevations.
 - A strong building “front door” should be created using recessed entries, usable front porches, or tower elements. Maintain entry visibility and identity from the street.
 - Rear facades should be articulated using changes in wall plane and recessing garage doors. Decorative garage doors are recommended.
 - Avoid blank side elevations using window placement, banding and changes in materials and wall plane.
 - Window and door proportions and detailing should be consistent with the overall architectural style of the building. Recess windows and doors or highlight with trim to create shadow and texture on wall surfaces. Vary entry details to lend individual identity to each home.
 3. Roof forms consistent with a given architectural style shall be used.
 - Flat roofs are not allowed, unless an exception is made for a specific architectural style.

- Use dormers and breaks in the roof plane or plate height to add interest to large expanses of roof.
 - Metal flashing, venting and rain gutters shall be either made out of copper or colored to match the roof, trim or wall color of the building.
 - All mechanical equipment, located either on the roof or on the ground shall be screened from view. Roof screens shall be integral with the architectural style of the building.
4. Color and material requirements shall conform to those listed in the “Residential Design Guidelines.”
 - Materials and colors shall be compatible with adjacent uses.
 5. Architectural style requirements shall conform to those listed in the “Residential Design Guidelines.”
 - Identical floor plans shall be articulated and arranged in a non-repetitive manner.

7.1.8 MULTI-FAMILY GARAGES AND PARKING

1. Multi-family garages and parking areas shall not dominate street frontages.
 - Locate parking to minimize visual impacts to the street.
 - Parking along streets should be buffered using landscaping, berming or low walls.
 - Large areas of parking should be divided into smaller courts using techniques such as landscape pockets, changes in orientation or changes in paving materials.
 - Bicycle parking racks shall be provided.



Carports Shall Match the Style of Residential Buildings

2. Carports shall match the architectural style and detail of the residential buildings.
 - Flat roofed carports are prohibited.
 - Trash enclosures shall be covered by a trellis or otherwise integrated into the carport structure. Enclosures shall be consistent with the overall architectural style of the project and buffered by walls and landscaping.
3. Where individual garages are provided in a multi-family project, access shall be taken from driveways, alleyways or private streets but not directly from public streets.
 - Detached garages shall be consistent with the overall architectural style of the project.
 - Garage doors should be wide enough and tall enough to accommodate SUV's and pickup trucks.
 - Detached garage setbacks from arterial and collector streets shall be equal to the building setback.

7.2 LANDSCAPE DESIGN GUIDELINES

The East Cypress Corridor Specific Plan Landscape Design Guidelines have been prepared to provide the framework for unifying and quality design within the Project Site. The guidelines express the desired character of future development with the goal of providing for an integrated community. Common development features such as landscape and hardscape, walls and fences, and trails are controlled throughout the development to provide unity and express a strong collective design statement.

The Design Guidelines establish development criteria to ensure a unified environment within the master plan. The landscape Design Guidelines are not intended to limit innovative design, but rather to provide clear direction and design criteria.

7.2.1 CHARACTER

The landscape character of the East Cypress Corridor Specific Plan Area will be a defining element in the overall visual quality of the area. Landscape treatments will visually clarify gateways and entries, road hierarchies and neighborhood structure. Landscaping will help define edges and community focal points. Landscape elements will highlight the system of parks, lakes, open spaces and trails that is at the core of the community image. The “Illustrative Landscape Master Plan” included as Exhibit 30 conveys the overall landscape concepts. The “Landscape Structure Diagram” included as Exhibit 31 illustrates the main landscape components discussed in this section.

7.2.2 STREETSCAPES

A well defined roadway hierarchy clarifies circulation, aids in way-finding and enhances the legibility of the area. The roadway system proposed

for the East Cypress Corridor Specific Plan is included in Exhibit 32 “Roadway Hierarchy Diagram.” In the East Cypress Corridor, generous landscape setbacks and medians characterize the two major arterials, East Cypress Road and Bethel Island Road (future Byron Highway extension). Project collector roads, as the primary access routes into the various project areas, have wide landscaped setbacks and medians. The loop roads, carrying traffic to the various local neighborhoods, receive distinctive treatment. Local roads within the neighborhoods have an intimate scale, with landscape setbacks for shade trees and decorative planting. The Project Site roadway landscape treatments are described in this section.

7.2.2.1 East Cypress Road

East Cypress Road will be the primary route into the Specific Plan Area and as such, will create first impressions and establish the area’s image. The arterial East Cypress Road will be constructed to the north of the existing East Cypress Road. The existing road will no longer carry any through traffic, serving only the existing residences along the south side of East Cypress Road. The new East Cypress Road will have a 150’ right-of-way for the length of its arterial designation. The most prominent visual feature will be closely spaced rows of columnar trees creating a strong vertical line along the right-of-way edge. This strong line will lead to the center of the Specific Plan Area, and function as a ceremonial entry zone.

The northern setback along East Cypress will be 40’ wide, allowing for a 10’ multi-use trail meandering through 30’ of planting. The row of columnar trees will be augmented by triangulated rows of canopy trees, providing summer shade over the multi-use trail, and fall color. The residential neighborhoods to the north will be buffered from the traffic by a community theme wall of a minimum 6’ in height, as



LEGEND

-  Levees
-  Parks
-  Powerline Easement
-  Wetlands

Exhibit 30
 Illustrative Landscape Master Plan



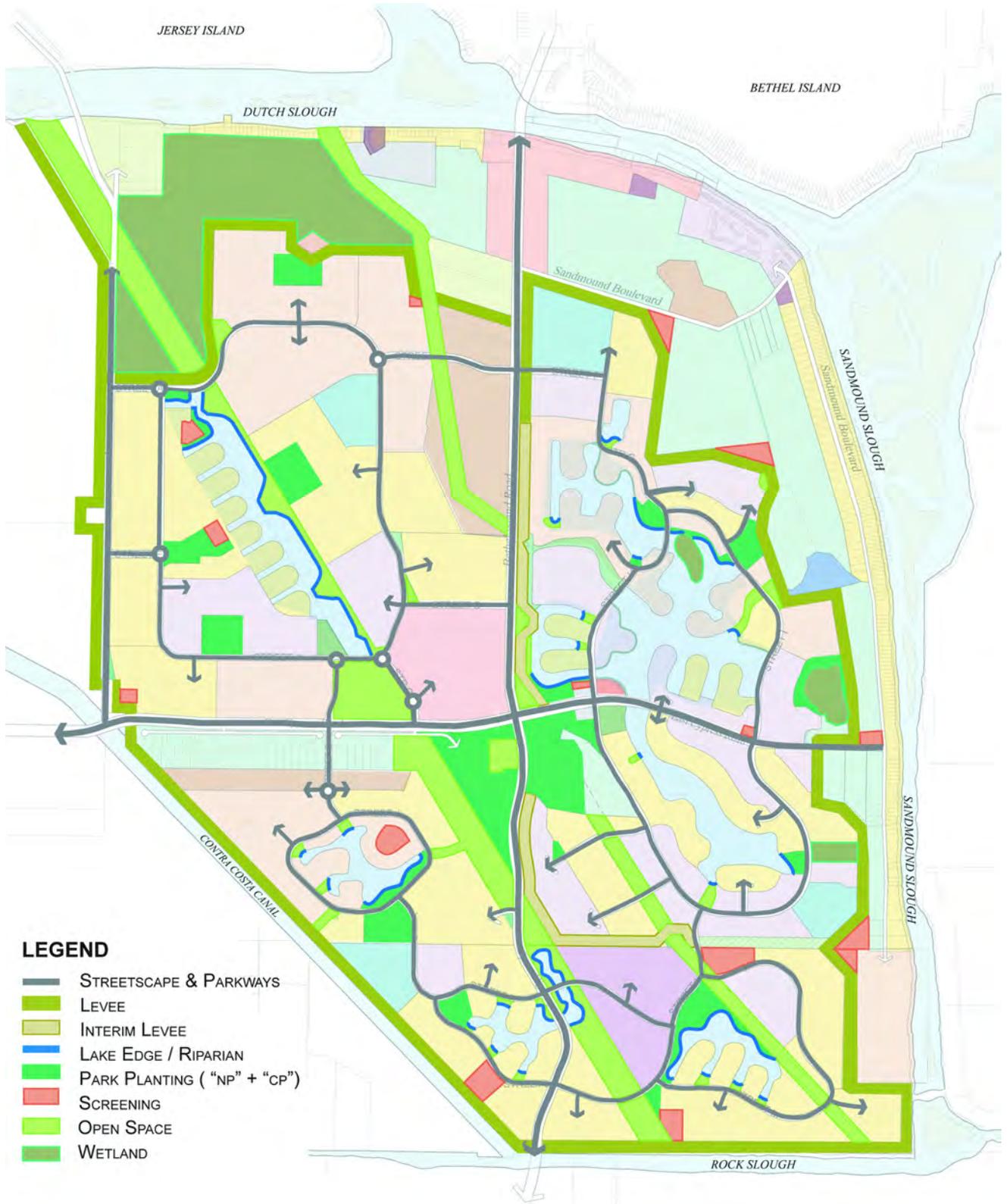


Exhibit 31
Landscape Structure Diagram



SECTION 7. DESIGN GUIDELINES



Exhibit 32
Roadway Hierarchy Diagram



determined by a noise study. The southern setback will be 30' wide, allowing for a 6' wide pedestrian trail. The rows of columnar trees will create a visual buffer to separate the existing right-of-way. The existing roadway will be further separated from the new East Cypress right-of-way by a 10' landscape setback. The existing residential neighborhood to the south will be buffered from the traffic by a community theme wall of a minimum 6' in height, as determined by a noise study. Where East Cypress Road abuts park or open space, there will be no walls, and views into the sites will be provided. A 16' wide landscaped median will be planted with a double row of flowering accent trees. All landscaped areas within the right-of-way will be planted with an understory of low shrubs and groundcover.

East Cypress Road will be elevated at its intersections with Jersey Island Road (entering the project) and Bethel Island Road. Where East Cypress rises as it approaches these intersections, the curb to curb width will remain at 80', but the roadway embankment will descend at a 2:1 slope, maximum, after a 2' shoulder on each side. Where the embankments slope in excess of 3:1, retaining structures will be required for tree planting along the embankment. Approaching the intersection with Bethel Island Road, the northerly multi-use trail will divert onto the Village Center entry road. On the south side of East Cypress Road, a 10' wide trail will remain, downslope from the roadway, rising along the embankment to the intersection.

East Cypress Road continues as an arterial from Bethel Island Road east to the lake. After crossing the lake, East Cypress becomes a "Loop Road," as described below.

The streetscape plan for the arterial portions of East Cypress Road is illustrated in Exhibits 33-36, "East Cypress Section," "East Cypress

Approaching Bethel Island Intersection," "East Cypress Plan," and "East Cypress/Bethel Island Intersection Plan."

7.2.2.2 Bethel Island Road

Bethel Island Road is the major north-south arterial. North of its intersection with East Cypress Road, Bethel Island Road will have the same 150' right-of-way as East Cypress Road. Bethel Island Road will be elevated at several locations, including its intersection with East Cypress Road, at the levee crossing north of the entry to Planning Area 6, approaching the Rock Slough Bridge, and approaching the Bethel Island Bridge. Street trees and ground cover will be planted in the parkway. Trees and understory planting will occur along the embankments. Where the embankments have a slope in excess of 3:1, retaining devices will be required for tree planting.

Where Bethel Island Road is at grade, a 40' landscaped setback along the west side will allow for a 10' wide multi-use trail and generous planting. Where the right-of-way abuts residential development, a community theme wall will be provided, at a minimum height of 6', as determined by a noise study. On the east side, a 30' landscape setback will accommodate tree planting and a 6' foot pedestrian trail. Groundcover and shrub planting will act as a barrier to limit access to the adjacent levee except at designated access points. Large canopy trees will create a visual edge, and shade the trails. The 16' wide landscaped median may include a double row of flowering trees. The streetscape treatment for Bethel Island Road, North is illustrated in Exhibit 37 "Bethel Island Road North of East Cypress Intersection (Looking North)" and Exhibit 38 "Bethel Island Road North Section (Looking North).

South of East Cypress Road, Bethel Island Road abuts park and open space for most of its

length. For this reason, the right-of-way is narrowed to 135', with narrower parkways consisting of a 25' landscape setback on the east side and 30' on the west side. A 6' pedestrian trail is provided on the east side, and a 10' multi-use trail on the west. Tree planting will continue in the same pattern as in the northern segment of Bethel Island Road. Where the right-of-way abuts parkland or open space, the landscaping will transition to the adjacent use, and the trail may meander outside the right-of-way into the park or open space. If the right-of-way includes portions of an archaeological site, buffering measures may be required and the setback treatment may have to be adjusted accordingly. Where Bethel Island Road approaches Rock Slough, it will again be elevated, and will have a narrower curb to curb width. A typical streetscape condition for Bethel Island Road, South is illustrated in Exhibits 39-41 "Bethel Island Road at Planning Area 4 Section (Looking South)," "Bethel Island Road at Planning Area 4 Plan," and "Bethel Island Road Approaching Rock Slough Section."

7.2.2.3 Jersey Island Road

Jersey Island Road at the western boundary of the Specific Plan Area will retain its character as a rural road. A 20' landscape setback along the eastern curb will allow for a 6' sidewalk with 7' of planting on either side. A double row of trees and either a community theme wall or a view fence, depending on the condition, will buffer residences. The 12' wide western setback will contain tree and groundcover planting, but no trail. Willows will be planted along the drainage channel west of the road. A multi-use trail will be located on the top of the levee which will parallel Jersey Island Road to the west. The streetscape for Jersey Island Road adjacent to the residential development is illustrated in Exhibit 42 "Jersey Island Road at

Residential Section" and Exhibit 44 "Jersey Island Road at Residential Plan."

North of the residential project, Jersey Island road will abut wetlands. Tree planting will end at the most northerly entrance to the residential development, and the sidewalk will become a 6' wide trail. The wetland will be buffered by low planting and 5' high fencing that allows views into the wetland area. The streetscape for Jersey Island Road is illustrated in Exhibit 43 "Jersey Island Road at Wetlands Section," and Exhibit 45 "Jersey Island Road at Wetlands Plan."

Jersey Island Road will be elevated at its intersection with East Cypress Road and toward the north end of the Specific Plan area, where it crosses the levees.

A multi-use trail will be located on the top of the levee which will parallel Jersey Island Road to the west. The levee along the west side of Jersey Island Road will be located on the CalFed "Dutch Slough Tidal Marsh and Floodplain Restoration Project" property. The option will be made available in the EIR for the Specific Plan Area to locate the levee along the east side of Jersey Island Road.

7.2.2.4 Project Collector Roads

Major project entry roads are classified as "Project Collector Roads." As the arrival zones into the projects, these roads will be given generous landscape setbacks and medians. The 100' right-of-way will accommodate a 16' median and two 22' setbacks. Because there will be on-street bike lanes, only 6' pedestrian sidewalks will be provided. Double rows of canopy trees will form the street edge, and a double row of flowering trees will be planted in the median. Residences will not front on collector roads, and they will have no on-street parking. Where the collector road abuts park

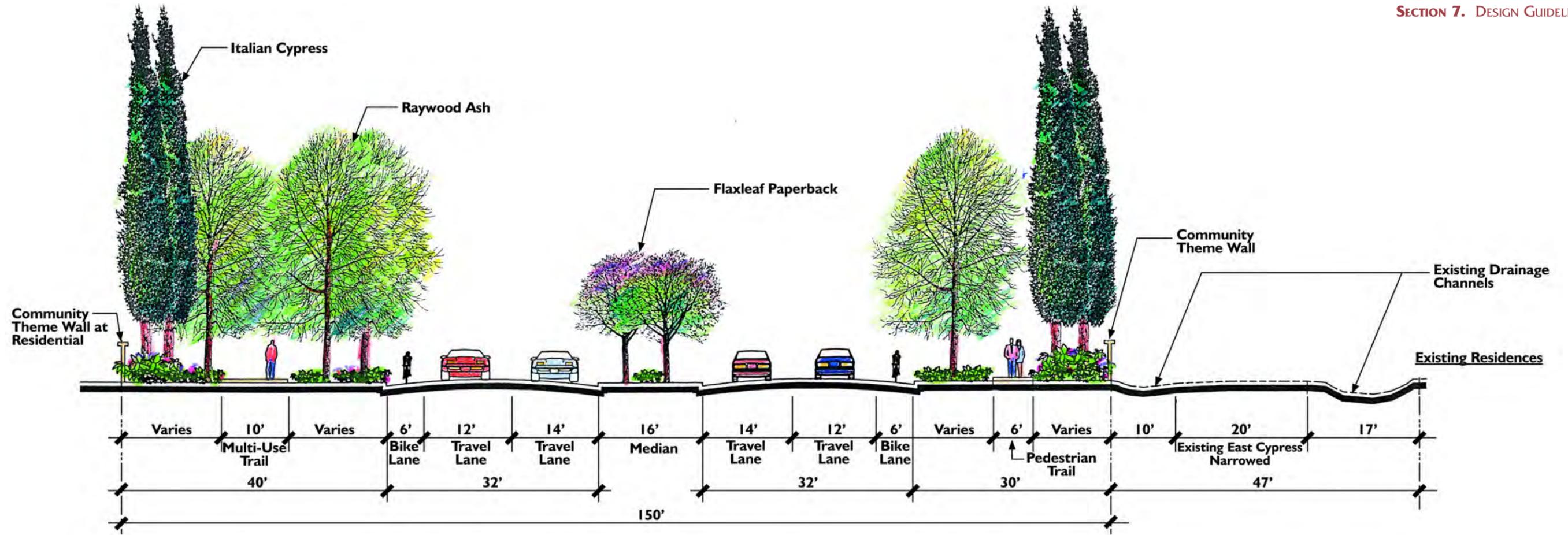


Exhibit 33
East Cypress Section

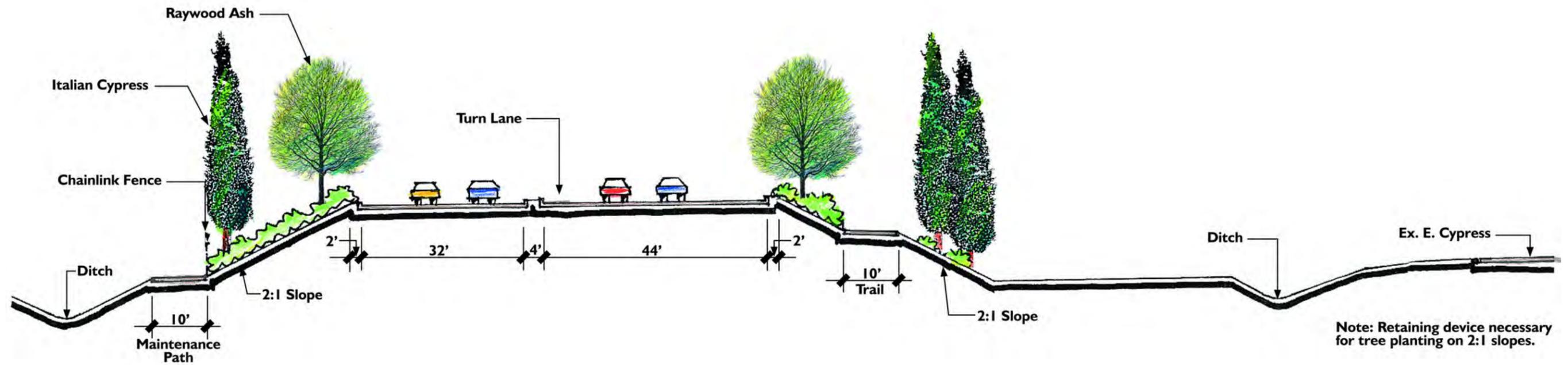


Exhibit 34
East Cypress Approaching Bethel Island Intersection

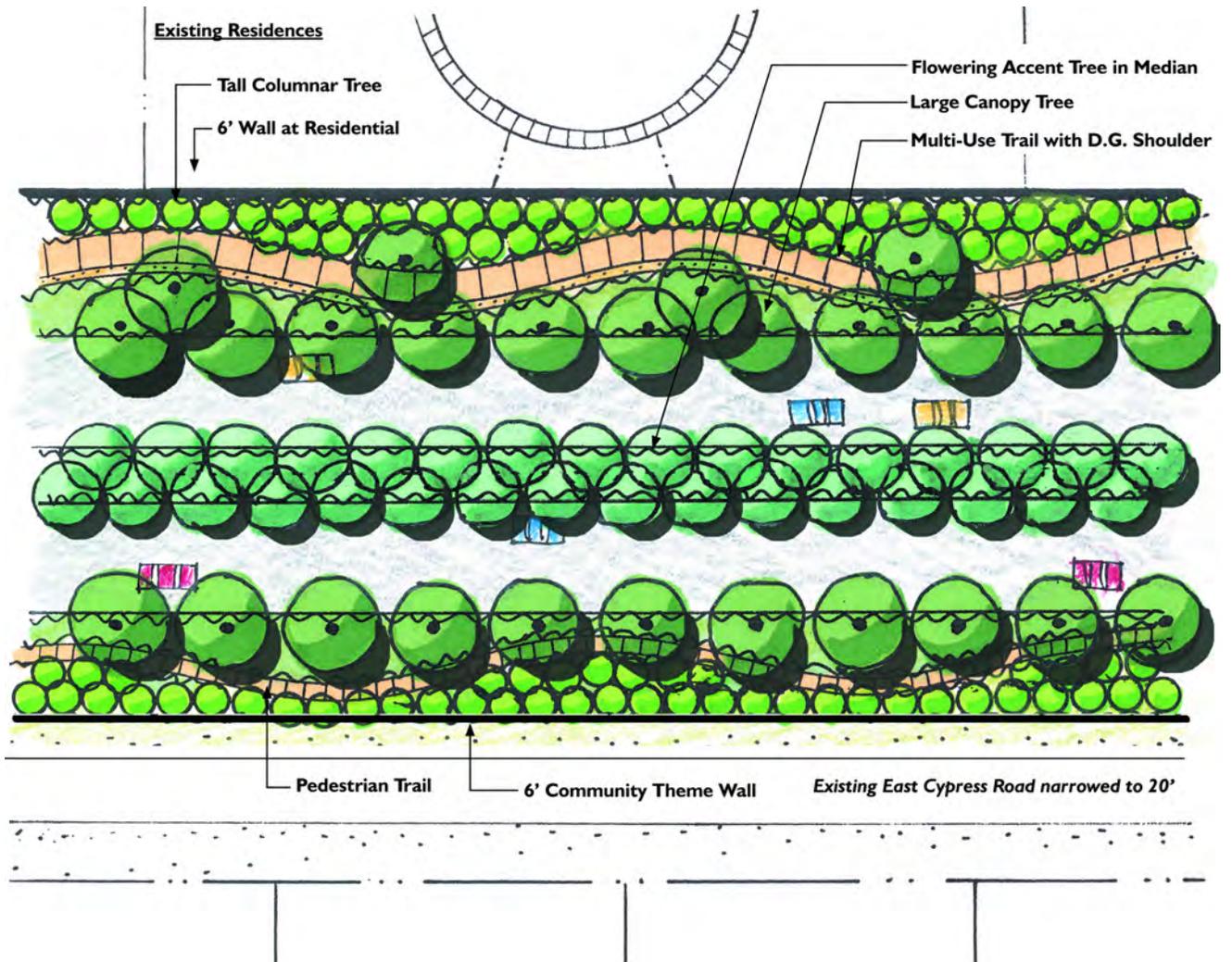


Exhibit 35
East Cypress Plan

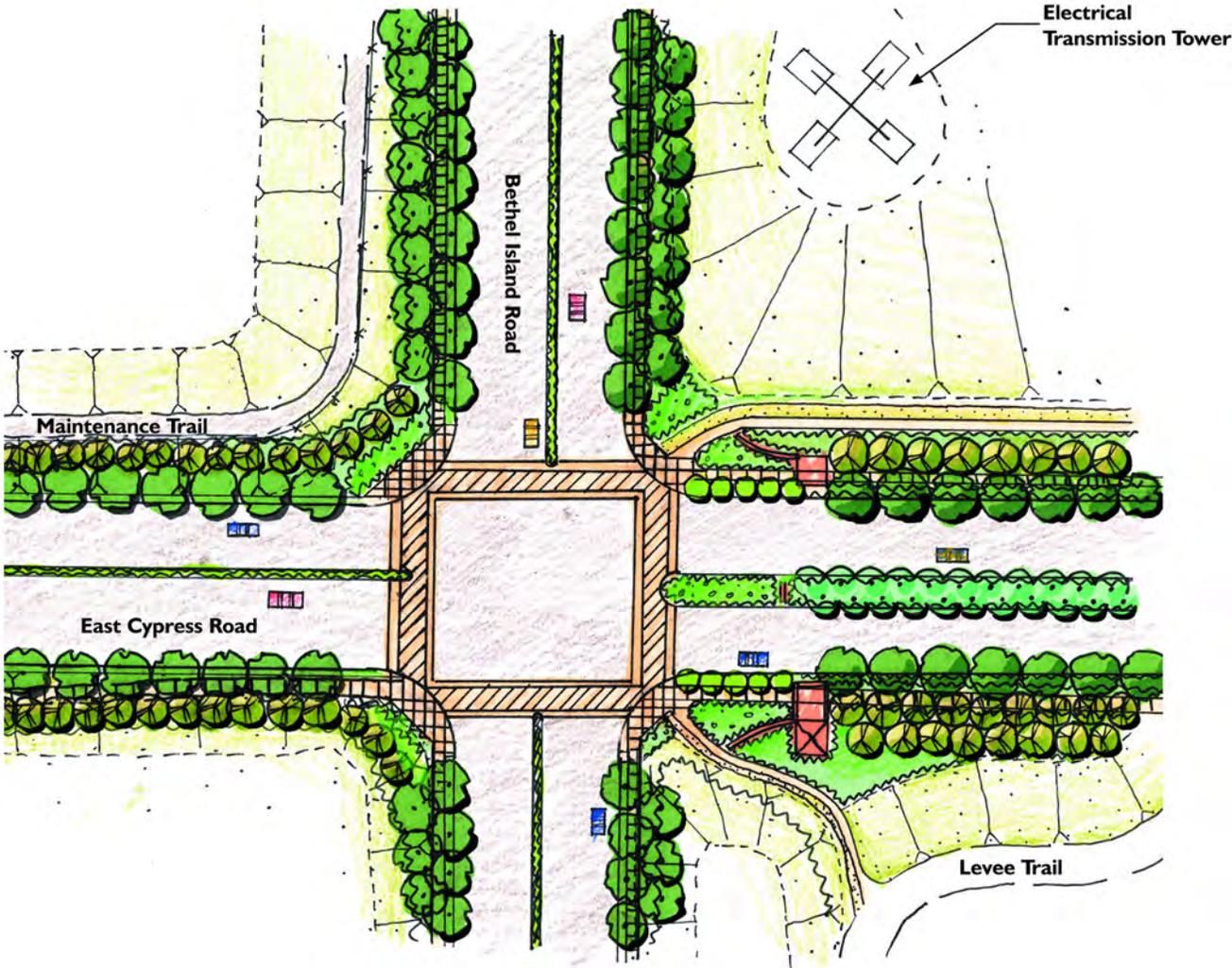


Exhibit 36
East Cypress / Bethel Island Intersection Plan

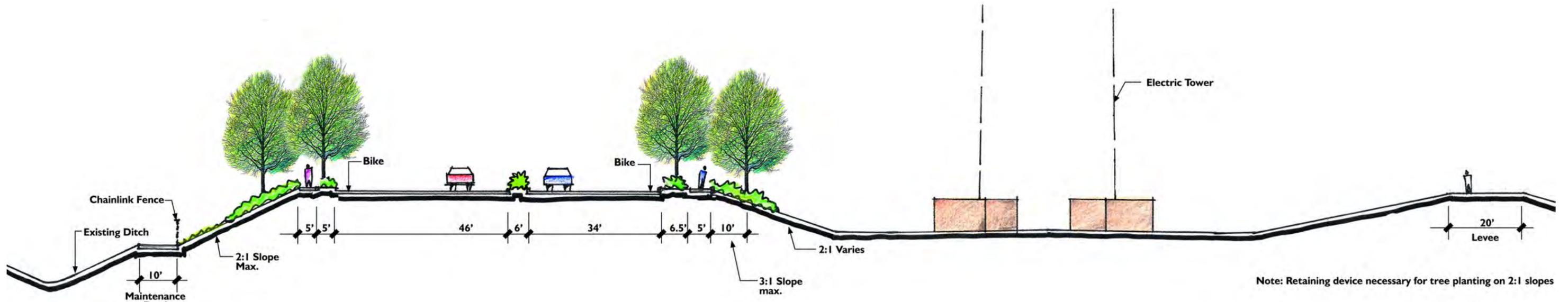


Exhibit 37
Bethel Island Road North of East Cypress Intersection
(Looking North)

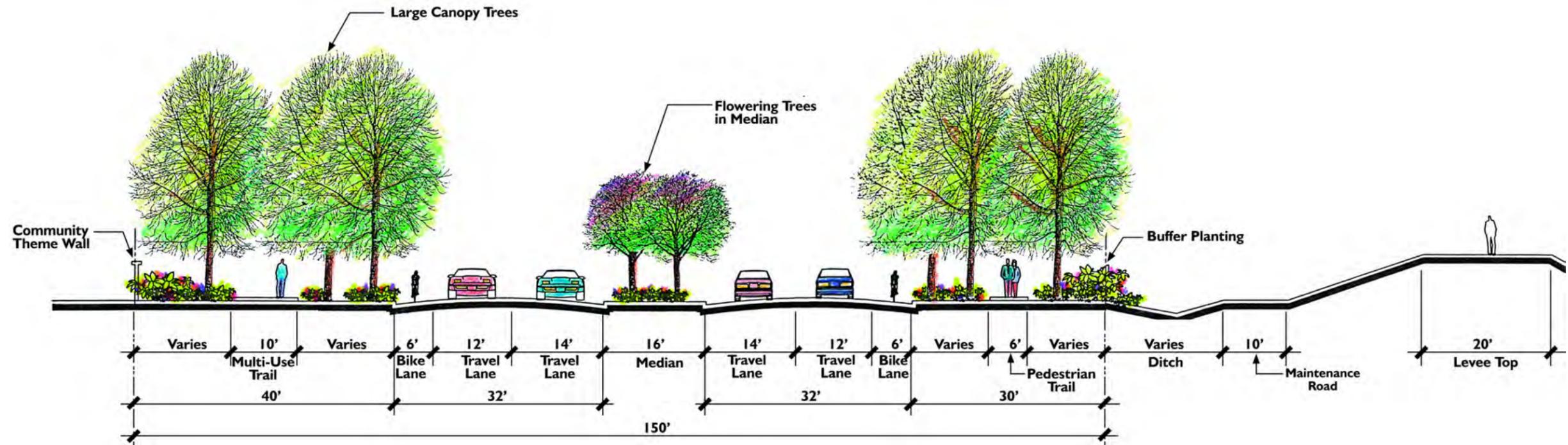


Exhibit 38
Bethel Island Road North Section
(Looking North)

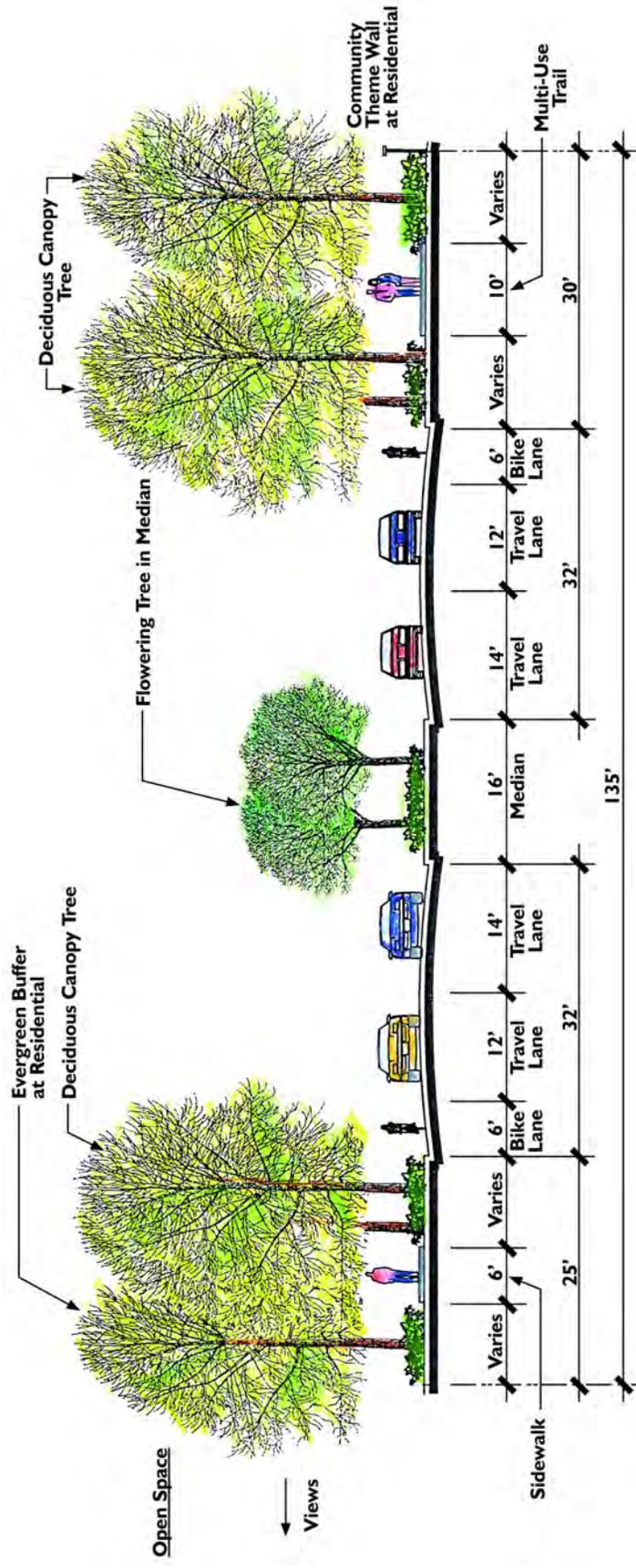


Exhibit 39
Bethel Island Road at Planning Area 4 Section
(Looking South)

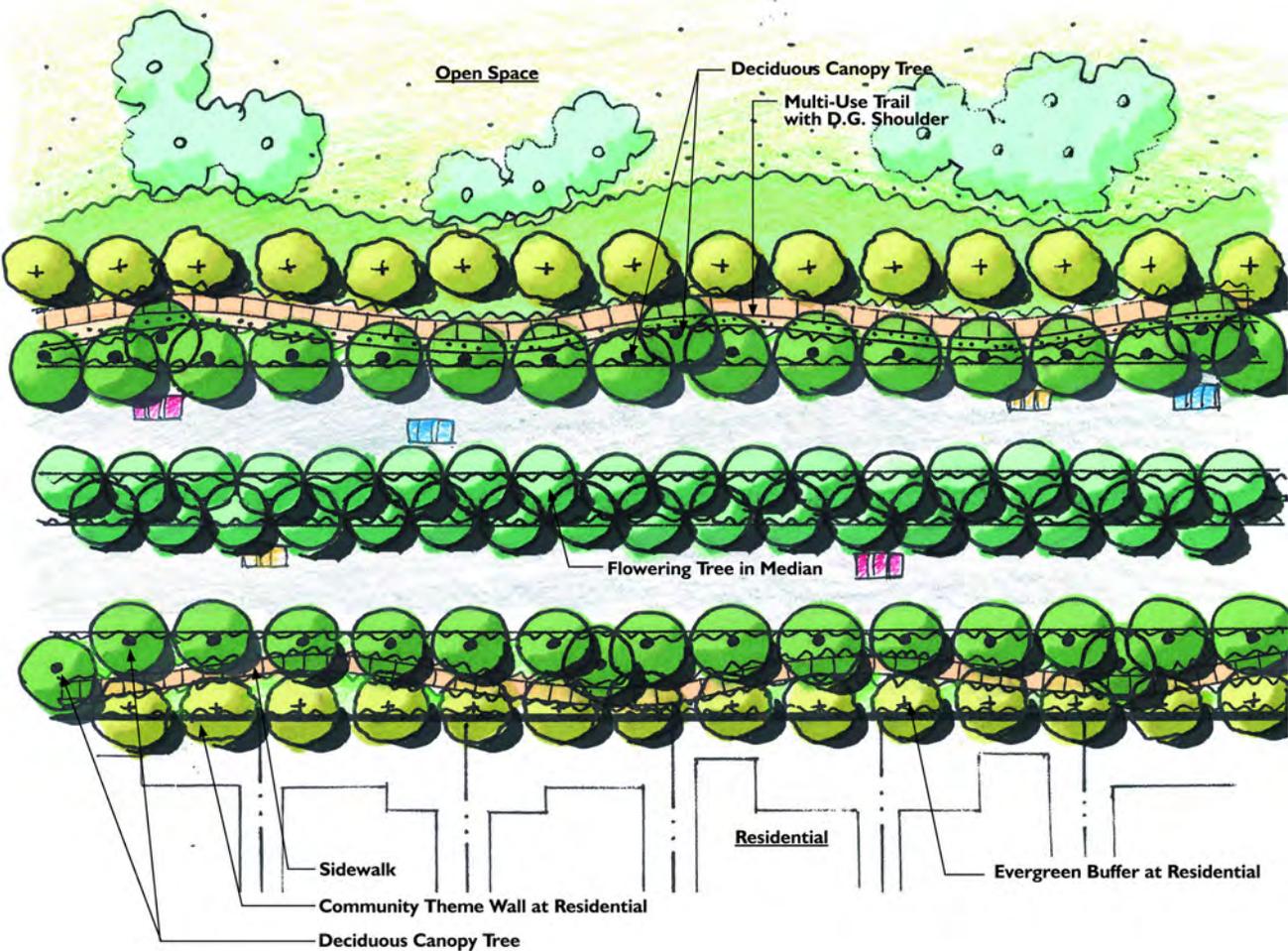


Exhibit 40
Bethel Island Road at Planning Area 4 Plan

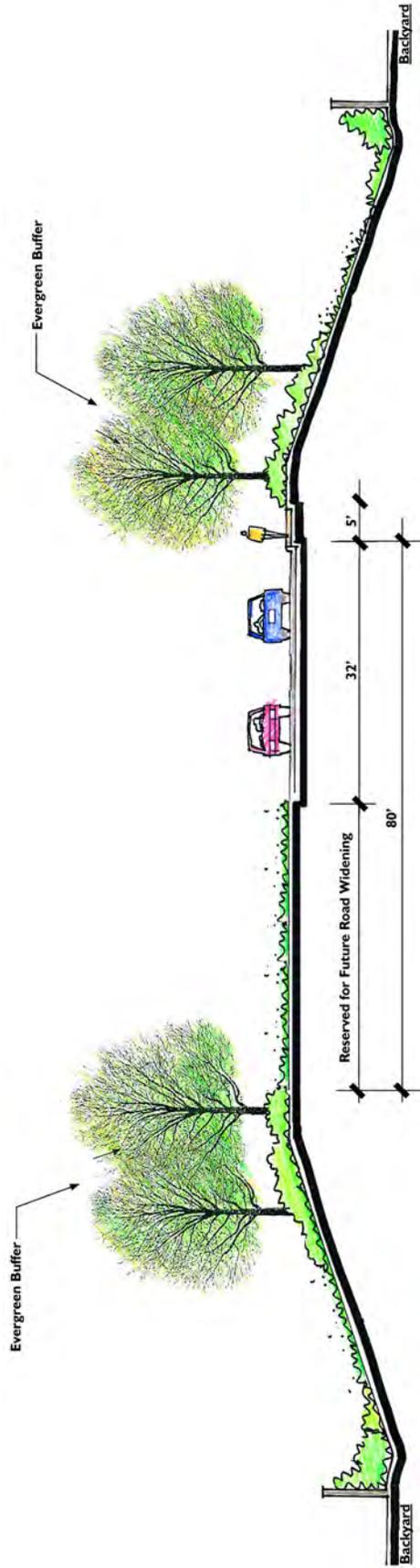


Exhibit 41
Bethel Island Road Approaching Rock Slough Section

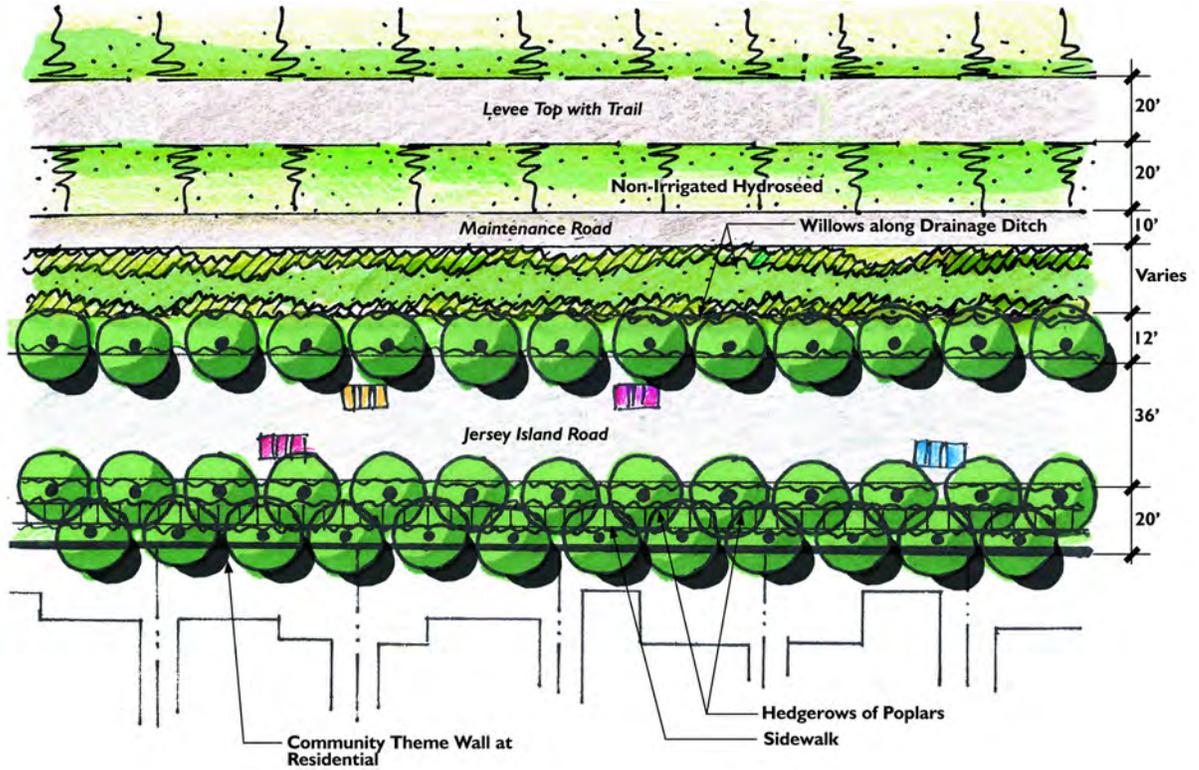


Exhibit 44
Jersey Island Road at Residential Plan

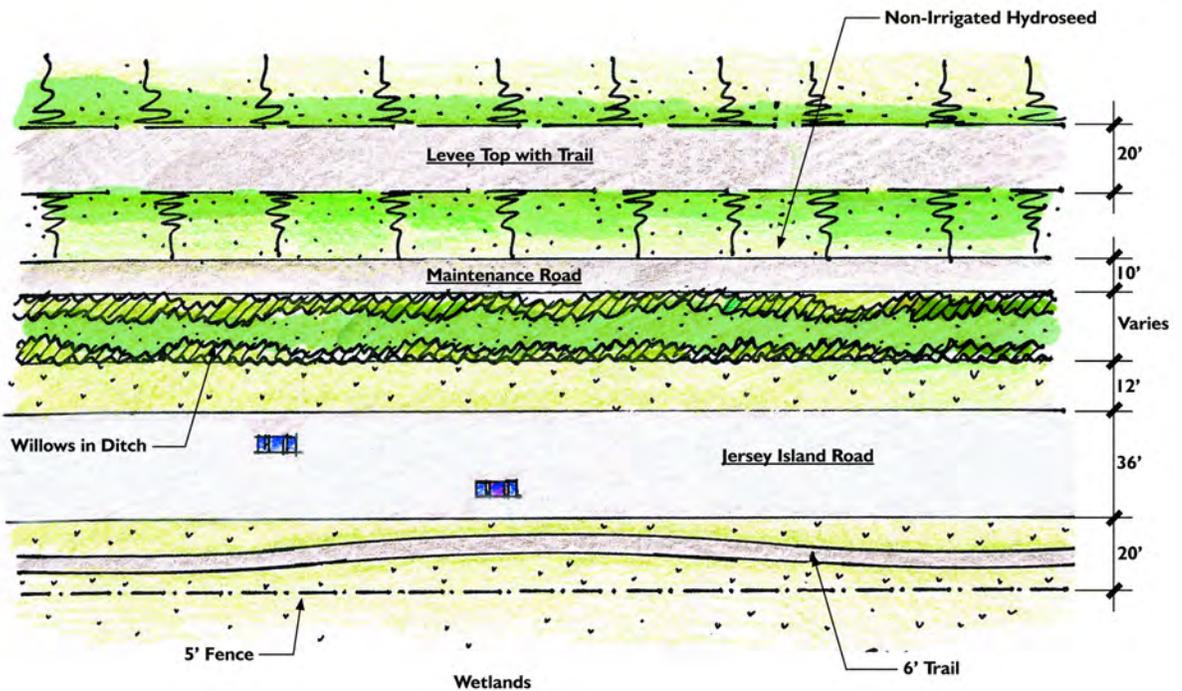


Exhibit 45
Jersey Island Road at Wetlands Plan

SECTION 7. DESIGN GUIDELINES

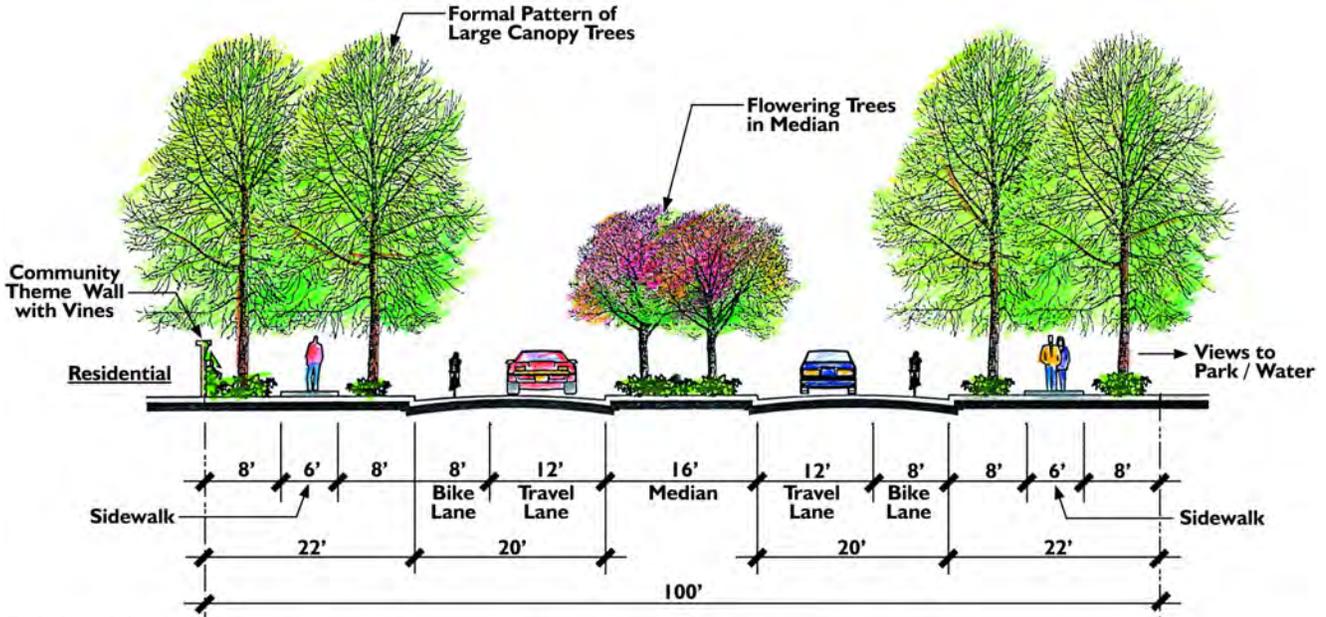


Exhibit 46
Project Collector Road Section

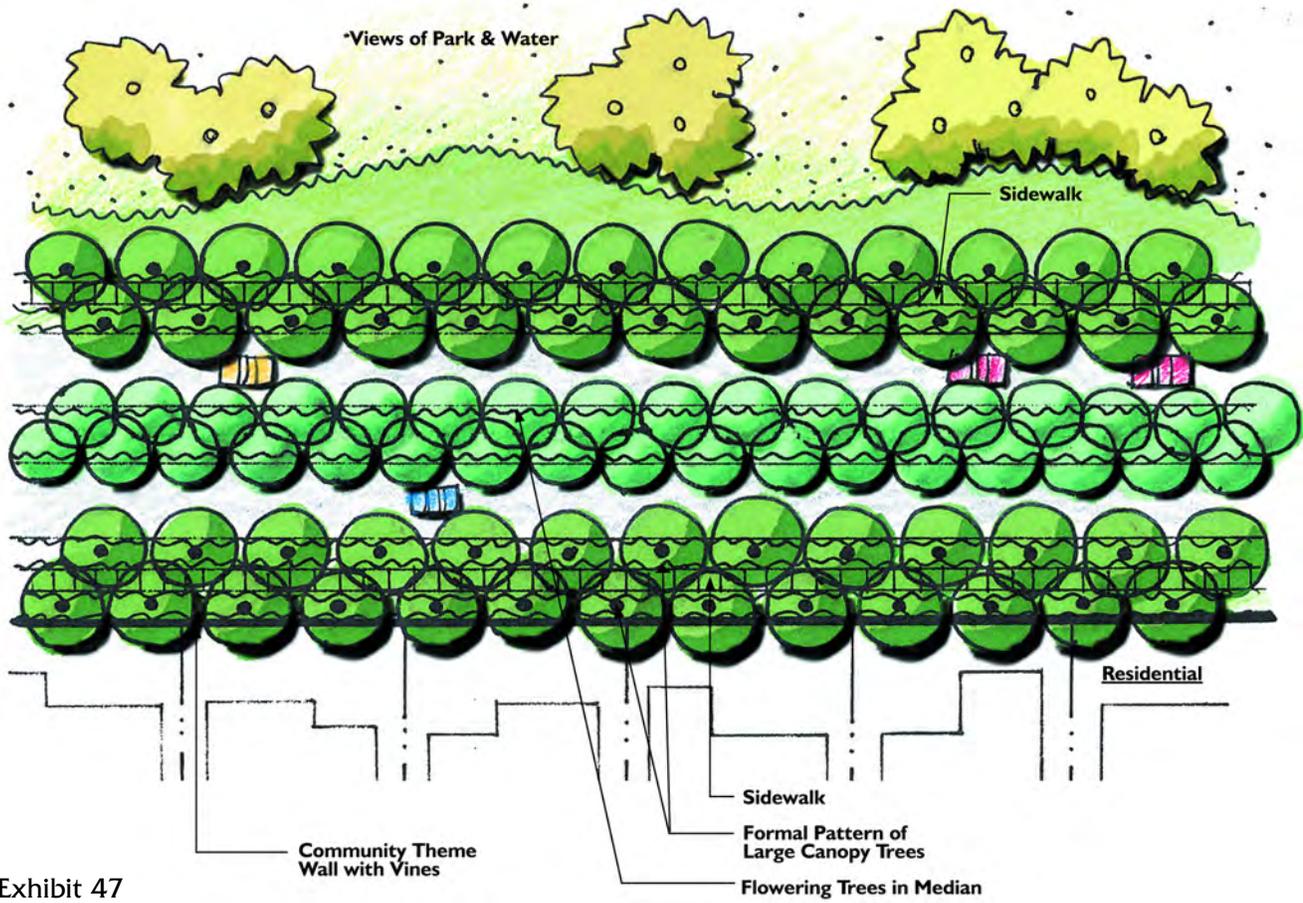


Exhibit 47
Project Collector Road Plan

or open space, the landscape will transition into that site. Where it is adjacent to residences, a community theme wall, of a minimum 6' height, determined by an acoustic study, will buffer the residences. The wall may be planted with vines. Typically, the Project Collector Road will lead to a rotary or other focal element. The streetscape plan for Project Collector Roadways is illustrated in Exhibits 46-47 "Project Collector Road Section" and "Project Collector Road Plan."

7.2.2.5 Loop Roads

The loop roads are the major circulation routes within each project, dispersing traffic to the neighborhoods. Local neighborhood entries occur off the loop roads. On-street parking is allowed. The configuration of the loop road varies depending on whether homes front onto the road.

Where homes do not front onto the loop road, the right-of-way will be 76'. There will be a 23' setback on at least one side of the road to allow for a multi-use trail consisting of 6' of paved trail and two 2' shoulders. The other landscape setback will be at least 17' wide, including a 7' parkway, 5' sidewalk and 5' planting area. The wider setback should be provided on the side of the loop road with less residential frontage (i.e. more park, open space and lake frontage). Double rows of canopy trees will enclose the street and provide shade. Drought tolerant walkable ground cover will be planted in the landscape strip between curb and sidewalk/trail. A 6' high good neighbor fence will buffer the residences. The streetscape for Loop Roads within the Project Site is illustrated in Exhibits 48-49 "Loop Road Section" and "Loop Road Plan."

Where homes front on the loop road, the right-of-way will be 60'. Setbacks of 12' on both sides of the road will consist of a 7' parkway, and 5' sidewalk. Appropriate traffic calming

measures will be incorporated into the design of front-loaded loop roads. The streetscape for Front Loaded Loop Roads within the Project Site is illustrated in Exhibits 50-51 "Front Loaded Loop Road Section" and "Front Loaded Loop Road Plan."

7.2.2.6 Local Streets

Local streets will be of an intimate scale enclosed by canopy trees. On-street parking is generally allowed. Several variations may occur, depending on traffic level and adjacent uses. The basic local street will have a 46' right-of-way, with a 36' foot curb-to-curb width and 5' attached sidewalks. Street trees will be planted in the 5' public utility easement (P.U.E.) setback behind the property line, and utility boxes will be located in that zone. Local streets which are cul-de-sacs or through streets serving fewer than 16 residences will be considered to be minor local streets. The right-of-way will be 42' on these streets, with the curb-to-curb dimension 32', and the sidewalk and setback dimensions as described above. On single loaded local streets the right-of-way will narrow to 38', with a 28' curb-to-curb width (with parking prohibited on one side) and setback and sidewalk dimensions as described above. All storm drain inlets should include storm drain markers upgraded from the standard polyurethane material to a decorative, more durable material, such as stamped brass or cast iron, subject to the review and approval of the City Engineer. Local Street streetscape plans are illustrated in Exhibits 52-55 "Local Street Section," "Local Street Plan," "Minor Local Street/Cul-de-Sac Section," and "Local Single Loaded Street Section."

7.2.2.7 Alleys

Homes on smaller lots may use private alleys for garage access. The entire alley 20' right-of-way will be paved, with planting occurring in the setback area to the sides of the garage

SECTION 7. DESIGN GUIDELINES

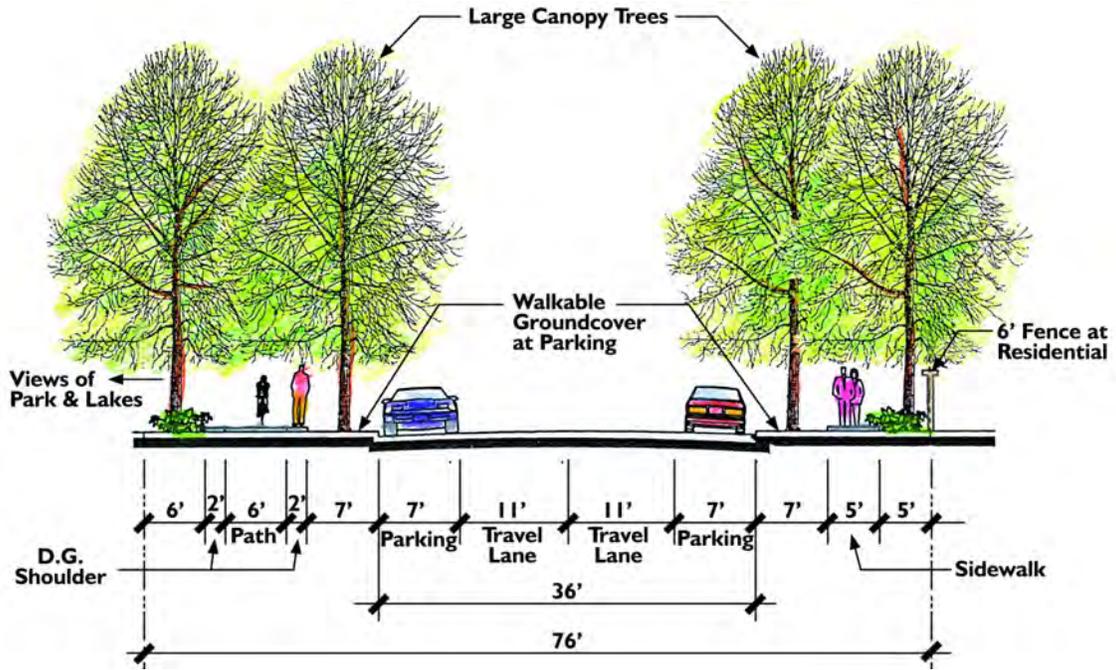


Exhibit 48
Loop Road Section

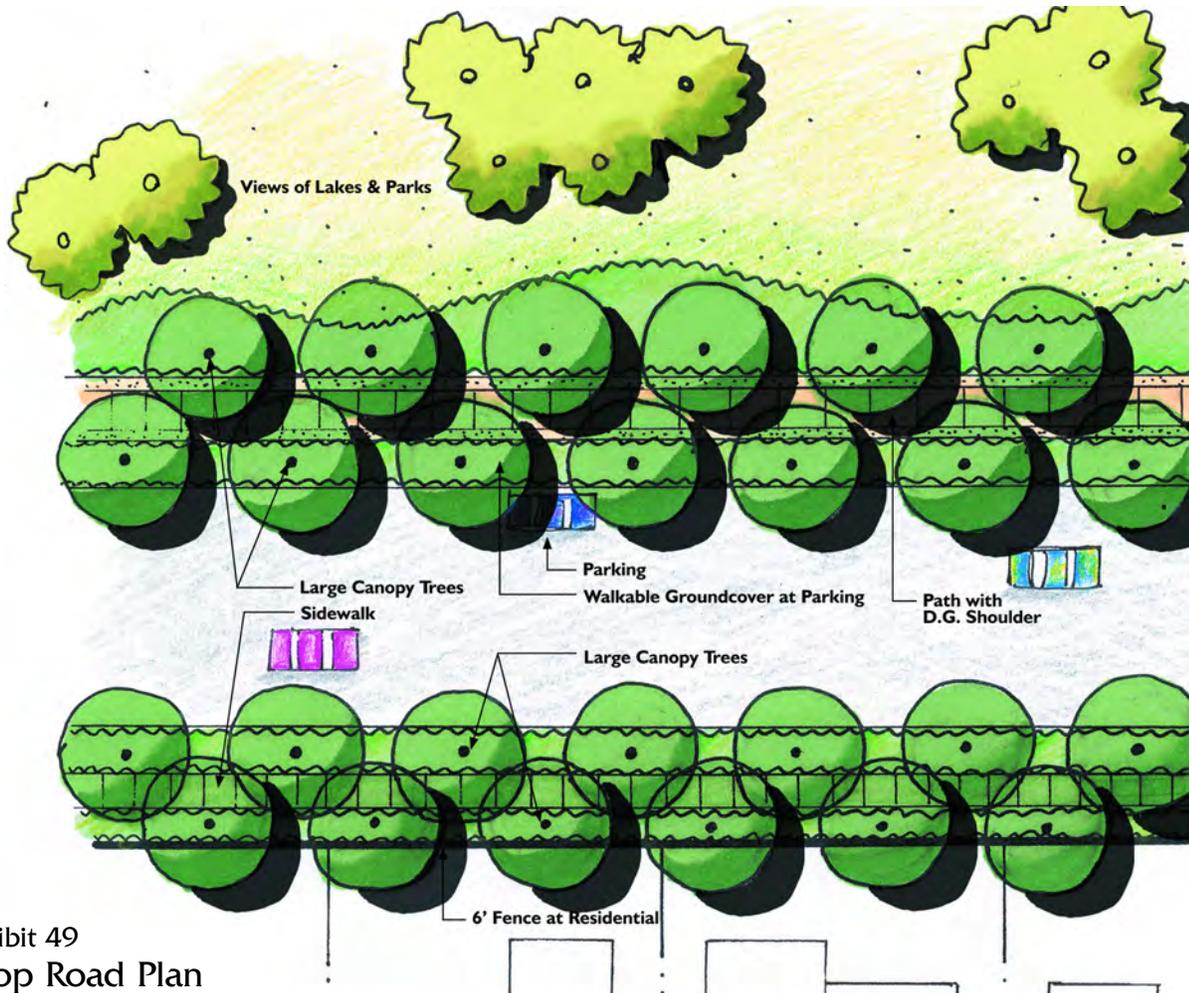


Exhibit 49
Loop Road Plan

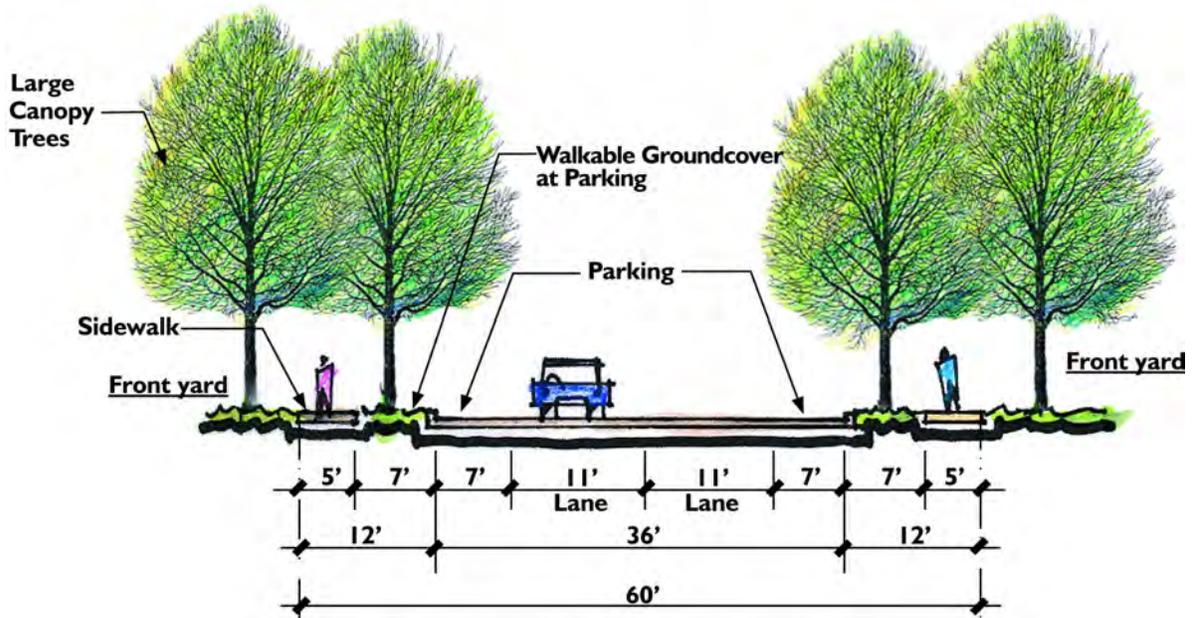


Exhibit 50
Front Loaded Loop Road Section

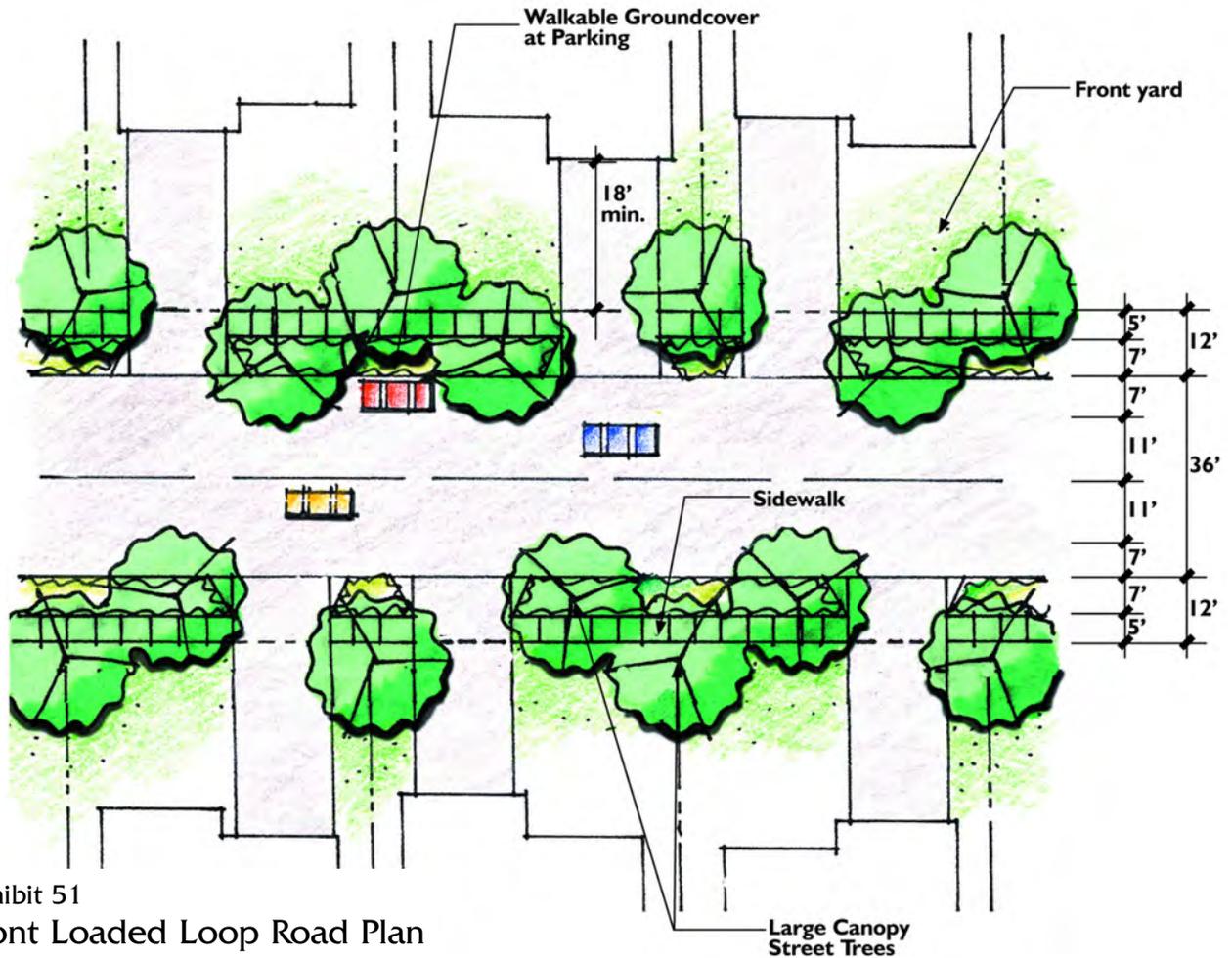


Exhibit 51
Front Loaded Loop Road Plan

SECTION 7. DESIGN GUIDELINES

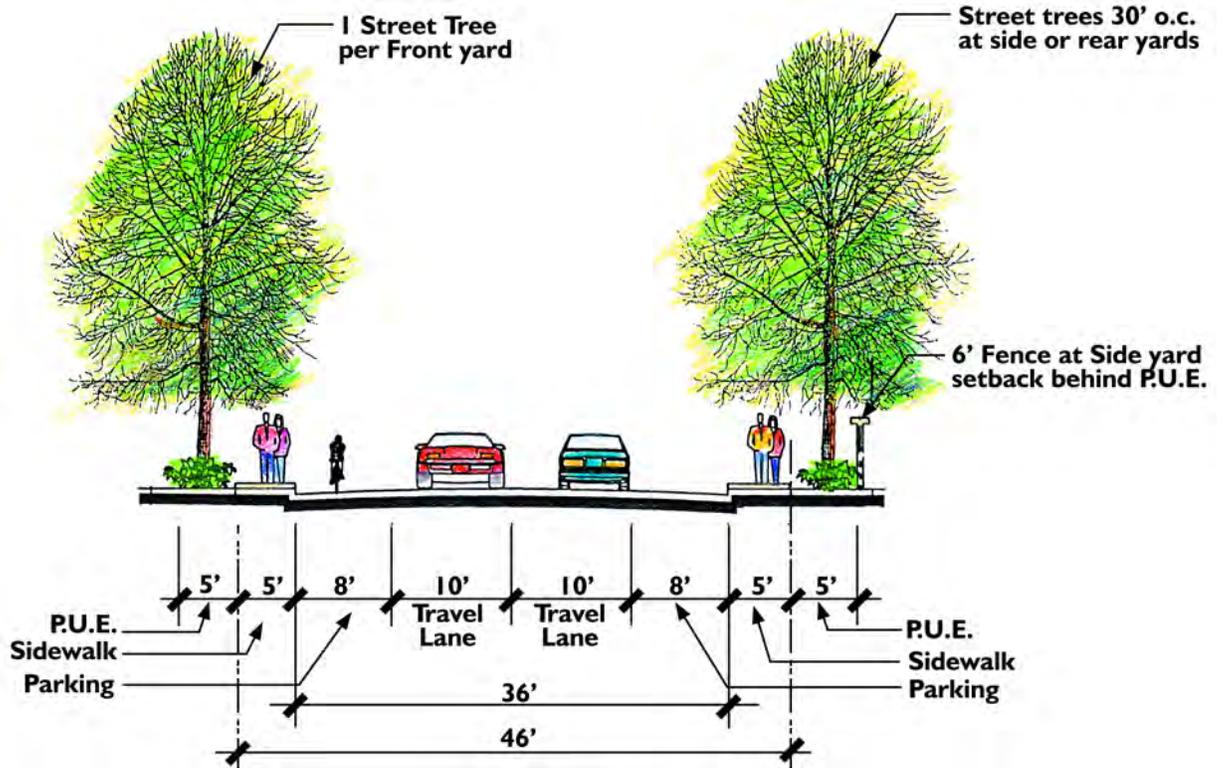


Exhibit 52
Local Street Section

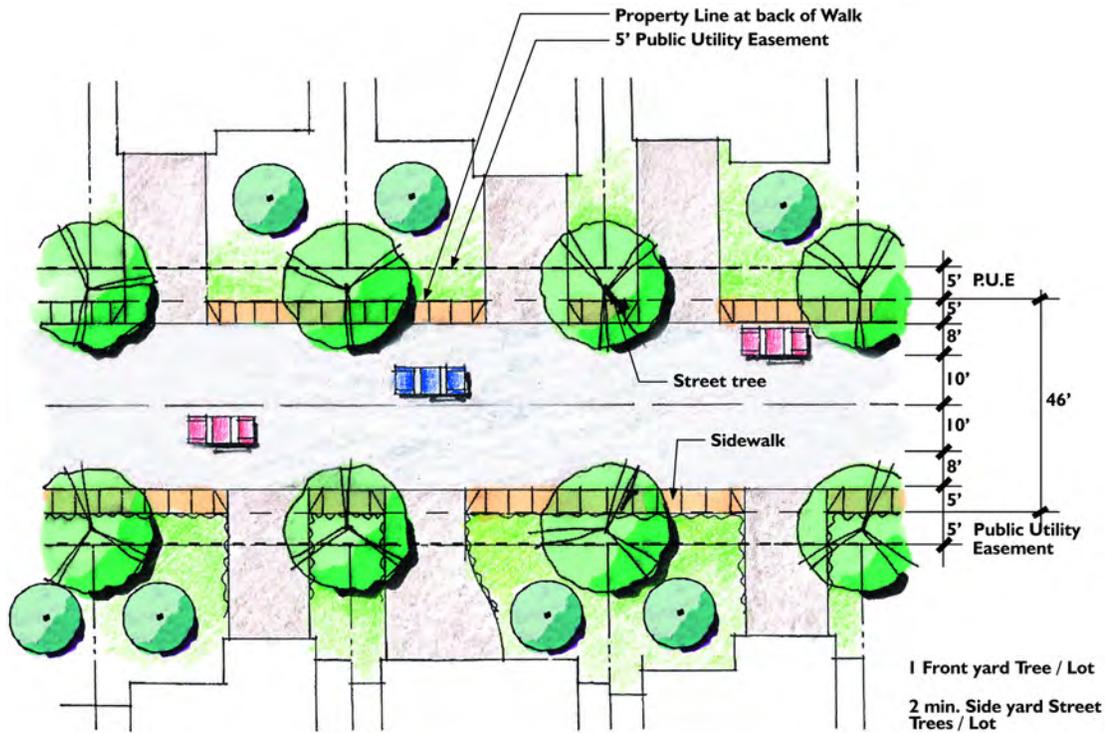


Exhibit 53
Local Street Plan

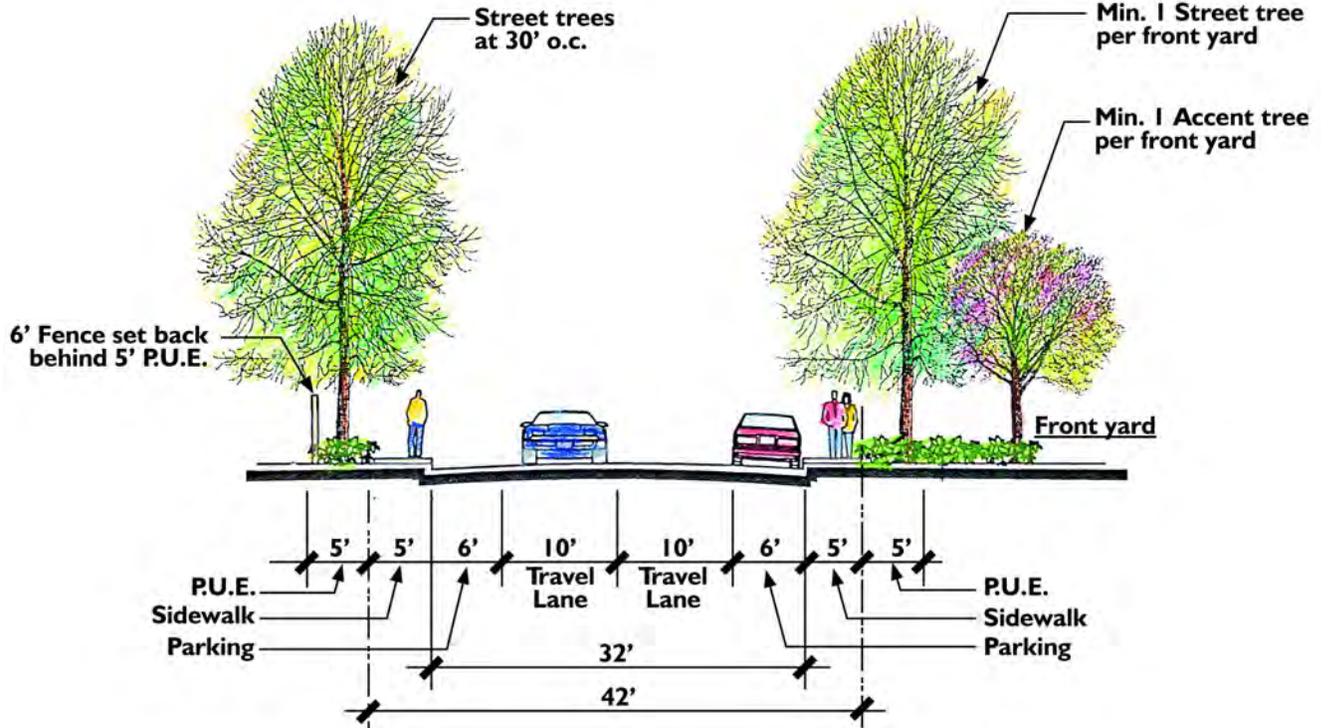


Exhibit 54
Minor Local Street/Cul-de-Sac Section

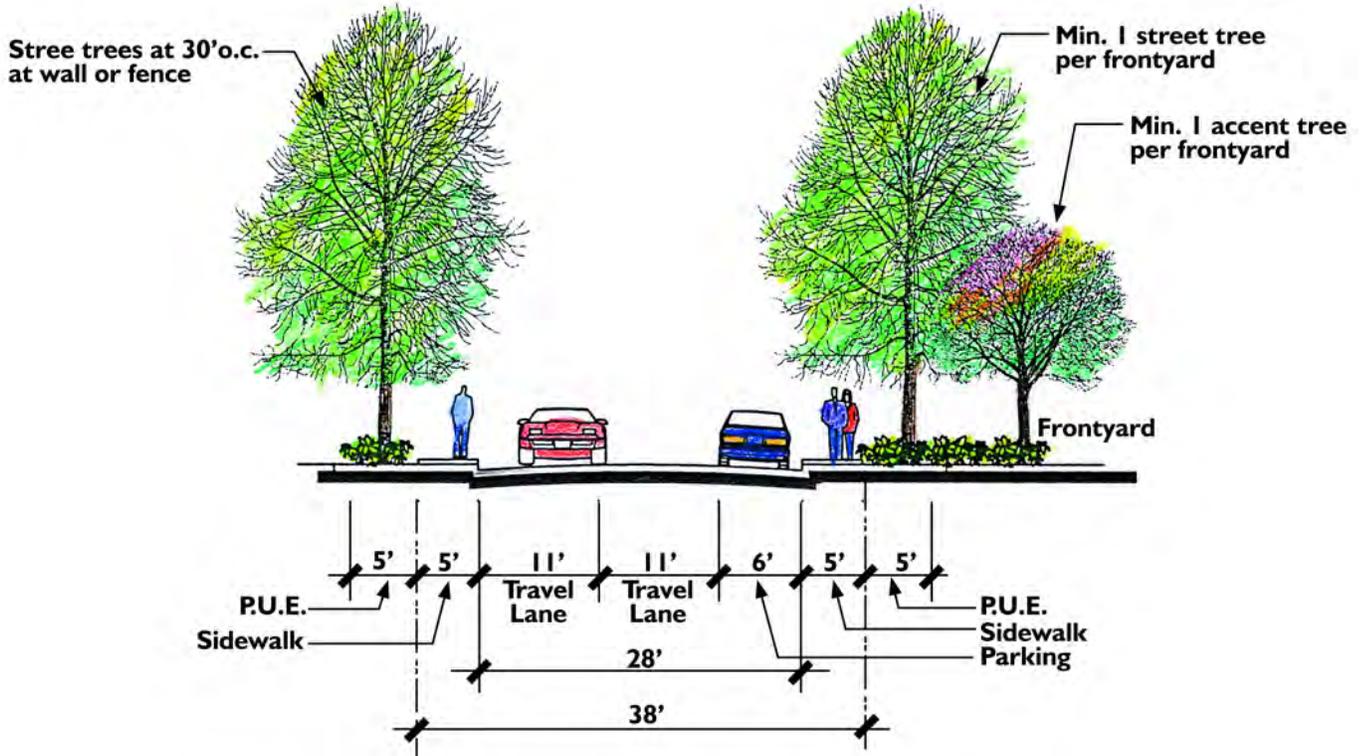


Exhibit 55
Local Single Loaded Street Section

SECTION 7. DESIGN GUIDELINES

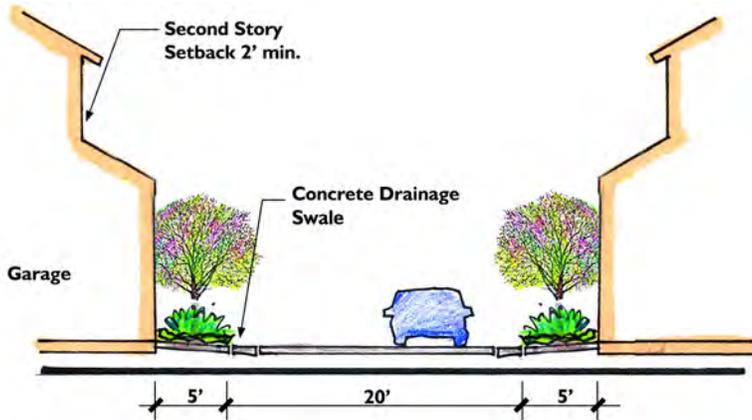


Exhibit 56
Alley Section

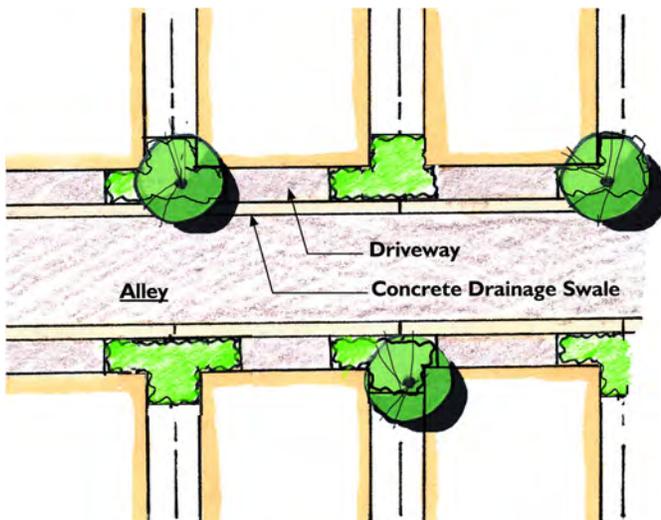


Exhibit 57
Alley Plan

aprons. Groundcover, shrubs, trees and vines will be incorporated to the extent possible. This type of road is illustrated in Exhibits 56-57 “Alley Section” and “Alley Plan.”

7.2.3 GATEWAYS, ENTRIES AND ARRIVALS

The gateways, entries and arrival points to the community and its neighborhoods create imagery that reinforces the character of the Specific Plan Area. The entries, like the roadways, have a hierarchy that enhances the area’s

legibility and helps orient people from the community scale to the neighborhood scale. The master plan for entries is illustrated in Exhibit 58 “Entries and Arrivals Diagram.”

7.2.3.1 Gateway

The primary gateway to the Specific Plan Area is located at the intersection of East Cypress Road and Jersey Island Road. Crossing the Contra Costa Canal, this intersection will be elevated over the levee, providing the first view to the Specific Plan Area. Monoliths will mark the entries at both East Cypress Road and Jersey Island Road, but the main focus will be on planting. Riparian tree clusters near the gateway will create a break from the lands to the west. The wide East Cypress Road right-of-way with its strong vertical line of trees will visually lead to the center of the Specific Plan Area as illustrated in Exhibit 59 “Conceptual East Cypress Gateway.” When Byron Highway is extended across Rock Slough levee to meet Bethel Island Road, it will form the second gateway to the area, and will receive enhanced landscape treatment along the widened Bethel Island Road frontage.

7.2.3.2 Community Entries

Community entries are the primary access points from the major arterials to the various project development areas. These entries will be treated with large landscape setbacks to create the sense of outdoor rooms, and to accommodate significant tree planting, entry walls and structures. Materials for the structures will include stone, stucco, metal and tile. Special paving will highlight the crossings and enhance the pedestrian areas.

Community entries from East Cypress to Planning Areas 1 and 3 will include a gatehouse element on a raised pad, and monolith passthroughs on either side of the pedestrian walks. The “outdoor room” will be further

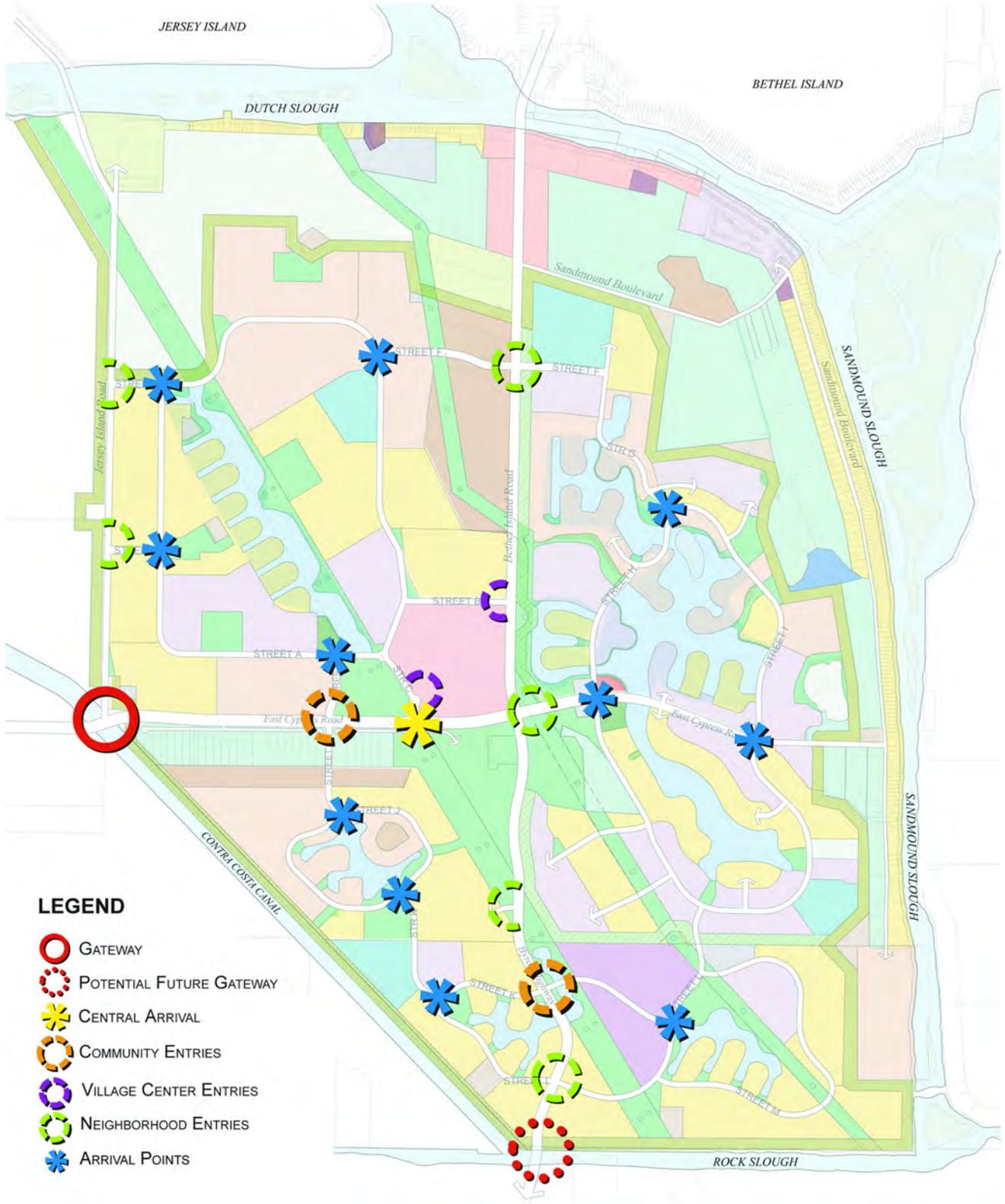


Exhibit 58
Entries and Arrivals Diagram



SECTION 7. DESIGN GUIDELINES

defined with a community accent wall, and generous planting. The central arrival point at East Cypress and the Village Center Gateway will incorporate a water element or water view. The open space area between the Planning Area 1 Community Entry and central arrival point will be landscaped as an integral part of the entry experience, with attractive groundcover and flowering shrubs, and with trees outside the limits of the archaeological site.

The community entry to Planning Areas 2 and 5 occurs at the east side of the East Cypress Road / Bethel Road intersection, and incorporates a gatehouse element, monoliths and pedestrian passthroughs, and a large landscaped area. From Bethel Island Road, community entries to Planning Area 4 will take advantage

of the lakes, as an entry feature or as a focal element. Low community accent walls will be used as an entry feature at the lake crossing. Community entries are illustrated in Exhibits 60-62 “Community Entry,” “Community Entries at East Cypress,” “Community Entries at Planning Area 4.”

7.2.3.3 Neighborhood Entries

Neighborhood entries are secondary entry points into the project development areas from East Cypress Road, Bethel Island Road or Jersey Island Road. They will have significant landscape setbacks, although smaller in scale than those of the community entries. The setbacks will accommodate tree planting, low community accent walls and monoliths. Special paving will highlight the

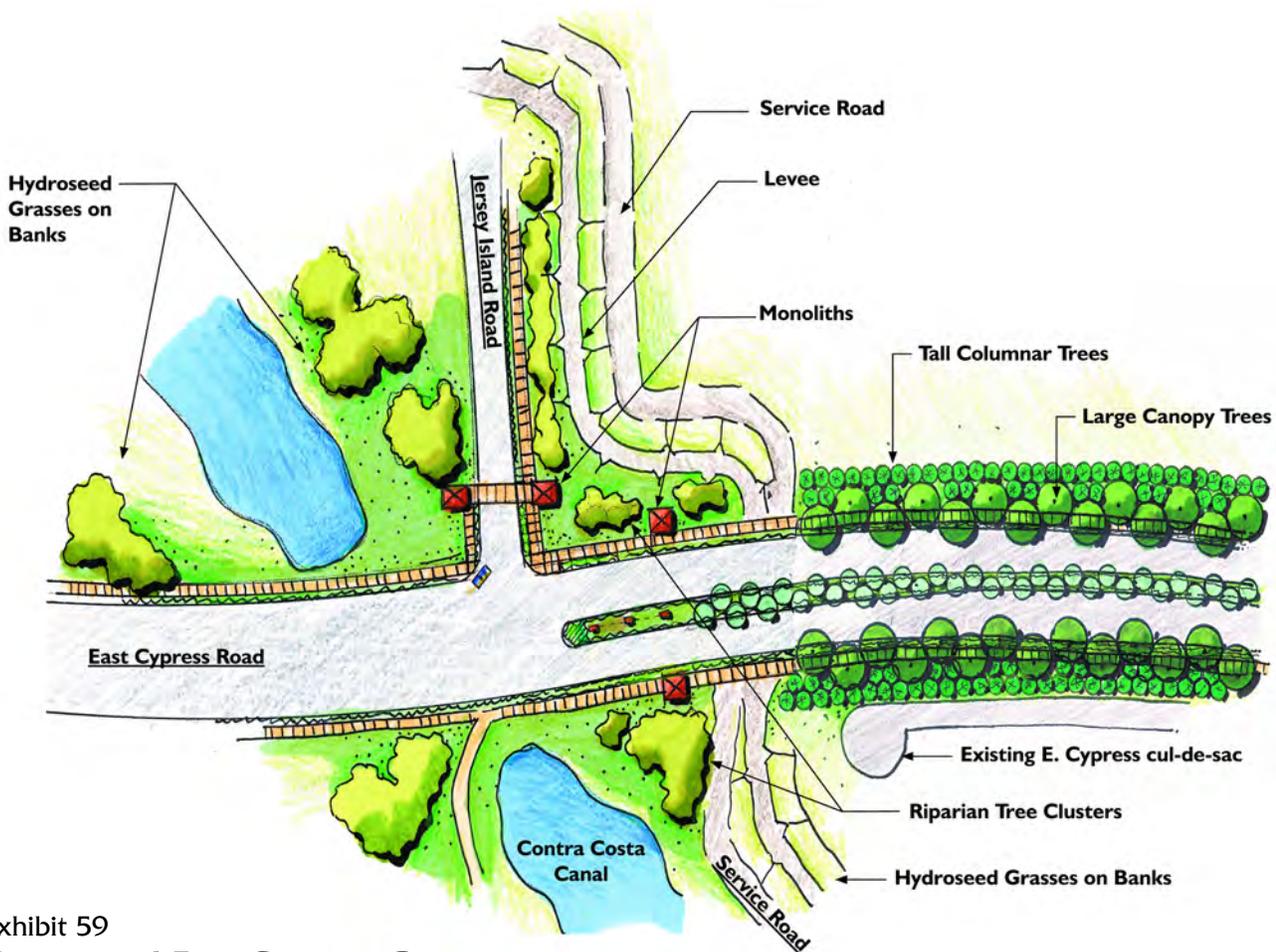


Exhibit 59
Conceptual East Cypress Gateway

crossings and enhance pedestrian areas. Materials will include stone, stucco, metal and tile as illustrated in Exhibits 63-64 “Neighborhood Entry Plan at Jersey Island Road” and “Neighborhood Entry Elevation (Typ).”

7.2.3.4 Arrival Points

The community and neighborhood entries lead to arrival points which are focal points of the neighborhoods. In most cases, the arrival point occurs where the Project Collector Road intersects with the loop road. In these instances, there will typically be a rotary with a landscaped center, or a view to a community amenity. The view at the arrival point typically focuses on water, parkland, or open space. Vertical elements will frame views and mark local neighborhoods. A typical neighborhood arrival point and associated elements are illustrated in Exhibits 65-67 “Neighborhood Arrival Elevation,” “Neighborhood Arrival Plan,” and “Neighborhood Icon.”

7.2.4 PARKS, OPEN SPACE, LAKES AND TRAILS

A comprehensive network of parks, trails and open space links all portions of the Specific Plan Area. See Exhibit 70 “Open Space System Diagram”; and Exhibit 71 “Trails Diagram.”

Community parks serve the active sports needs of the community. The community parks provided within the Specific Plan Area will be designed to maximize usable playing fields and sports courts, including soccer/football, baseball, basketball and tennis. Adequate parking to accommodate the proposed uses will be provided. The community parks will be landscaped to provide shaded and wind-protected areas for picnic and passive recreation in addition to sports areas. The community parks abut open space easements and levees containing multi-use trails, connecting them to the trail system and the larger community. The interim levee in the community park southeast of the East Cypress Road/Bethel Island Road intersection may be removed when it is no longer needed for flood control, expanding the park’s useable space. The community park to the southwest of the East Cypress Road/Bethel Island Road intersection is adjacent to a significant archaeological site, the Hotchkiss Mound. The archaeological site, designated in the Land Use Plan as open space, will have controlled access only, and interpretive signage will be provided.

Neighborhood parks serve the informal recreational needs of the neighborhoods. Neighborhood parks are distributed throughout the Specific Plan area. They are linked to the



Exhibit 60
Community Entry

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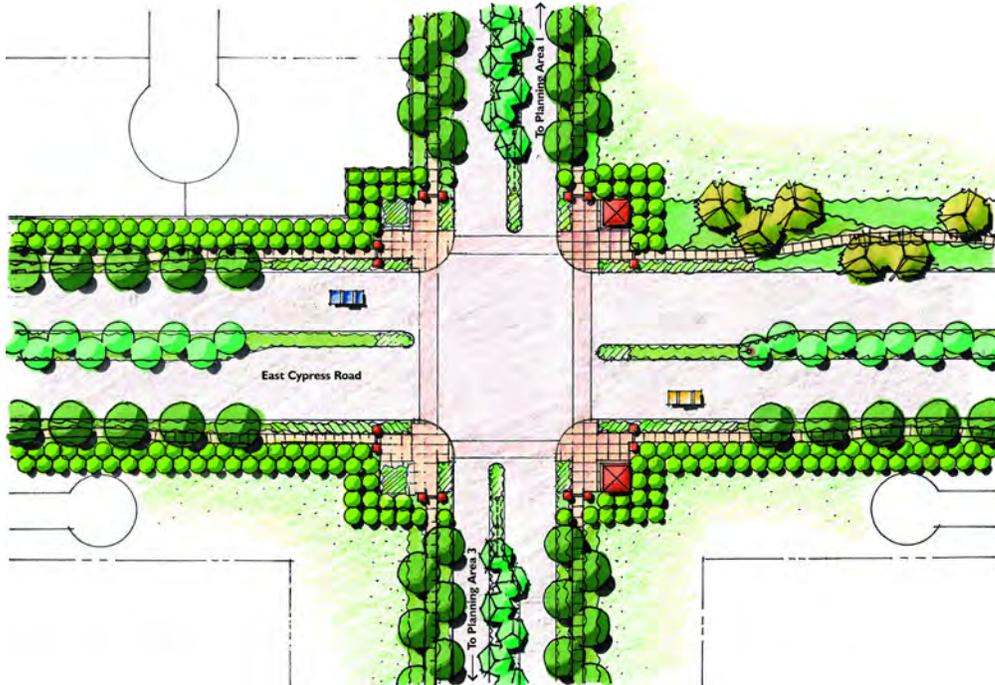


Exhibit 61
Community Entries at East Cypress



Exhibit 62
Community Entries at Planning Area 4

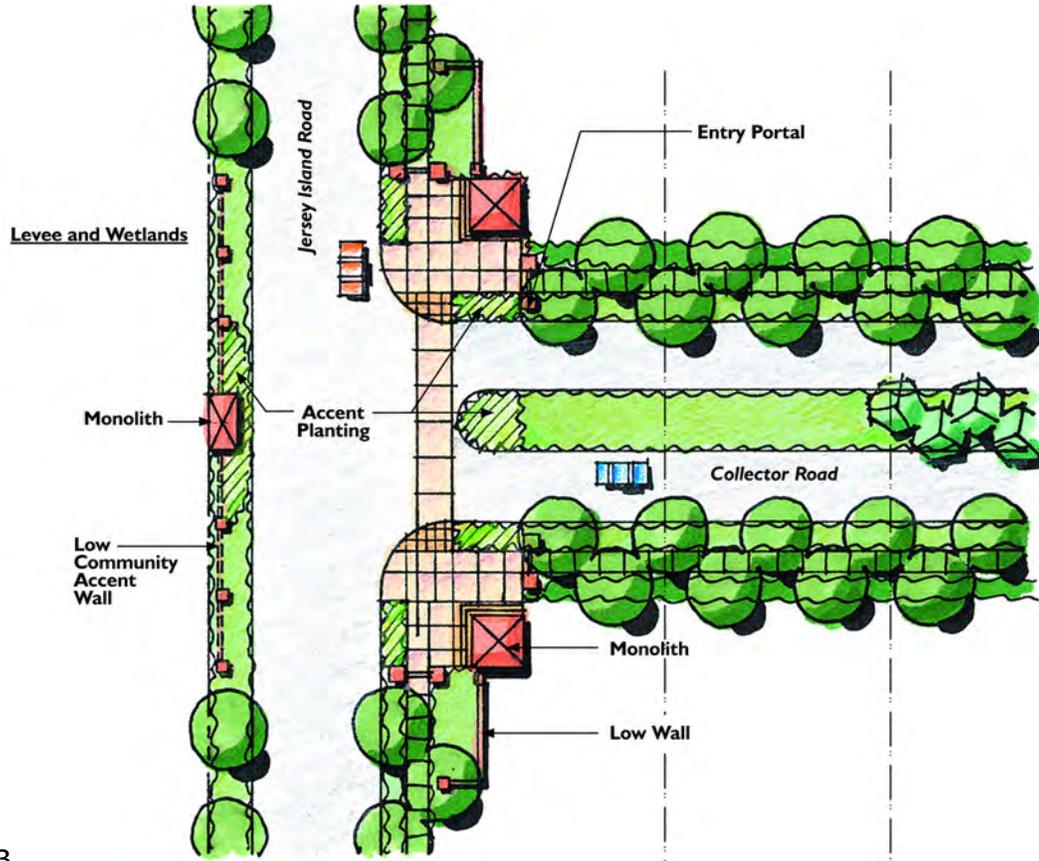


Exhibit 63
Neighborhood Entry Plan at Jersey Island Road

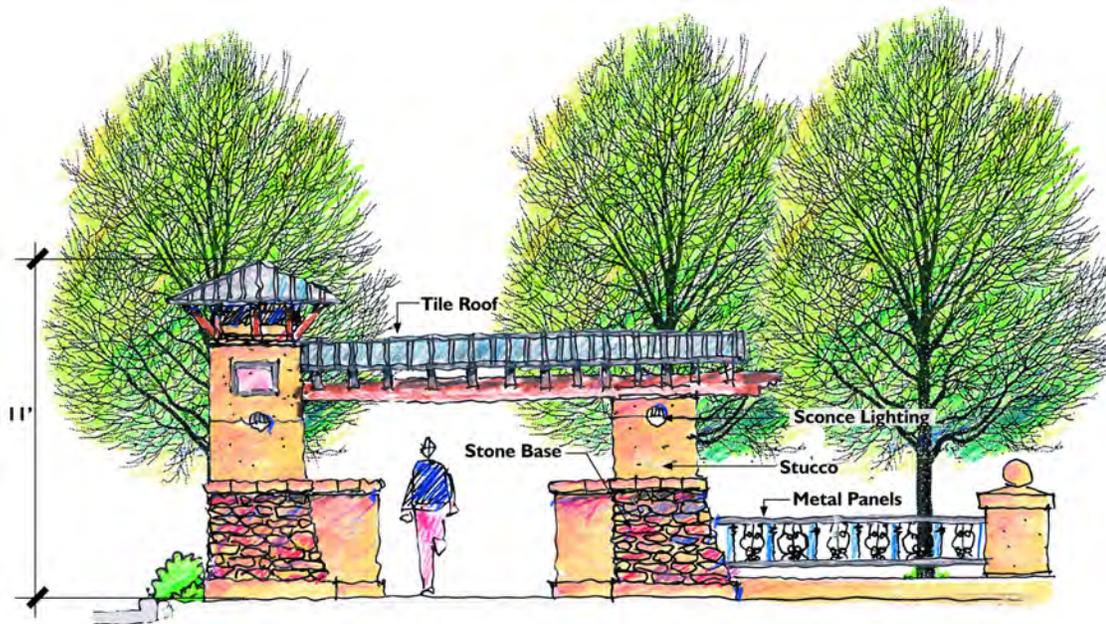


Exhibit 64
Neighborhood Entry Elevation (Typ.)

SECTION 7. DESIGN GUIDELINES

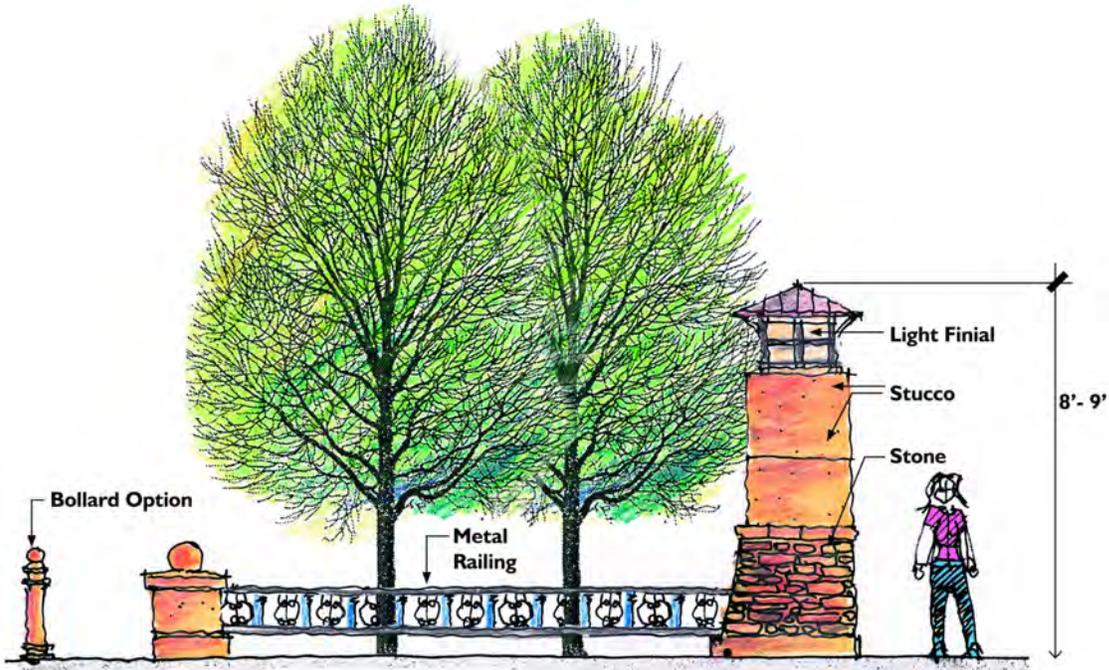


Exhibit 65
Neighborhood Arrival Elevation

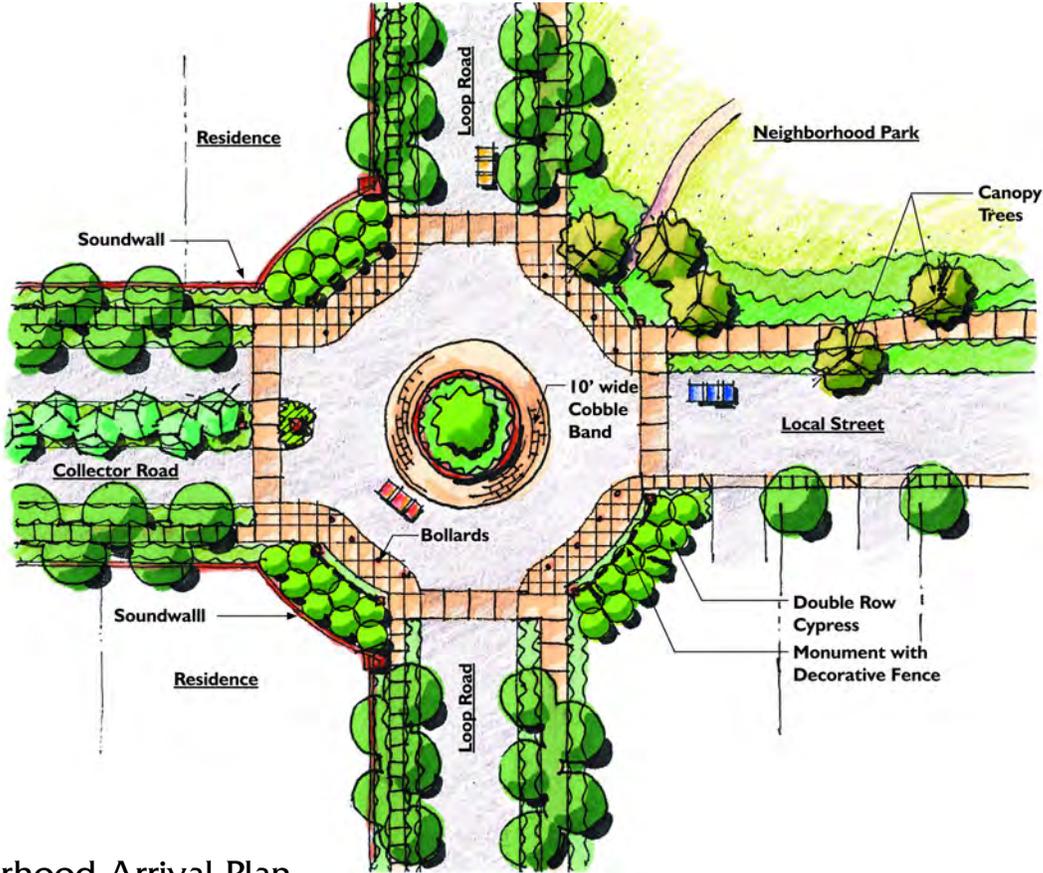


Exhibit 66
Neighborhood Arrival Plan

trail system, making them easily and safely accessible by foot or bicycle. Open lawn areas for informal recreation and picnic, deciduous canopy trees for summer shade and winter sun, and shrub planting for wind protection are all desirable landscape elements. Small court recreation facilities and playgrounds are appropriate elements. Each park will be designed to take advantage of its unique location and relation to surroundings. See Exhibit 68 “Park Entry.”

Many of the neighborhood parks are situated to take advantage of the community’s lakes, either directly adjacent to the lakefront, or with a clear view to the water. A variety of lake edge treatments may occur at park frontages, including trail, overlooks, or water steps. See Exhibit 69 “Park Lake Edge,” and Exhibit 72-73 “Lake Overlook,” and “Trail Lake Edge.”

The lakes will be the central feature characterizing the East Cypress Corridor. They will be visual and recreational amenities. Edge treatments will range from natural wetland transition areas to hard edges. Most of the lake frontage will be publicly accessible through trail, park or open space interface. See Exhibit 74 “Narrow Lake Trail.”

Two open space easements extend through the site on a northwest to southeast diagonal.

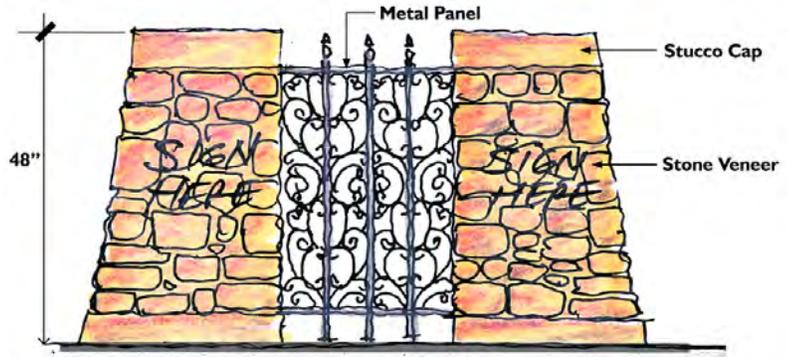


Exhibit 67
Neighborhood Icon



Exhibit 68
Park Entry

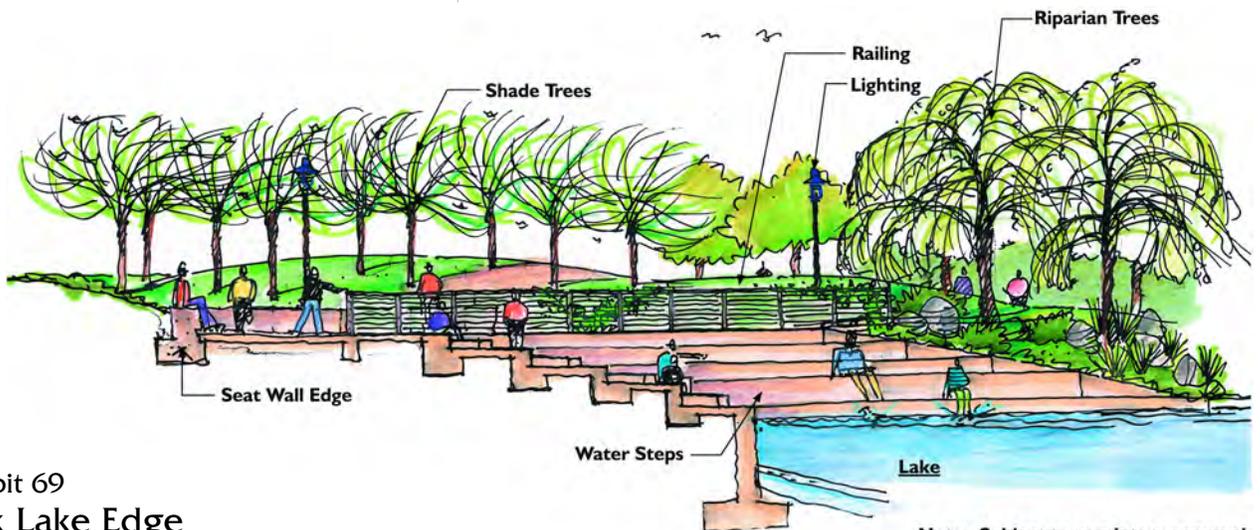


Exhibit 69
Park Lake Edge

Note: Subject to regulatory approval

SECTION 7. DESIGN GUIDELINES

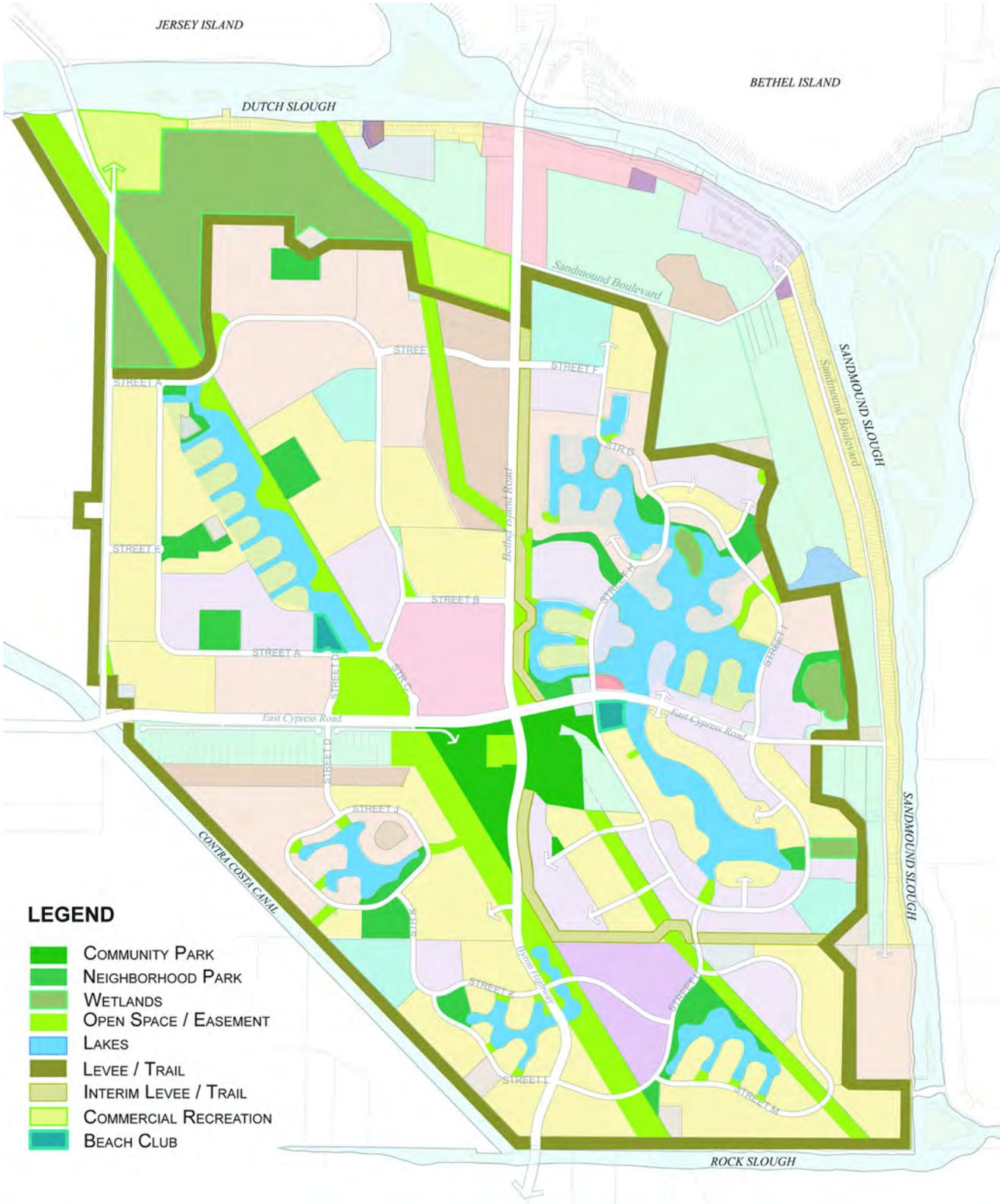


Exhibit 70
Open Space System Diagram



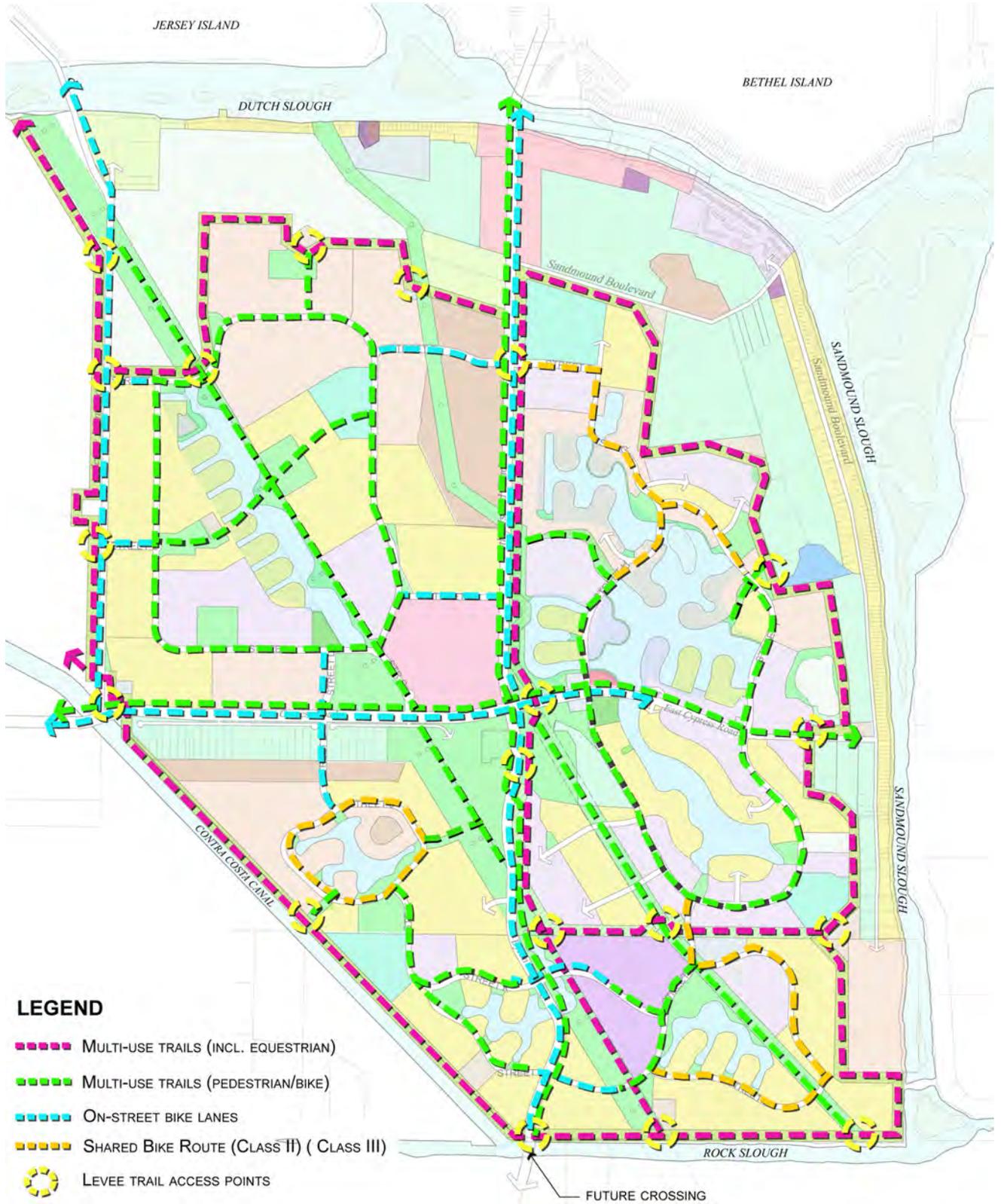


Exhibit 71
Trails Diagram



SECTION 7. DESIGN GUIDELINES



Exhibit 72
Lake Overlook



Exhibit 73
Trail Lake Edge

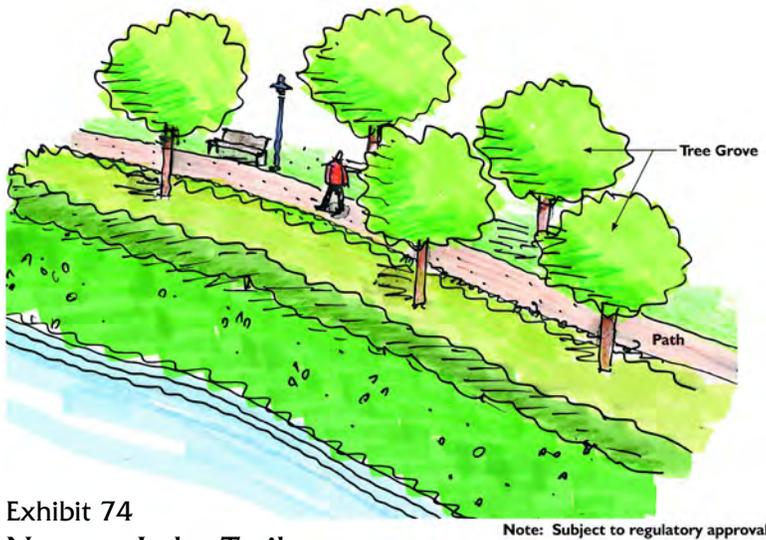


Exhibit 74
Narrow Lake Trail

These two power line easements will contain multi-use (in some cases including equestrian) trails. These easements will have an open character, planted primarily with low growing trees, and seasonal grasses, transitioning to native wetland planting at lake edges. Where roads cross the open space easements, trees will be planted in an orchard pattern, recalling the agricultural heritage of the area and providing spring bloom, summer shade, and fall color. Away from the road crossings, trees will be planted in informal clusters to shade the trail and provide visual interest. Trees with riparian forms will be used near lake edges. The groundplane will be grasses, for ease of maintenance. Generally, the open space easements will be irrigated for plant establishment only. Some community and neighborhood parks are adjacent to open space easements, effectively expanding the park beyond its designated boundaries. In some instances, the open space may be incorporated into the park design, providing additional active recreation space (such as practice soccer fields). In other instances, the neighborhood edges of the park with lawn, trees and more formal planting areas will transition to the more informal, native grasses of the open space easement. See Exhibits 75-76 “Trail through Open Space Easement Section” and “Trail through Open Space Easement Plan.”

Levee trails form a significant part of the trail system, accommodating equestrian as well as pedestrian and bicycle use. To protect the structural integrity of the levees, planting will be restricted to a limited palette of low growing groundcovers and grasses. Seasonal color will be emphasized within this palette. Access to the levee trails will be limited to specific points, to prevent erosion problems that result from informal trails. To the extent possible, the levee trail access points will make use of levee maintenance access roads. These access points will be ADA compliant. When interim levees become redundant due to completion of

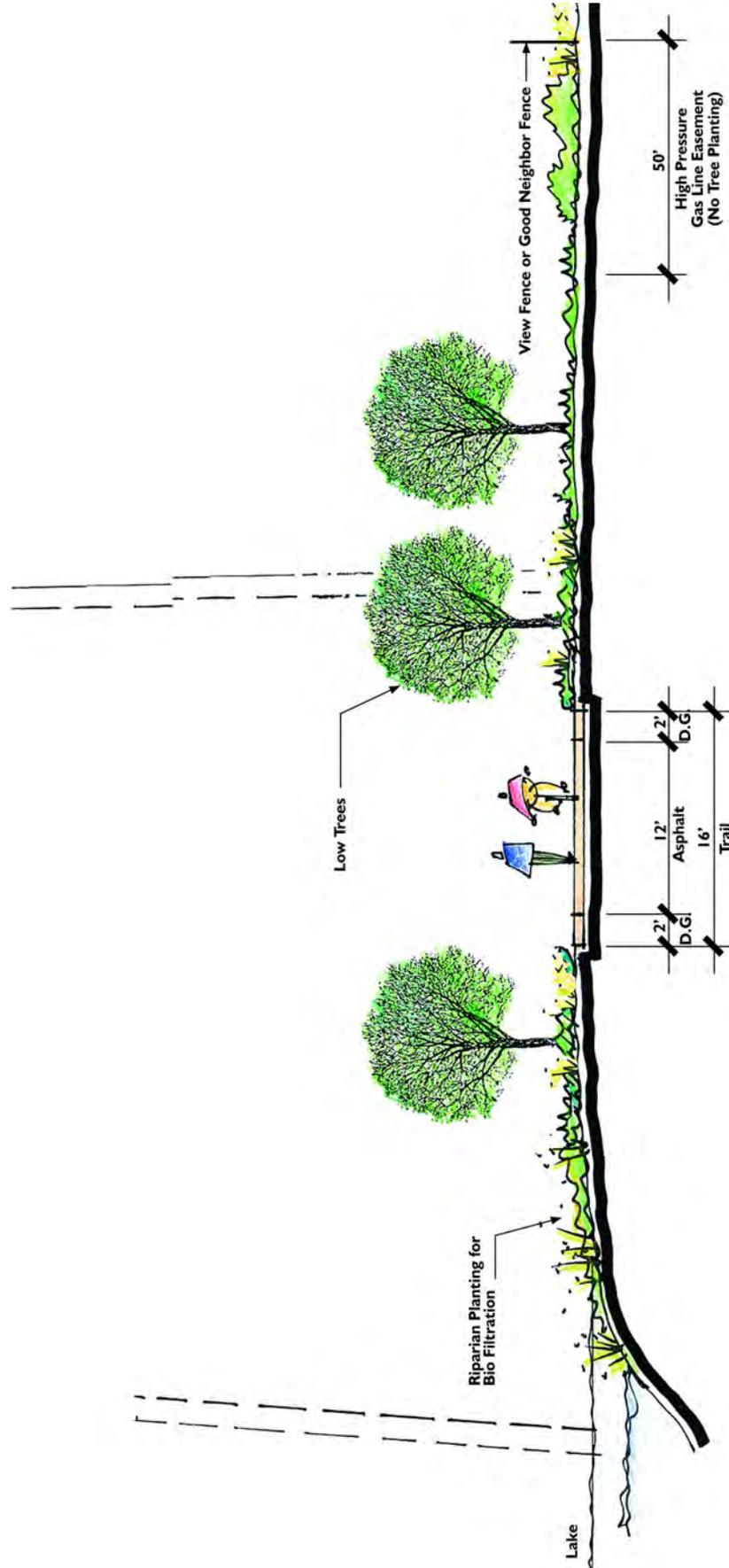


Exhibit 75
Trail through Open Space Easement Section



Exhibit 76
Trail through Open Space Easement Plan

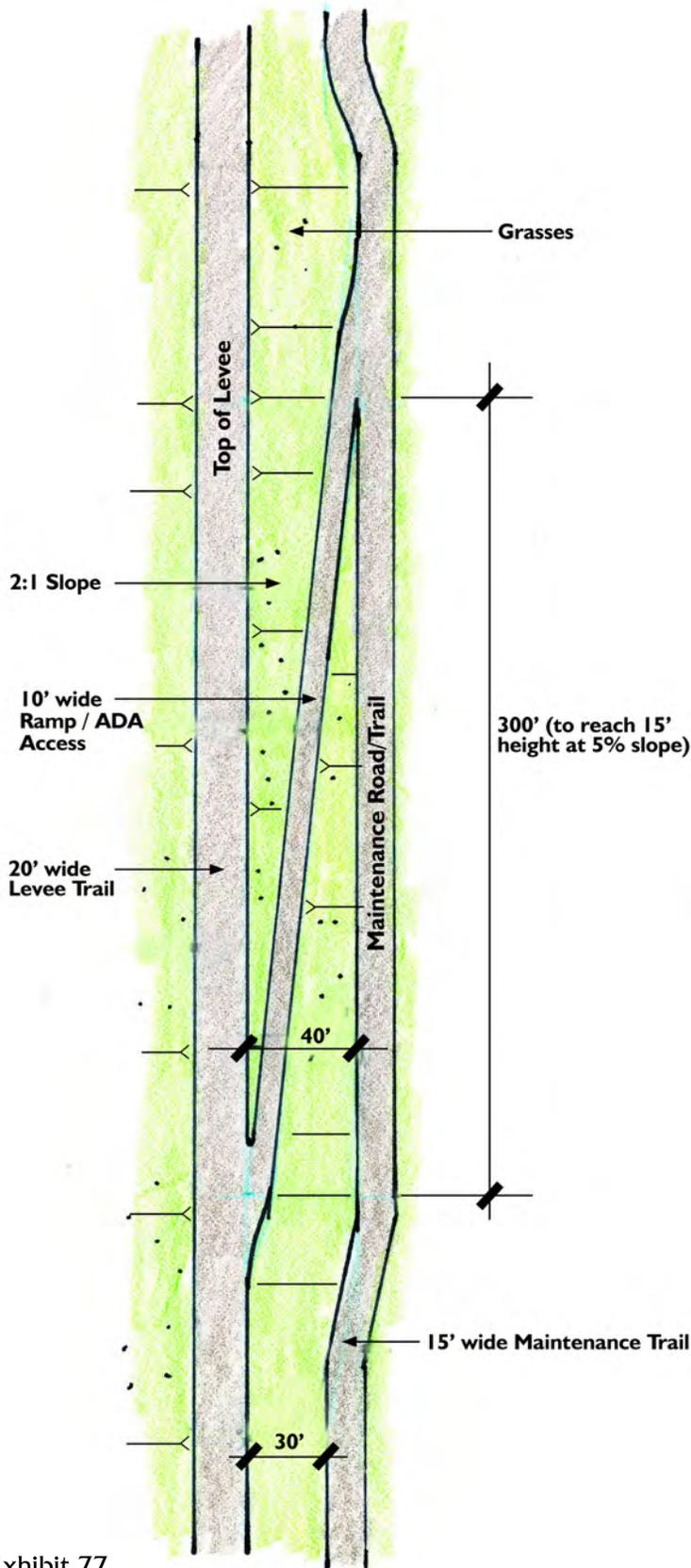


Exhibit 77
Levee Trail Access Plan

perimeter levees, those levees that are no longer necessary for flood protection may be planted with trees, shrubs and other landscaping to soften their appearance, create visual interest and provide shade. See Exhibits 77-79 “Levee Trail Access Plan,” “Trail on Engineered Levee Section” and “Landscaped Trail on Interim Levee Section.”

Wetlands comprise another important open space element in the Specific Plan Area. These areas will be visual amenities as well as habitat. Because wetlands are environmentally sensitive buffers and setbacks are required. Typically, where wetlands are adjacent to homes, rear or side yard view fencing or hogwire fencing will provide the buffer. Where wetlands are adjacent to open space or publicly accessible areas, an average minimum setback of 25’ will be required to any trail or access point. The setback may be greater or lesser, depending on the level of environmental sensitivity at the wetland edge. Protected from intrusion by hogwire fencing, barrier planting and/or berming, typically, the wetlands will be clearly visible from many open space and levee trails, and in some instances, in parks or from the roadway. Interpretive signage will be incorporated where appropriate. See Exhibits 80-82 “Wetland Interface - Trail at Wetland Section,” “Wetland Interface - Northern Engineered Levee Trail with Wetland Overlook,” and “Wetland Interface - Trail at Wetland Plan.”

The trail system links neighborhoods, connects to destinations such as parks and schools, and ties to regional multi-use trail networks. Trail connections at ends of cul-de-sacs allow for pedestrian access from neighborhoods to trails at open space areas, and to trails or sidewalks along arterials and collectors. See Exhibits 83-84 “Neighborhood Trail Connection at Cul-de-Sac Plan,” and “Neighborhood Connection to Open Space Trail Plan.”

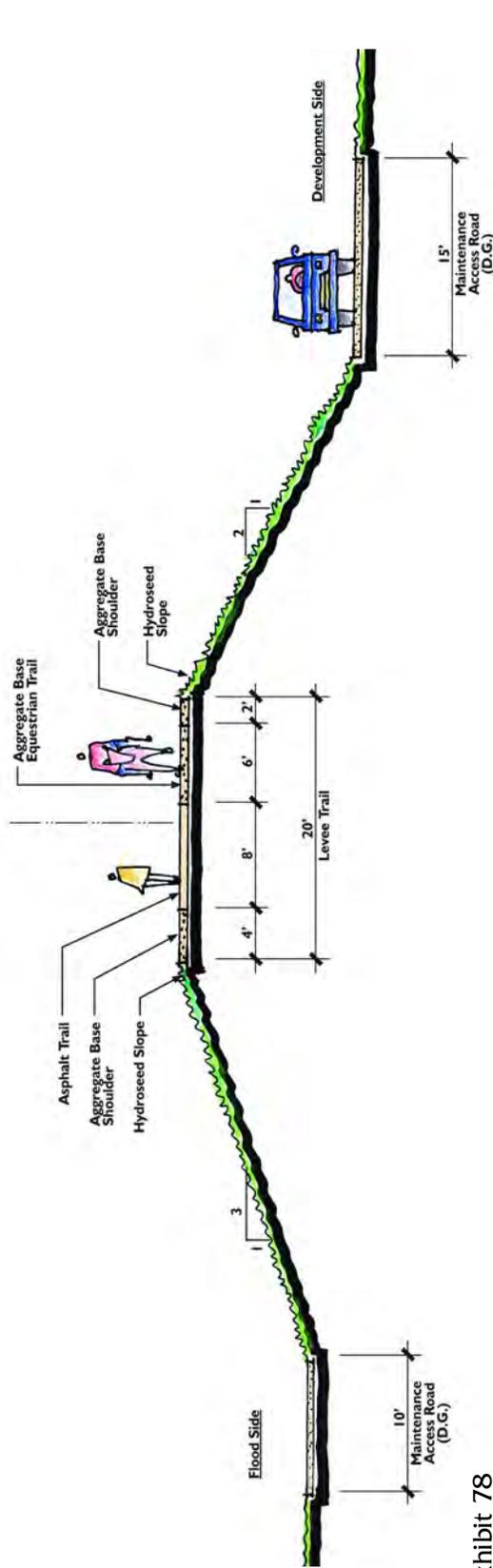


Exhibit 78
Trail on Engineered Levee Section

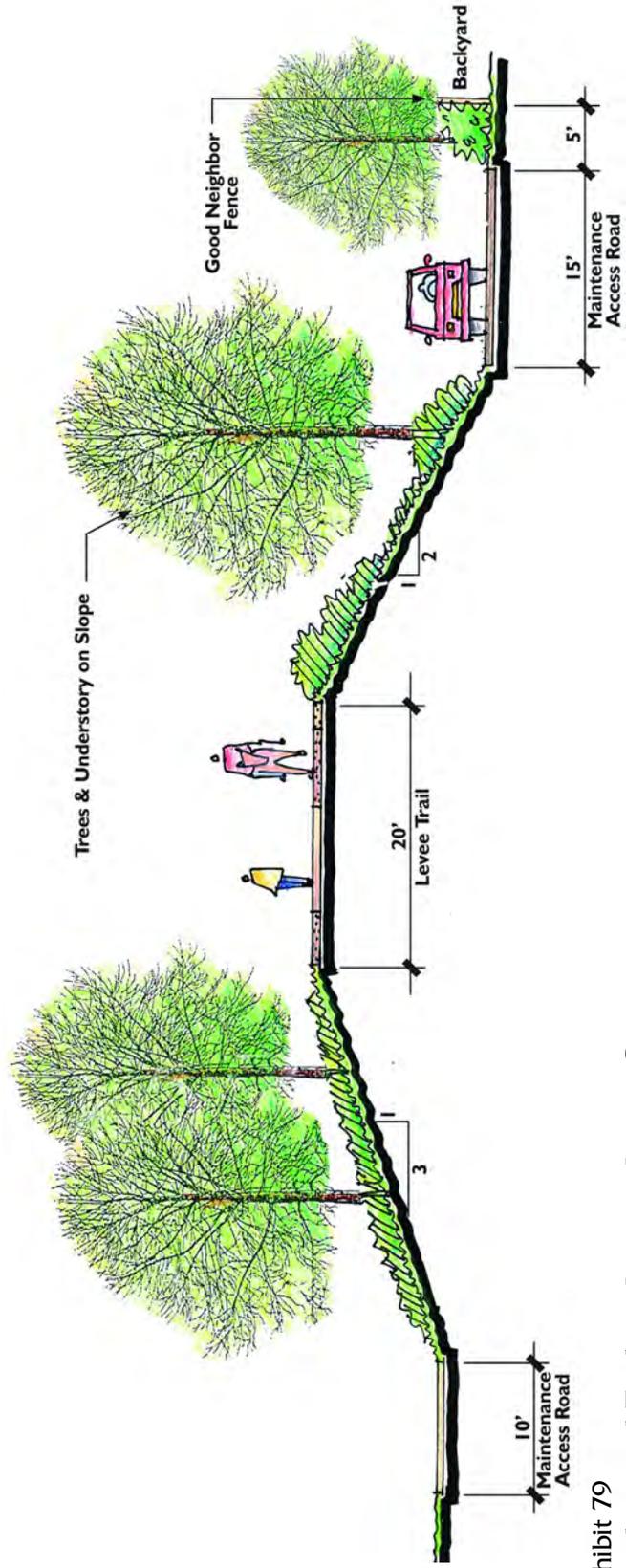


Exhibit 79
Landscaped Trail on Interim Levee Section

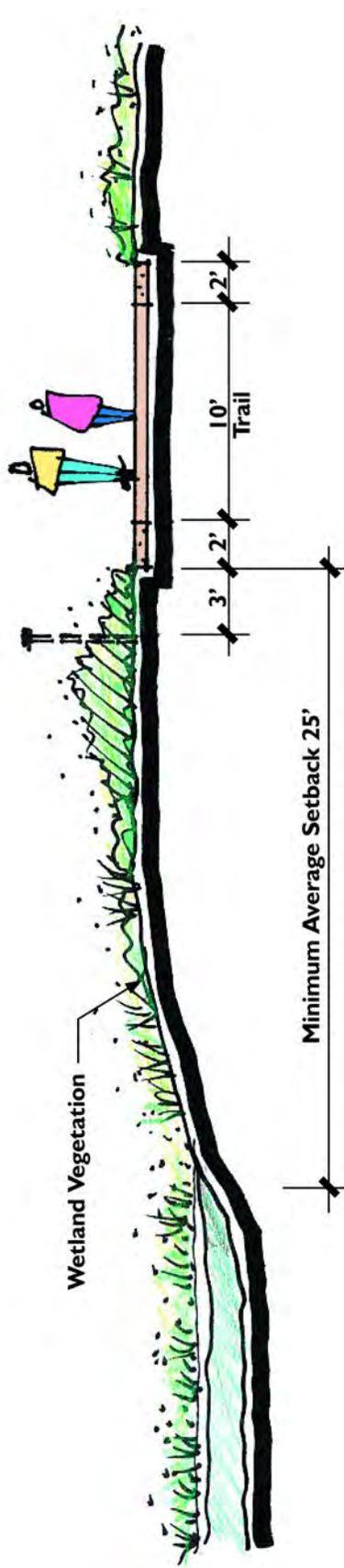


Exhibit 80
Wetland Interface - Trail at Wetland Section

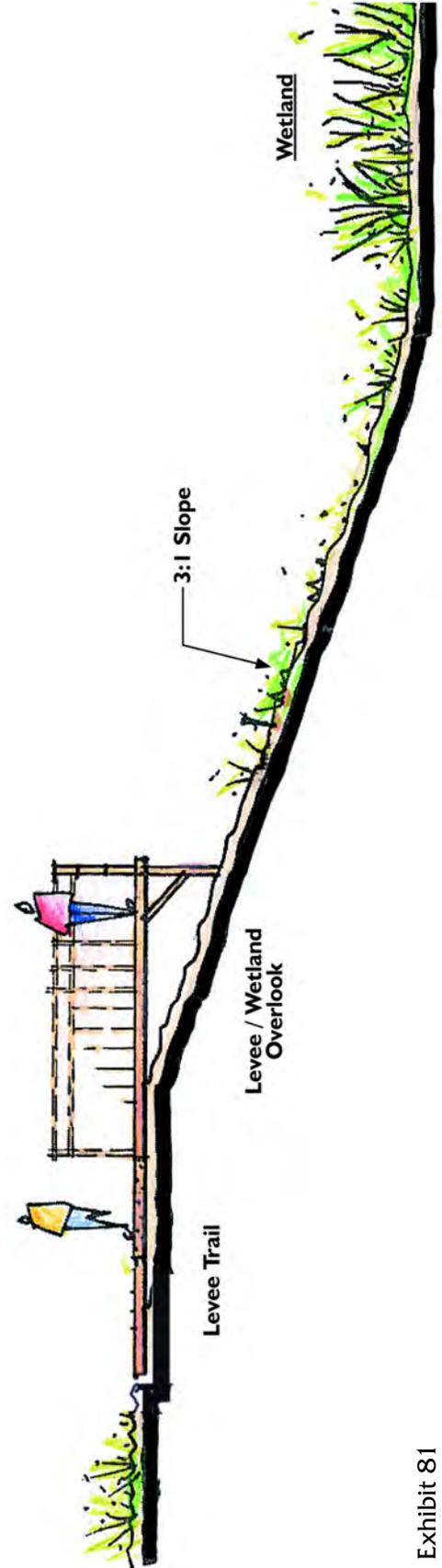


Exhibit 81
Wetland Interface - Northern Engineered Levee Trail with Wetland Overlook

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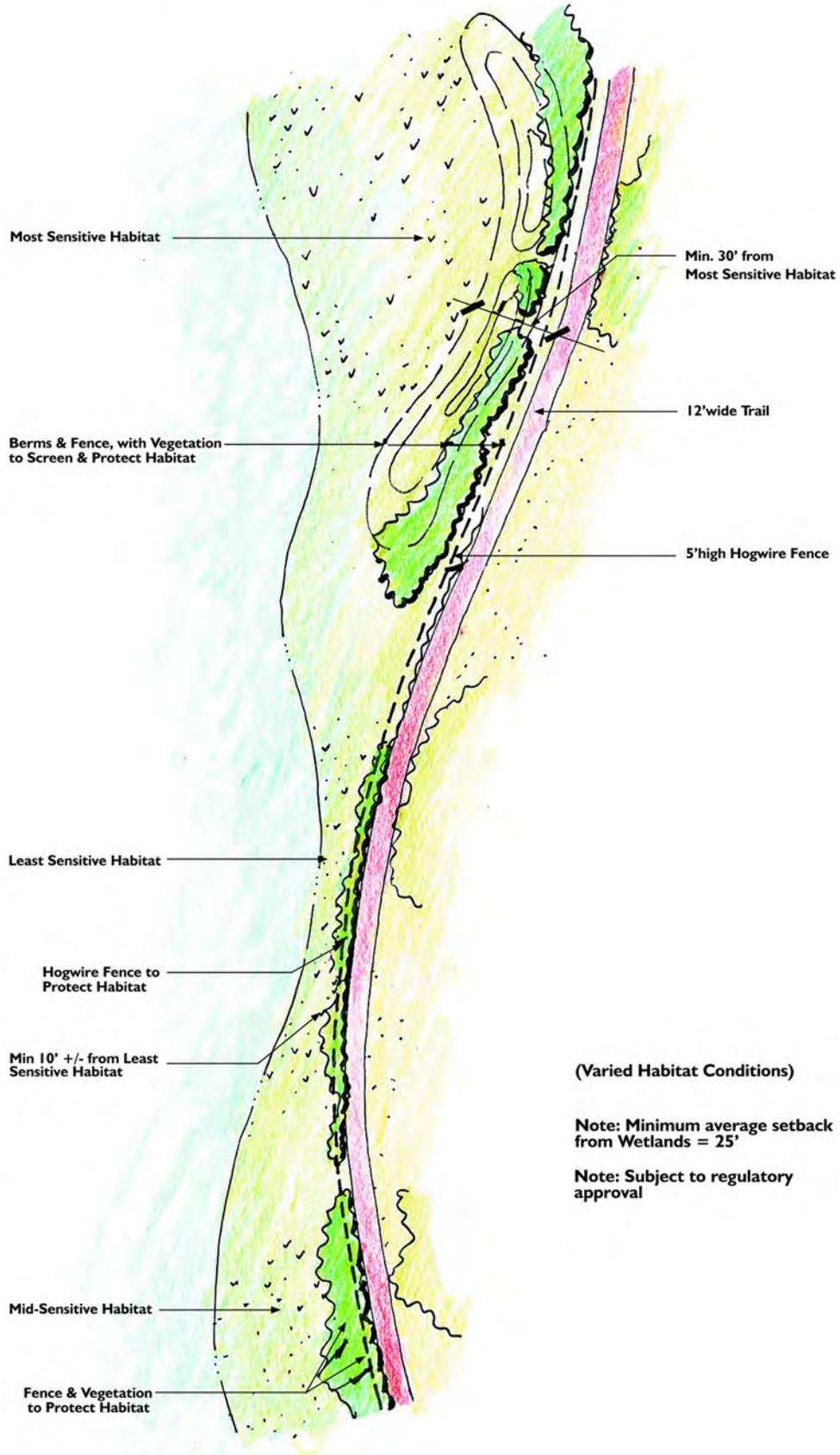


Exhibit 82
Wetland Interface - Trail at Wetland Plan

Multi-use trails for bicycles and pedestrians should have a minimum width of 8', with two-foot wide shoulders. A minimum vertical clearance of 8' is required for trails intended for bicycle use. Trails intended for equestrian use should have a minimum clear width of 8', including a minimum of 4' of tread width. Vertical clearance for equestrian trails should be a minimum of 10'.

7.2.5 BUFFERS AND SCREENING

Various land uses within the plan area, such as gas wells or infrastructure such as pump stations, will require screening from surrounding uses. Sensitive sites in the plan area will require buffering for protection from intrusion. See Exhibit 85 "Screening and Buffer Diagram."

Sensitive sites include wetlands and archaeological sites. Wetlands, as discussed in the previous section, will be buffered by fencing, planting, and/or berming. An average minimum 25' setback from wetlands will be maintained, depending on sensitivity of habitat. Where habitat values are lesser, trail alignment may be closer and interpretive signage may be provided.

Sensitive archaeological sites will be capped for their protection. A water permeable warning barrier will be overlaid with an earth layer of sufficient depth to prevent disturbance. Planting may be allowed over such sites. Any significant archaeological sites that are not capped will be protected by view fencing and signage, to prevent disturbance. See Exhibit 86 "Archaeological Mound Cap."

Water tanks needed for backup water supply to the area will be located in Planning Area 4, just south of the interim levee. These tanks, anticipated to be 30' high, will be set back from road and trail, and screened by a wall and a densely spaced double row of evergreen trees such as

redwoods. See Exhibit 87 "Water Tank Screening Plan."

A number of gas drilling sites exist within the Specific Plan Area. These sites consist of operating equipment and staging areas which require truck access for maintenance. Gas well sites which are outside the project levees of Planning Areas 1-5 will be secured with chain link fence-

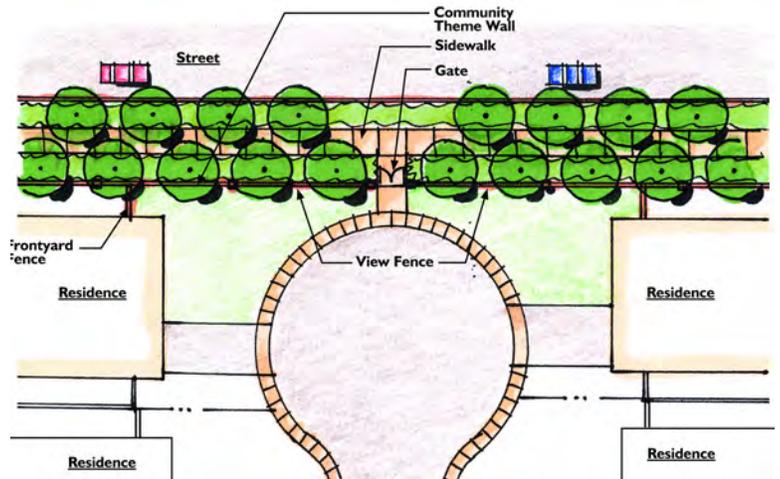


Exhibit 83
Neighborhood Trail Connection at Cul-de-Sac Plan

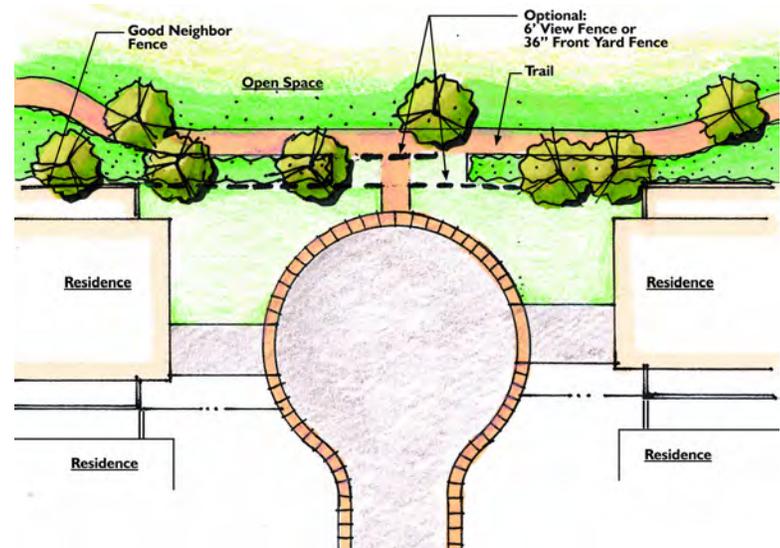


Exhibit 84
Neighborhood Connection to Open Space Trail Plan

ing, and screened as appropriate to adjacent uses. Within the levees, the gas well sites will be screened from surrounding areas by walls, fencing, berming and/or landscaping, as appropriate to the adjacent land uses. Security for the well sites will be provided as required. See Exhibits 88-89 “Gas Well Screening Section,” and “Gas Well Screening Plan.”

7.2.6 WALLS AND FENCES

The following table indicates locations for the various types of walls and fences in the Specific Plan Area. In some areas, solid walls will alternate with view fences depending on the adjacent conditions. Where homes back onto lakes, view fencing will be used if fencing is desired. The wall and fence concept is described in the table below and illustrated in Exhibit 90 “Walls & Fences Diagram.” The walls and fences listed in the table are illustrated in Exhibits 91 and 92 “Walls and Fences.”

7.2.7 SITE FURNISHINGS

Site furnishings add to the character of the neighborhoods within the Specific Plan Area. A discrete family of site furnishings shall be used in each planning area to create an identity for each area. Site furnishings shall be consistent within each planning area and within the Village Center, but vary between those areas. Site furnishings include such elements as bus shelters, newspaper racks, benches, drinking fountains, trash receptacles and mailboxes. Materials and furnishings should be of durable, high quality materials such as metal, stone, masonry, concrete, tile and stucco.

Transit stops shall include a permanent, covered structure. The structures should incorporate architectural design elements of the surrounding neighborhood.

Condition	Fence Type	At Cul-de-Sac or Single Loaded Frontage
Residential side or rear yard at arterial, project collector or Jersey Island Road	Community theme wall (masonry)	Metal view fence with gate or opening, unless wall is necessary for sound attenuation
Residential side or rear yards abutting loop road, local street, park or school, or other side or rear yards	Good neighbor fence: Redwood or cedar fence with overlapping fence boards and wood-encased tubular steel posts in concrete footing, per City approved detail in Appendix B	Where adjacent to street or to open space with trail, the fence line should be discontinuous to allow for pedestrian connections through. Bollards, low walls or view fence segments may be used to prevent vehicular access.
Residential rear yard at levee, or open space	Metal view fence or good neighbor fence (Good neighbor fence per City approved detail in Appendix B)	Open
Residential rear yard at lake	Metal view fence (optional)	N/A
High visibility areas such as water crossings, neighborhood entries or park entries	Community accent wall	N/A
At wetlands	5’ high hogwire fence with 2” grid	N/A

Table 5
Walls and Fences Table

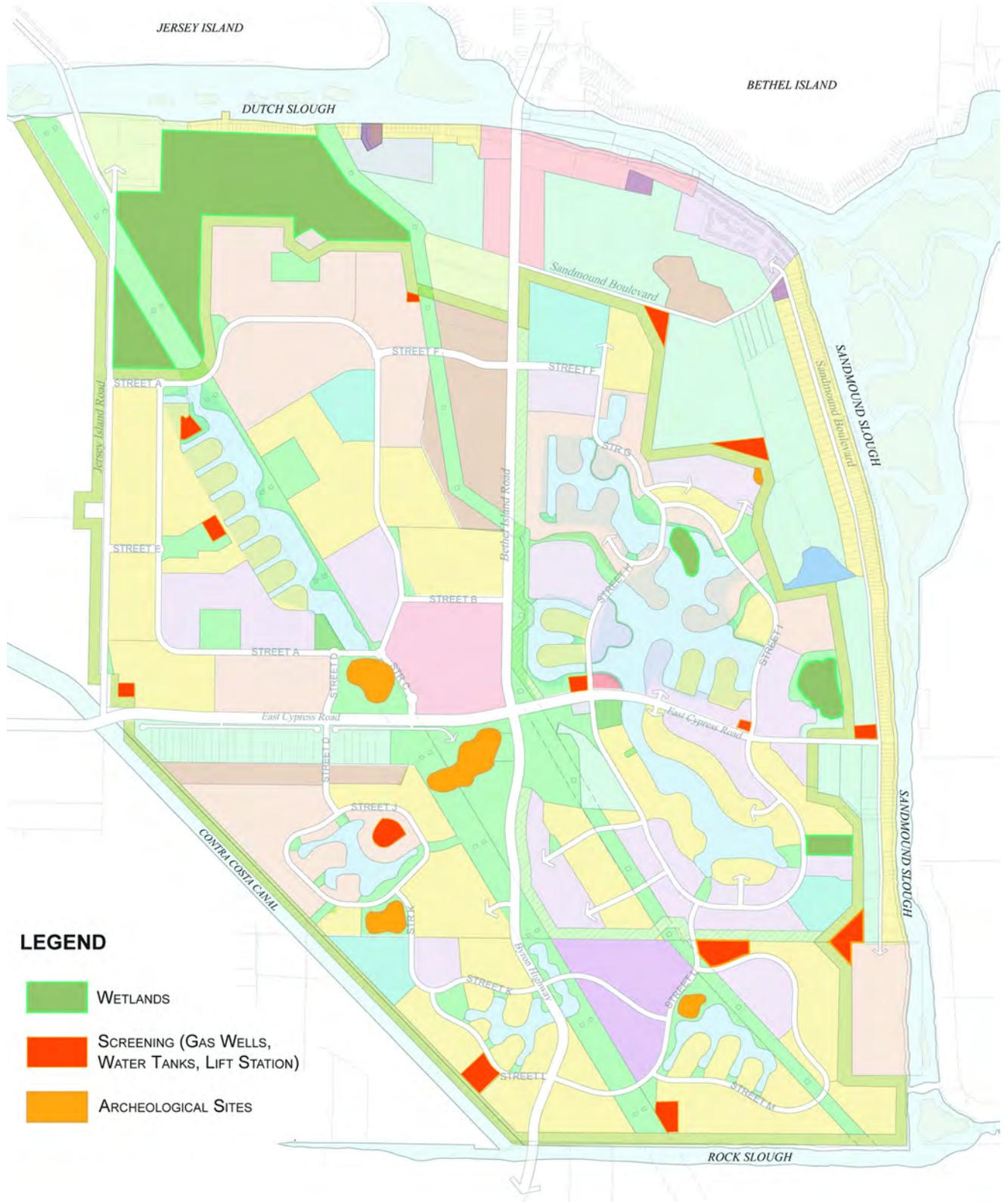


Exhibit 85
Screening and Buffer Diagram



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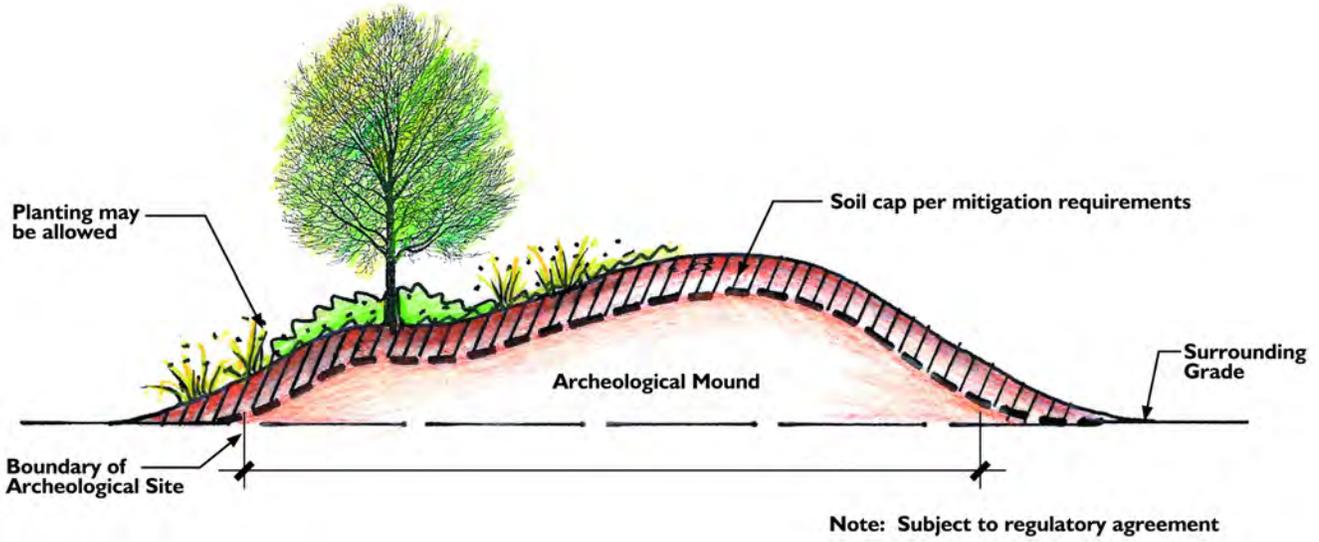


Exhibit 86
Archeological Mound Cap

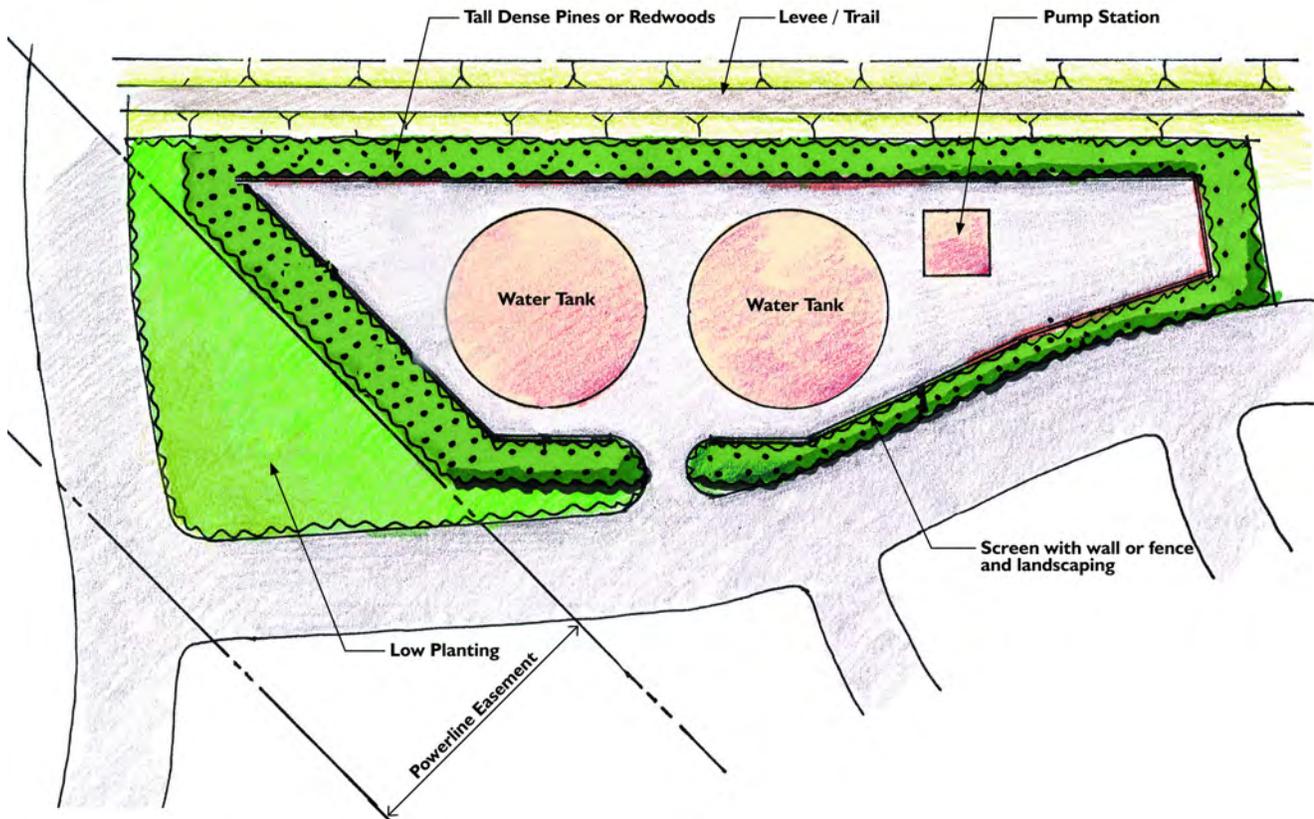


Exhibit 87
Water Tank Screening Plan

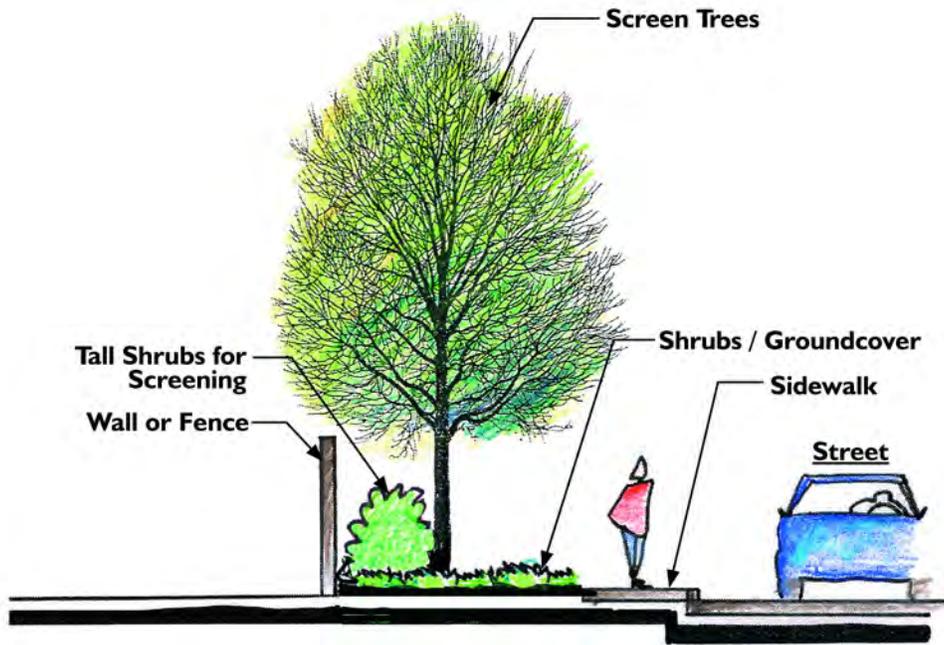


Exhibit 88
Gas Well Screening Section

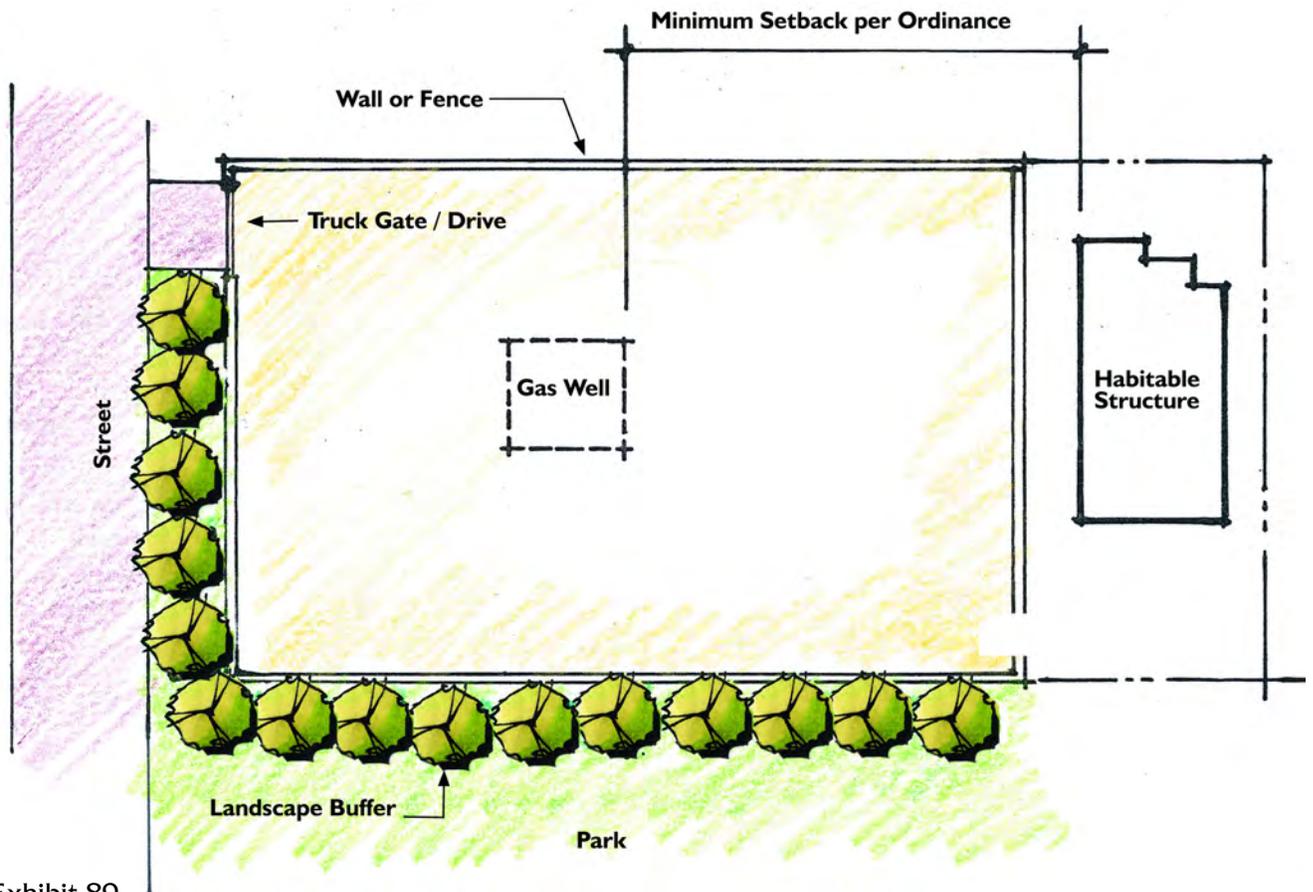


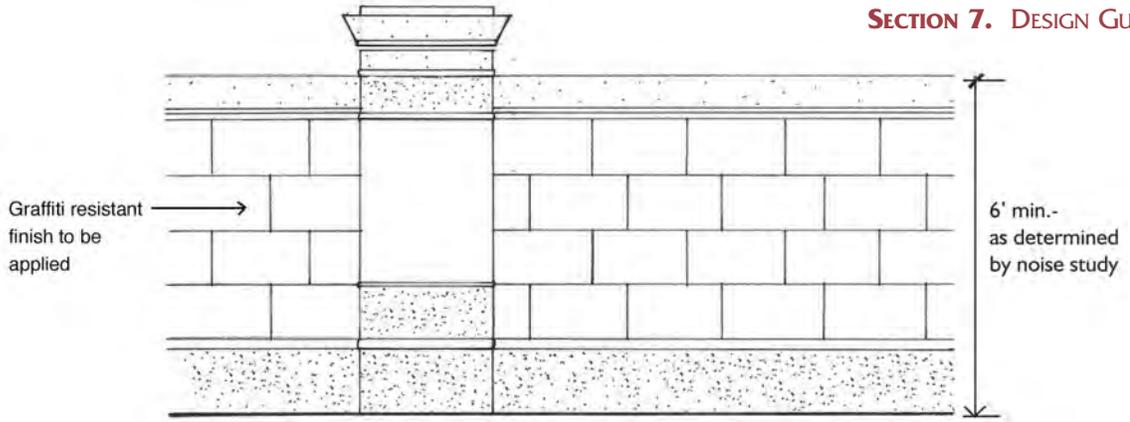
Exhibit 89
Gas Well Screening Plan

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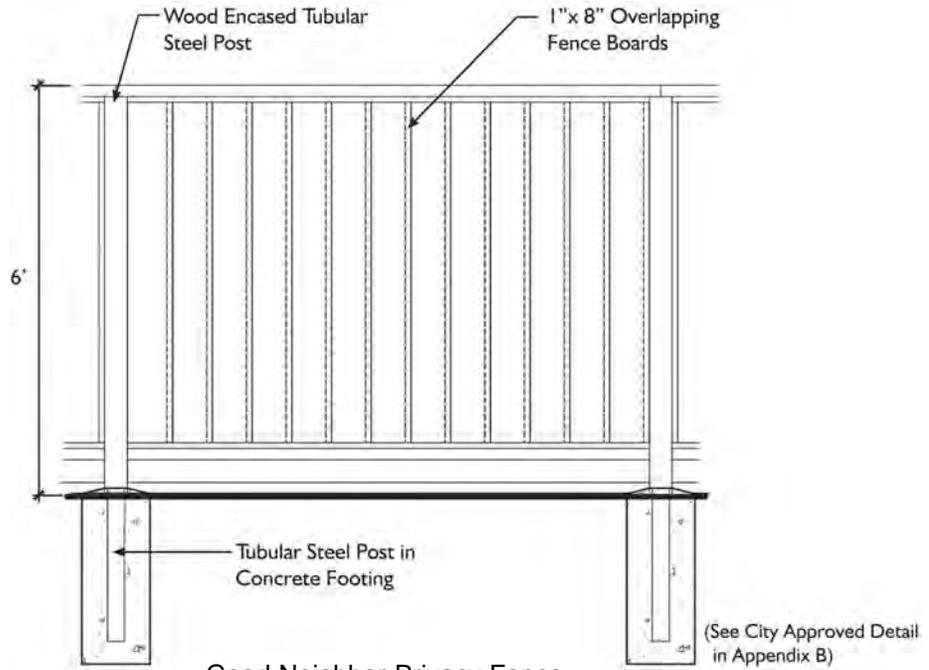


Exhibit 90
Walls & Fences Diagram

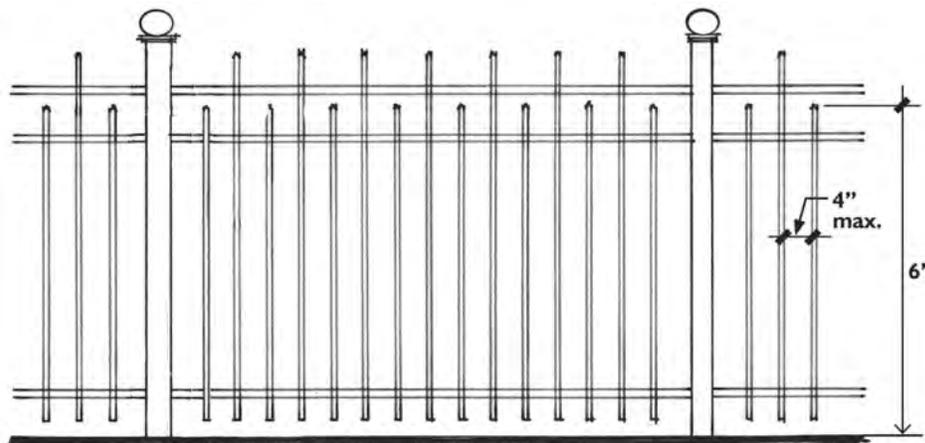




Community Theme Wall



Good Neighbor Privacy Fence

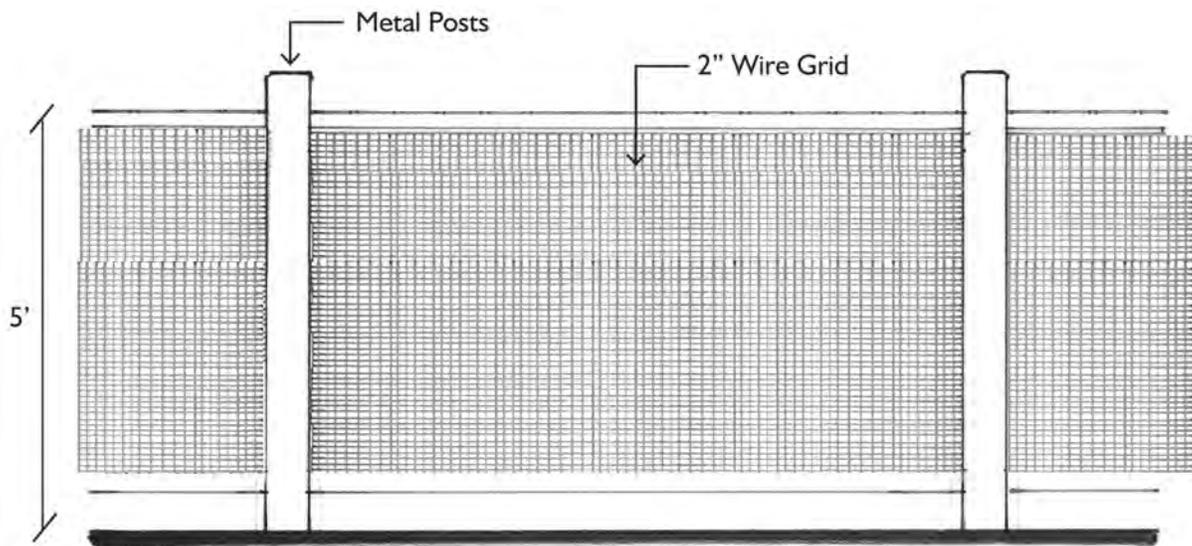


Residential View Fence

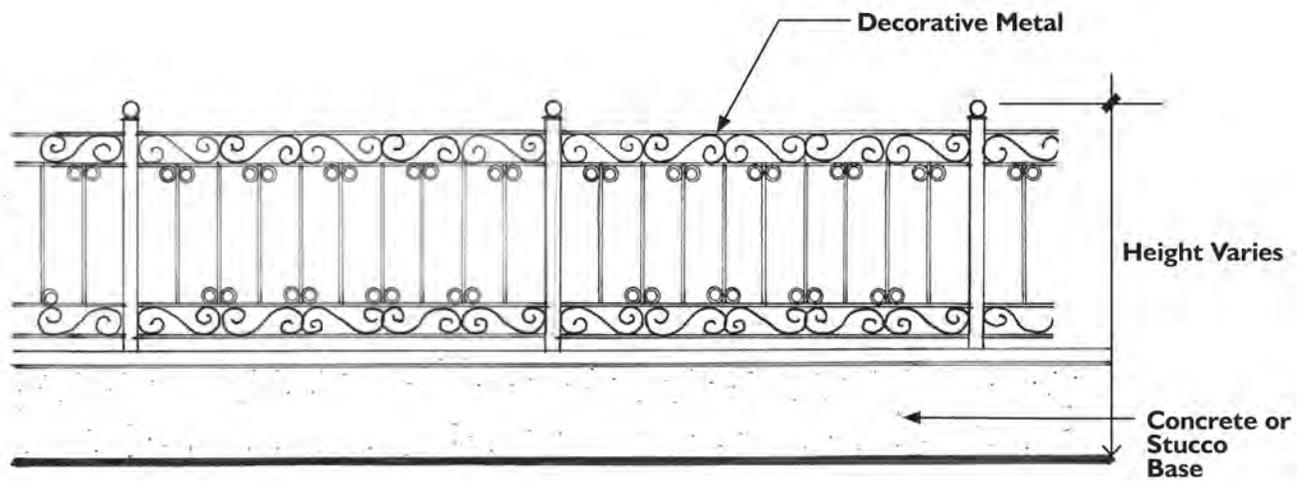
Exhibit 91
Walls and Fences

Community Theme Wall, Good Neighbor Fence, Residential View Fence

SECTION 7. DESIGN GUIDELINES



Wetland Buffer Fence



Community Accent Wall

Exhibit 92

Walls and Fences

Wetland Buffer Fence and Community Accent Wall for High Visual Impact Areas

Benches should be located near areas of public outdoor use and gathering. They shall be permanently mounted, and shall not obstruct circulation. Benches with backs are preferred, although flat benches may be appropriate in some locations.

Trash receptacles should be placed along trails and community paths, and throughout parks, schools, and commercial areas and other places where people gather.

Gang mailboxes in multi-family neighborhoods, should be integrated into the building(s) or clustered, in conformance with Postal Service requirements and compatible with building architecture. Double mailboxes serving two homes are encouraged as a way to reduce the number of separate elements along the street. Mailboxes should be simple and understated, and of a common design for each neighborhood.

7.2.8 LIGHTING

Lighting within the Specific Plan Area will help to differentiate between land use areas, emphasize community amenities, provide continuity along street corridors, and ensure safety.

Lighting should be designed to provide comfortable and safe spaces to walk, and provide sufficient illumination for roadway safety. Light standards should be of a consistent scale, character and color along roadways and within each neighborhood. The City of Oakley's street light standard will be used within the project areas.

Lighting should generally be designed to minimize light levels for any given application and emphasize high use areas or objects to be illuminated. Exterior lighting shall be shielded or recessed to minimize glare and reflections. Lighting should be designed to prevent spillover onto adjacent properties and should include dark-sky cutoffs.

High use pedestrian areas and sports facility parks will be illuminated. Lighting at pedestrian areas and roadside trails and walks should include pole lights in the 12-15' height range and/or bollard lighting. Pedestrian scale lighting should be used along publicly accessible lake edges. A combination of pole lighting, bollard lighting, or other appropriately scaled fixtures may be used in outdoor gathering areas. Park lighting should be kept to a minimum adjacent to the open space areas and wetlands. Athletic field and court lighting should be designed to minimize illumination of adjacent uses.

Light standards along roadways should be located at regular intervals, either along the sides of the roadway or as double-headed fixtures in the median, as appropriate. Light standards should be consistent along the entire length of an arterial roadway.

Lighting in commercial areas should be compatible with the architectural elements. Parking lot fixtures should be no taller than necessary to provide efficient lighting.

Landscape lighting fixtures should be hidden from direct view unless designed to be viewed.

7.2.9 SIGNAGE

Signage within the Specific Plan Area shall be used only where necessary, and in an understated manner, emphasizing the community's image of permanence and quality. Permanent signs should be used to convey information and to aid in identifying neighborhoods or other special areas. The advertising functions of signs should be minimized.

All signage shall be constructed of durable, high quality materials such as stone, metal or masonry.

7.2.9.1 Community and Neighborhood Signage

Community and neighborhood signage identifies the community and its neighborhoods, reinforces neighborhood character, and increases the legibility of the community by marking important entries and amenities.

Community identification signage should be used at the locations identified as "Gateways" on the "Entries and Arrivals Diagram," Exhibit 58. The signage should be incorporated into the overall landscape and architectural entry treatment. Community identification signage at the gateway entries will be larger in scale than other community entries.

Neighborhood identification signage should occur at the locations identified as "Community Entries" and "Neighborhood Entries" on the "Entries and Arrivals Diagram," Exhibits 58. Signage will vary for each planning area. Neighborhood entries should be discrete and understated. The signage should be incorporated into the overall landscape and architectural entry treatment, typically on a low wall in the landscape setback.

Signs should be sized to indicate scale and importance as follows: major streets, minor streets, neighborhood entries, private entries, parks, directional, and facility identification signs.

7.2.9.2 Commercial Signage

Building signs may be used to identify businesses. Building signs shall be designed as an integral part of the architecture of the building. Signs shall have borders and trim, and be recessed into their frames. They should be easily legible from pedestrian and parking areas, and generally located on the fascia, above or near the entrance. Within the Village Center, tenant identification signs should be consistent

in scale, placement, typeface, color, mounting details and illumination.

Freestanding commercial signs may be used to identify commercial sites. Freestanding commercial signs should be groundmounted, and located either in the landscape setback along the roadway or in the landscape median of the entry drive. They should be integrated into the landscape treatment of the roadway and landscape setback. Freestanding commercial signs should incorporate materials which reinforce the architectural character of the area.

7.2.9.3 Trail Signage

Signage will direct users to publicly accessible off-street trails, and to access points for publicly accessible levee trails. The project proponents will work with East Bay Regional Parks to develop signage compatible with their trail system signage. Signage will also identify the existing levees that are private property not accessible to the public. The project proponents will work with RD 799 and local residents to determine appropriate signage for private levees.

7.2.10 PLANTING DESIGN INTENT

This section describes the design intent for various street types, landscape zones and conditions within the Specific Plan Area and lists trees that are recommended to carry out that intent. For the major arterials and for Jersey Island Road, specific trees have been recommended. For the other road types, parks, and landscape zones, trees should be chosen from the palette that follows. The palette is not intended as an exclusive list, but as recommendations for appropriate plant materials. Non-toxic vegetation shall be used adjacent to all public open space areas.

7.2.10.1 East Cypress Road

A double row of tall columnar trees defines the right-of-way corridor. Large canopy trees shade and enclose the trails and scale this wide street. A double row of flowering accent trees highlight the medians. A combination of deciduous and evergreen trees should be used, with accent trees at entries. Views to water and parks should be created by breaks in the evergreen tree rows. The following trees are recommended for use on East Cypress Road:

<i>Cupressus sempervirens</i>	<i>Italian Cypress</i>
<i>Fraxinus oxycarpa 'Raywood'</i>	<i>Raywood Ash</i>
<i>Melaleuca linarifolia</i>	<i>Flaxleaf Paperbark</i>

7.2.10.2 Bethel Island Road

A double row of deciduous canopy trees encloses the roadway and shades the trails. Where Bethel Island Road is adjacent to residential development, evergreen screen trees buffer the residences from the road. Flowering accent trees are to be used in the median, and at project entries. The following trees are recommended for use on Bethel Island Road:

<i>Pyrus calleryana 'Aristocrat'</i>	<i>Aristocrat Flowering Pear</i>
<i>Arbutus 'Marina'</i>	<i>Hybrid Strawberry Tree</i>
<i>Metrosideros excelsus</i>	<i>New Zealand Christmas Tree</i>

7.2.10.3 Jersey Island Road

Jersey Island Road will retain its character as a rural road. The street trees will be reminiscent of a rural windbreak. The following is recommended:

<i>Populus fremontii</i>	<i>Fremont Cottonwood</i>
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Accent trees should be used at project entries from Jersey Island Road.

7.2.10.4 Project Collector Roads

Each project's identity will be established through the use of the same tree palette for all collector roads into the project. Project collector roads should be planted in a formal manner. Accent trees announce the project entry, and large canopy trees enclose the street and trail, and scale the street. Flowering accent trees should be planted in the medians and rotaries. Where the collector roads are adjacent residential development, evergreen background trees should provide additional buffering. Where they are adjacent to parks, water or open space, views should be opened.

7.2.10.5 Loop Roads

Each project's Loop Road should have one primary canopy tree to shade and enclose the street and trail, and one primary accent tree to announce neighborhood entries.

7.2.10.6 Parks

Park trees should provide shade and wind protection. They must be wind tolerant, and able to thrive in and adjacent to lawns.

7.2.10.7 Open Space

The open space corridors at the utility easements will be landscaped for a more natural look and will be mostly non-irrigated. Where roads cross the open space easements, trees will be planted in an orchard pattern. Crabapple, Chinese pistache, strawberry tree, or hawthorne are appropriate examples of trees for these areas. Away from the road crossings, trees with more natural forms, such as African sumac or western redbud, should be clustered to provide shade along the trail. Trees with riparian forms, such as Australian willow, should be used near lake edges. In the open space easements, the groundplane will be grasses, for ease of maintenance.

nance. Temporary irrigation will sustain plantings until they are established. Power line and gas line clearances must be respected.

The open space area between the Planning Area 1 community entry and central arrival point will be landscaped as an integral part of the entry experience, with attractive groundcover and flowering shrubs, and with trees outside the limits of the archaeological site.

7.2.10.8 Lake Edges

Riparian trees and trees with a weeping form are appropriate at lake edges. Where residential rear yards back onto the lakes, trees should not block water views from residences and yards.

7.2.10.9 Levees

Levees will be hydroseeded with a non-irrigated seed mix comprised of grasses and flowering groundcovers. Wildflowers and seasonal color will be emphasized to the extent possible. If at a future point the levee becomes redundant, it will be planted with trees and shrubs to function as a landscaped trail corridor.

The recommended seed mix is:

40%	<i>Distichlis stricta</i>	Saltgrass
27.5%	<i>Hordeum californicum</i>	Prostrate California Barley
20%	<i>Cynodon dactylon</i>	Common Bermuda Grass
5%	<i>Lupinus bicolor</i>	Dwarf Lupine
5%	<i>Eschscholzia californica</i>	California Poppy
2.5%	<i>Sisyrinchium bellum</i>	Blue-Eyed Grass

7.2.11 TREE PALETTE

7.2.11.1 Canopy Trees

Botanical Name	Common Name
<i>Acer rubrum</i> 'Red Sunset'	Red Maple
<i>Aesculus carnea</i>	Red Horsechestnut
<i>Celtis australis</i>	European Hackberry
<i>Celtis sinensis</i>	Chinese Hackberry
<i>Cinnamomun camphora</i>	Camphor Tree
<i>Fraxinus America</i>	Autumn Purple White Ash
<i>Fraxinus holotricha</i> 'Moraine'	Moraine Ash
<i>Fraxinus oxycarpa</i> 'Raywood'	Raywood Ash
<i>Fraxinus uhdei</i>	Evergreen Ash
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree
<i>Koelreuteria paniculata</i>	Goldenrain Tree
<i>Magnolia grandiflora</i>	Southern Magnolia
<i>Maytenus boaria</i>	Green Showers Chilean Mayten
<i>Melaleuca linarifolia</i>	Flaxleaf Paperbark
<i>Melaleuca quinquenervia</i>	Cajeput Tree
<i>Pistacia chinensis</i>	Chinese Pistache
<i>Platanus acerifolia</i>	Sycamore Cultivars 'Yarwood', 'Bloodgood', and 'Columbia'
<i>Prunus sargentii</i>	Sargent Cherry
<i>Pyrus calleryana</i>	Flowering Pear
<i>Quercus ilex</i>	Holly Oak
<i>Quercus lobata</i>	Valley Oak
<i>Quercus palustris</i>	Pin Oak
<i>Quercus schumardii</i>	Schumard Oak
<i>Quercus suber</i>	Cork Oak
<i>Quercus virginiana</i>	Southern Live Oak

<i>Quercus wislezenii</i>	Interior Live Oak
<i>Robinia ambigua idahoensis</i>	Idaho Locust
<i>Robinia</i> 'Purple Robe'	Purple Robe Locust
<i>Sapium sebiferum</i>	Chinese Tallow Tree
<i>Schinus molle</i>	California Pepper
<i>Sophora japonica</i>	Chinese Scholar Tree
<i>Tilia cordata</i>	Little Leaf Linden
<i>Tristania conferta</i>	Brisbane Box
<i>Ulmus parvifolia</i>	Chinese Elm
<i>Ulmus</i> 'Frontier'	Frontier Elm
<i>Ulmus</i> 'Pioneer'	Pioneer Elm

7.2.11.2 Small Accent Trees

Botanical Name	Common Name
<i>Arbutus</i> 'Marina'	Hybrid Strawberry Tree
<i>Arbutus unedo</i>	Strawberry Tree
<i>Carpinus betulus fastigiata</i>	Fastigate Hornbeam
<i>Cercis occidentalis</i>	Western Redbud
<i>Chionanthus retusus</i>	Chinese Fringe Tree
<i>Chitalpa tashkentensis</i>	Pink Dawn
<i>Crataegus phaenopyrum</i>	Washington Hawthorne
<i>Dodonea viscosa</i>	Hopseed bush
<i>Eriobotrya deflexa</i>	Bronze Loquat
<i>Geijera parviflora</i>	Australian Willow
<i>Lagerstroemia indica</i>	Crape Myrtle
<i>Malus ssp.</i>	Crabapple
<i>Olea europa</i>	Olive
<i>Prunus cerasifera</i>	Purple Leaf Plum
<i>Prunus yedoensis</i> 'Akebono'	Flowering Cherry

SECTION 7. DESIGN GUIDELINES

<i>Pyrus calleryana</i>	<i>Flowering Pear</i>
<i>Pyrus kawakami</i>	<i>Evergreen Pear</i>
<i>Rhus lancea</i>	<i>African Sumac</i>
<i>Schinus terebinthifolius</i>	<i>Brazilian Pepper</i>

7.2.11.3 Tall Accent Trees

Botanical Name	Common Name
<i>Betula jacquemontii</i>	<i>Jacquemontii</i> <i>Birch</i>
<i>Phoenix dactylifera</i>	<i>Date Palm</i>
<i>Washingtonia species</i>	<i>Palm</i>
<i>Cedrus deodara</i>	<i>Deodar Cedar</i>
<i>Eucalyptus camaldulensis</i>	<i>Red Gum</i>
<i>Eucalyptus nichollii</i>	<i>Peppermint</i> <i>Willow</i>

7.2.11.4 Screen Trees

Botanical Name	Common Name
<i>Casuarina stricta</i>	<i>Coast Beefwood</i>
<i>Cupressus arizonica</i>	<i>Arizona Cypress</i>
<i>Grevillea robusta</i>	<i>Silk Oak</i>
<i>Laurus nobilis</i>	<i>Grecian Laurel</i>
<i>Pinus eldarica</i>	<i>Afghan Pine</i>
<i>Pinus halepensis</i>	<i>Aleppo Pine</i>
<i>Pinus muricata</i>	<i>Bishop Pine</i>
<i>Prunus caroliniana</i>	<i>Carolina Laurel</i> <i>Cherry</i>
<i>Sequoia sempervirens</i>	<i>Coast Redwood</i> (<i>'Aptos Blue,'</i> <i>Soquel'</i>)

7.2.11.5 Lake Edge Trees

Botanical Name	Common Name
<i>Geijera parviflora</i>	<i>Australian</i> <i>Willow</i>
<i>Populus fremontii</i>	<i>Fremont</i> <i>cottonwood</i>
<i>Rhus lancea</i>	<i>African Sumac</i>
<i>Salix nigra</i>	<i>Black Willow</i>
<i>Salix babylonica</i>	<i>Weeping Willow</i>

SECTION 8. IMPLEMENTATION

The City of Oakley will adopt the East Cypress Corridor Specific Plan by ordinance and will concurrently adopt a pre-zoning district designation for the Specific Plan Area of “Specific Plan-1” (SP-1). Following certification of the East Cypress Corridor Specific Plan EIR, approval of a General Plan Amendment, Pre-zoning of the Specific Plan Area and adoption of the East Cypress Corridor Specific Plan by the City, the Specific Plan Area will be annexed into the City. The Specific Plan will serve as the implementation tool for the General Plan as well as establish the zoning regulations for the SP-1 zoning district for the Specific Plan Area. Following the annexation of the Specific Plan Area, Final Development Plans and Tentative Tract Maps establishing legal lots for Planning Areas 1-4 of the East Cypress Corridor Specific Plan will be reviewed and approved by the City.

8.1 INTERPRETATION

Unless otherwise provided, any ambiguity concerning the content or application of the East Cypress Corridor Specific Plan shall be resolved by the City of Oakley Community Development Director (Director) or his/her designee in a manner consistent with the goals, policies, purpose and intent established in this Specific Plan.

8.2 IMPLEMENTATION OF DESIGN GUIDELINES

Adoption of the East Cypress Corridor Specific Plan by the City includes adoption of the design guidelines contained herein, which shall be the design criteria by which development projects within the Specific Plan Area will be reviewed during the design review process. The design guidelines are intended to be flexible in

nature while establishing basic evaluation criteria for the review by the City of developer projects during design review.

8.3 IMPLEMENTATION OF DEVELOPMENT STANDARDS

Adoption of the East Cypress Corridor Specific Plan by the City includes adoption of the development standards for each land use depicted on the Land Use Plans in Section 4, “Land Use,” and as described in Section 6, “Development Regulations.” The requirements of the Specific Plan shall take precedence over the City of Oakley Zoning Ordinance. In instances where the Specific Plan is silent, the City of Oakley Zoning Ordinance shall prevail.

8.4 PRELIMINARY DEVELOPMENT PLAN / FINAL DEVELOPMENT PLAN

Approval of the East Cypress Corridor Specific Plan constitutes approval of a Preliminary Development Plan (PDP) for Planning Areas 1-4. All development projects within the Specific Plan Area shall be subject to approval of a Tentative Tract Map pursuant to the City’s Subdivision Ordinance and the State’s Subdivision Map Act. Concurrently with the approval of Tentative Tract Maps, Final Development Plans (FDP), pursuant to the City’s Subdivision Ordinance and State’s Subdivision Map Act, will be approved. Pursuant to these provisions, approval of a project through the FDP process constitutes approval of project architecture, site plans, and landscape plans for development.

8.5 DENSITY ALLOCATIONS

The East Cypress Corridor Specific Plan Land Use Plan allocates a maximum number of residential dwelling units and density to each Planning Area and the Specific Plan Area as a whole. Variations in number of residential dwelling units per land use designation and density allocation may occur within each of the six Planning Areas at the time of final design of the Planning Area depending upon the residential product identified for development, provided the total number of dwelling units assigned to each Planning Area as part of the Specific Plan is not exceeded. If the development potential is reduced due to environmental constraints, the residential units may be reallocated to other Planning Areas provided densities stay within the ranges shown on the Land Use Map and the total number of dwelling units for the Specific Plan is not exceeded. Variations in number of residential dwelling units and/or variations of land use boundaries and acreages within each Planning Area shall be subject to approval by the Director.

8.6 SPECIFIC PLAN MODIFICATIONS AND AMENDMENTS

8.6.1 MINOR MODIFICATIONS

The following constitute minor modifications to the Specific Plan, not requiring a Specific Plan Amendment, and are subject to review and approval by the Director. The Director shall have the discretion to refer any such request for modification to the Planning Commission or the City Council.

1. Change in utility and/or public service provider.
2. Collector roadway alignment when the change results in a centerline shift of less than 250 feet.

3. Adjustment of a planning area boundary provided the total acreage of the affected area does not increase or decrease by more than 15% of the total stated in the approved Specific Plan and the adjustment is agreed to by affected property owners. The form and content of property owner agreements shall be determined by the Director.
4. Minor changes to adopted development standards provided the change does not result in a change of over ten percent (10%) to an adopted development standard.
5. Minor changes to landscape materials, wall materials, wall alignment, entry design, and streetscape design which are consistent with the conceptual design set forth in the design guidelines contained within the Specific Plan.
6. Minor changes to the design guidelines, which are intended to be flexible in implementation.
7. Other modifications of a similar nature to those listed above, which are deemed minor by the Director, which are in keeping with the purpose and intent of the approved East Cypress Corridor Specific Plan and which are in conformance with the General Plan.

8.6.2 SPECIFIC PLAN AMENDMENTS

Amendments to the East Cypress Corridor Specific Plan may be requested by the applicant or by the City at any time pursuant to Section 65453(a) of the Government Code.

Amendments shall be processed pursuant to the provisions of the Government Code for Specific Plan Amendments. In the event the proposed amendment requires supplemental environmental analysis pursuant to CEQA, the applicant(s) are responsible for the costs associated with preparing the necessary CEQA documentation.

8.7 APPEALS

Appeals from any determination of the Director shall be made to the Planning Commission. The applicant(s) shall have the right to appeal the decision of the Director or Planning Commission on any determination by filing an application on forms provided by the City of Oakley within fifteen (15) calendar days following the final date of action for which an appeal is made. Appeals shall be processed consistent with the provisions of Chapter 10, Section A, “Appeals,” of the City of Oakley Zoning Ordinance.

8.8 COMPLIANCE WITH MITIGATION MONITORING PLAN

The City of Oakley will certify the East Cypress Corridor Specific Plan EIR prior to the adoption of the East Cypress Corridor Specific Plan. Development within the East Cypress Corridor Specific Plan Area shall comply with all applicable mitigation measures as described in the Mitigation Monitoring Program included as part of the EIR.

8.9 PROJECT FINANCING

Construction of public improvements including all infrastructure to serve the East Cypress Corridor Specific Plan will be financed through private and public funding mechanisms as approved by the City of Oakley.

8.9.1 INFRASTRUCTURE FINANCING MECHANISMS

Public improvements may be financed in whole or in part by benefit assessment and improvement act bonds, Community Facility Act bonds and other bonded indebtedness financing mechanisms permitted by statute. The determination of the financing mechanism and the

amount of debt financing shall be subject to approval by the City of Oakley.

8.9.2 DEVELOPMENT IMPACT FEES

Construction of infrastructure improvements identified in the Specific Plan will be financed by the development in the form of either fees, assessments, construction by the developers or a combination of the above. All development within the Specific Plan Area shall be subject to the City of Oakley's development impact fees in accordance with the provisions of the City's development impact fee ordinances, and will be subject to the development impact fees of other agencies applicable to the Specific Plan Area.

8.10 PROJECT PHASING

8.10.1 ANNEXATION

Following the approval by the City of Oakley of the General Plan Amendment, Pre-Zoning, Specific Plan and certification of the EIR for the East Cypress Corridor Specific Plan, the City will file three concurrent boundary reorganization applications with the LAFCO to annex the Specific Plan Area into the City of Oakley, the Contra Costa Water District, and the Diablo Water District. The boundary reorganization applications will include a municipal services plan as part of the applications. Annexation is anticipated to occur concurrently in three areas as depicted on Exhibit 93, “Annexation Area Map.” Annexation Area I includes the development property in Planning Area 5, the areas proposed for development within Planning Areas 1-4, and several contiguous properties to these areas located within Planning Area 6. These contiguous properties contain collectively fewer than 12 registered voters. Annexation Area II includes the strip of land along East Cypress Road within Planning Area 6 which includes 35 residential parcels and approximately 30 registered voters.

SECTION 8. IMPLEMENTATION

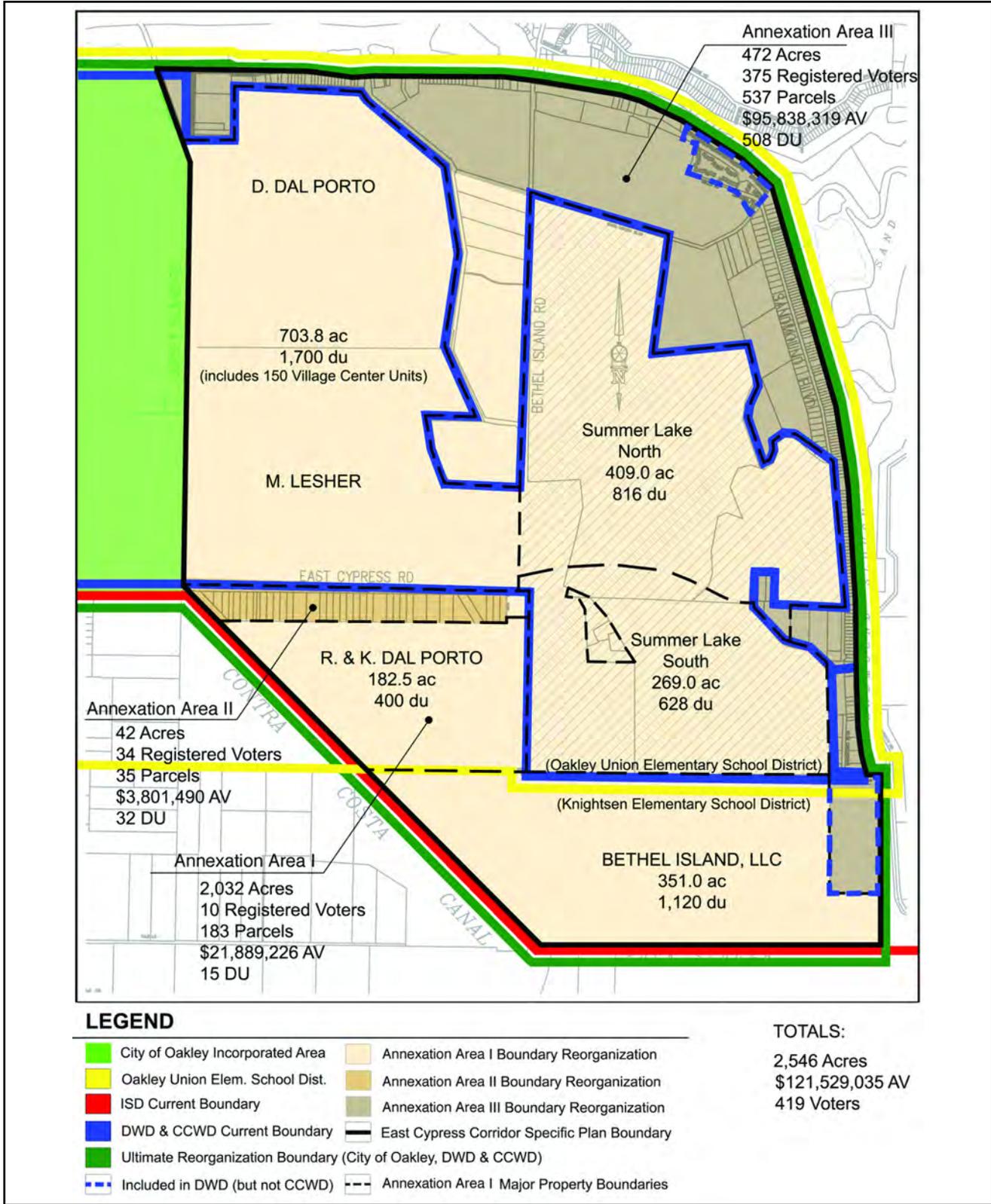


Exhibit 93
 Annexation Area Map

Annexation Area III includes the remaining approximately 462 acres at the northern and eastern edges of Planning Area 6 which contain approximately 375 registered voters. It is the City's intent to pursue concurrent annexation of all three Annexation Areas with a plan also submitted describing how Annexation Areas II and III could continue to receive services from the County in the event a majority of the voters elect for these areas not to be annexed.

This concurrent annexation option would result in all six Planning Areas being placed within the City, CCWD and DWD service boundaries concurrently. The entire Specific Plan Area is currently within the ISD service boundary. However the timing of delivery of new services for portions of Planning Area 6 would continue to be phased based on the availability of services from adjoining developing areas, as discussed in Section 5 of the Specific Plan.

8.10.2 DEVELOPMENT PHASING

Development Phasing of Planning Areas 1-5 of the East Cypress Corridor Specific Plan will meet the following objectives:

- Orderly build-out of the community based upon market and economic conditions.
- Provision of adequate infrastructure and public facilities concurrent with development of each phase.
- Protection of public health, safety and welfare.

8.10.2.1 Residential Development

The phasing of residential development areas within each planning area will be determined by the developers and the City of Oakley. The development of residential uses will be implemented through the approval of Final Development Plans by the City of Oakley and

recordation of final tract maps for each planning area, or portion thereof, as developed. Appropriate levels of infrastructure and community facilities will be installed and public services will be available to serve each phase of residential development as it occurs. Based on current market information, residential development within the Specific Plan Area is anticipated to occur over an approximately 10-15 year time frame.

1. The timing for installation of infrastructure and utilities and the provision of public services for development within Planning Areas 1-4 will be determined as part of the City's approval of Tentative Tract Maps for Planning Areas 1-4.
2. The timing for installation of infrastructure and utilities and the provision of public services for development within Planning Area 5 will occur pursuant to approvals granted by Contra Costa County for Subdivision 7562, Development Plan 3032-90 and Rezoning 2918-RZ as modified by the Contra Costa County Zoning Administration in October 2005.
3. The required levee improvements for the Specific Plan Area will be completed by the developers of Planning Areas 1-5 pursuant to the requirements of FEMA, Reclamation District 799, and the City of Oakley. Prior to the issuance of any permits for habitable structures, excluding residential model homes, a complete engineered levee system must be certified.
4. The required improvements to East Cypress Road, Jersey Island Road, and Bethel Island Road for development of the Specific Plan Area will be completed by the developers of Planning Areas 1-5 pursuant to the requirements of the final development plans and Tentative Tract Maps approved for Planning Areas 1-5.

5. The design and construction of the new bridge at Bethel Island Road crossing Dutch Slough has received funding and is to be completed by the County Public Works Department.
6. Grading and installation of backbone infrastructure within Planning Areas 1-5 to serve the residential portion of the Specific Plan Area will be completed as necessary to complete a phase of development. Following completion of the initial phase of grading and infrastructure installation, the developer will construct model complexes for each of the product types within the planning areas of the Specific Plan Area. These improvements will include provision for service extensions and sufficient reserve capacity to serve the remaining portions of Area 6, subject to concurrent annexation to CCWD, DWD and the City as part of a boundary reorganization.

8.10.2.2 Commercial Development

VILLAGE CENTER

The phasing of development of the Village Center within Planning Area 1 will be determined by the developer of Planning Area 1. The development of commercial uses will be implemented through the approval of parcel maps and approval of a final development plan by the City of Oakley for the Village Center. Appropriate levels of infrastructure and community facilities will be installed and public services will be available to serve the Village Center site at the time development of the Village Center commences. A minimum of 20 acres of the Village Center shall be reserved for commercial development. If, after using reasonable marketing methods, the entire approximately 40 net acre Village Center site is not sold or leased to one or more commercial development entities within seven years after the recorded annexation of Planning Areas 1-5 of the Specific Plan Area or by the issuance of the

3,000th building permit within Planning Areas 1-5, whichever is later, an application for a tentative map for up to 150 residential dwelling units for development of residential uses within the Village Center site could be filed for processing with the City of Oakley.

COMMERCIAL/COMMERCIAL RECREATION/LIGHT INDUSTRIAL

The phasing of development of the commercial center within Planning Area 2 will be determined by the developer of Planning Area 2 and is anticipated to occur at the build out of Planning Areas 2 and 5. The phasing of development of the commercial land use designated within Planning Area 6 will be determined by the property owner. The phasing of the commercial recreation and light industrial land uses will be determined by the property owner.

8.11 COMMUNITY FACILITIES

The timing for installation of community facilities and payment of impact fees for community facilities for the East Cypress Corridor Specific Plan will be determined as part of the City's approval of development applications in accordance with the provisions of the City's development impact fee ordinances. Community facilities such as the master plan of trails will be constructed in conjunction with development of open space areas and with construction of the public street improvements. The timing for the construction of a fire station by the developer(s) on the site designated for a fire station within Planning Area 2 will be determined by the East County Fire Protection District and will be constructed to the specifications of the District. Development of public schools within the Specific Plan Area will occur based upon the schedule identified by the respective school districts.

8.12 MAINTENANCE

The public and private improvements constructed as part of development of the Specific Plan Area will be maintained through a combination of public and private entities as described below.

8.12.1 PUBLIC AGENCY MAINTENANCE

1. All major arterial, collector, and local streets abutting the Specific Plan Area and interior to the Specific Plan Area will be dedicated as public streets to the City of Oakley and will be maintained by the City.
2. The proposed engineered levee system to be developed within the Specific Plan Area shall be dedicated to and maintained by Reclamation District 799. If Reclamation District 799 does not accept any part of the levee system for maintenance, a maintenance district consisting of the Specific Plan Area shall be formed to maintain the proposed engineered levee system. The redundant engineered levees constructed as part of the Summer Lake South (Planning Area 5) project may be abandoned at the time of the construction of the proposed master levee plan for the entire Specific Plan Area. These abandoned levees will be landscaped and maintained by a City managed special district established for the Specific Plan Area.
3. All on-site water, sewer, and storm drains within the Specific Plan Area shall be privately constructed to City and/or special district standards. Such facilities shall be maintained by the City, City maintenance district or appropriate special district through access easements granted by the developer to the City and/or special district for maintenance purposes.
4. Off-site/off-property infrastructure improvements such as water, sewer, and storm drain facilities constructed by the developer to

serve the Specific Plan Area will be dedicated to and maintained by the City and/or special district.

5. The Specific Plan Area will be annexed to existing City managed landscape and lighting districts, maintenance districts or community facility districts. Alternatively, new districts will be created to finance on-going maintenance and operations of public and quasi-public improvements. Improvements subject to maintenance and operations funding through these mechanisms may include, but not be limited to landscape improvements, parks, bicycle and pedestrian trail system, open space areas, wetland areas, habitat areas, street lighting, lakes and storm drain systems, to include storm water management plan best management practices, and other improvements and services. Any existing County managed community service areas (CSA) formed for the purpose of providing services to the the Summer Lake South development will be dissolved concurrently with the contemplated first annexation boundary reorganization.
6. Existing roadways and utility systems within Area 6 will continue to be maintained by a combination of private entities, ISD and the County, until such time as the area is brought into the City, CCWD and DWD as part of a boundary reorganization. Following annexation, existing private utility and roadway systems will continue to be privately owned and maintained, subject to review and possible upgrading to Specific Plan standards as part of any future development proposals or by decision of the property owners. Public roadways within the annexed portions of Area 6 would subsequently be maintained by the City of Oakley, and future water delivery systems would be maintained by DWD.

8.12.2 HOMEOWNER ASSOCIATION /PRIVATE PROPERTY OWNER MAINTENANCE

1. A Homeowner Association(s) (HOA) may be established for the maintenance of common area landscape and open space improvements within the residential portion of the Specific Plan Area. Private improvements to be maintained by the HOA include:
 - Private streets.
 - Private beach clubs and other private recreational facilities within residential areas.
 - Walkways, paseos, recreational facilities, and common areas within residential attached dwelling unit areas.
 - Private courts, parkways and landscaping within the residential areas.
 - Reverse frontage landscape areas of lots, common area facing wall surfaces, and internal slopes fronting streets along interior residential streets shall be maintained by an HOA or other methods such as a landscape maintenance district as approved by the City of Oakley.
 - Levee system landscape strip, unless assumed by a special district or other entity.
 - Street sweeping, of private streets, unless assumed by the City or other entity.
2. All improvements within the Village Center located in Planning Area 1 and the local serving commercial area located within Planning Area 2 shall be maintained by the respective property owners of the commercial centers.

8.13 PUBLIC SERVICES

1. A fiscal impact analysis has been prepared for the Specific Plan Area, comparing the revenue and expenditure impact on City of Oakley and the services it will provide to the Specific Plan Area. The results of the fiscal impact analysis will be used to determine additional funding mechanisms that may be required to provide adequate services to the Specific Plan Area.
2. The Specific Plan Area will be required to participate in the City of Oakley "P-6" police service special tax program to provide funding for the annual cost of providing police services to the area.
3. The Specific Plan Area will be required to participate in a fire service special tax program to provide funding for the annual cost of providing fire services to the area.

SECTION 9. GENERAL PLAN CONSISTENCY

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the general plan.

Prior to the adoption of the East Cypress Corridor Specific Plan, the City of Oakley will adopt a General Plan Amendment changing the General Plan Land Use Diagram for the Specific Plan Area to SP to reflect the East Cypress Corridor Specific Plan Land Use Plan, changing the land use designation for approximately 9 acres from “Agriculture Limited” to a park land use, changing the land use designation for approximately 6 acres from “Delta Recreation” to “Light Industrial” and increasing the total acres allocated for commercial land uses within the Cypress Corridor Expansion Area from 69 to 93. The East Cypress Corridor Specific Plan has been prepared in conformance with the goals and policies of the City of Oakley General Plan, as amended. This section describes the manner in which the Specific Plan and new development proposed within the Specific Plan Area comply with the General Plan, as amended. Applicable policies from the General Plan are listed for each General Plan Element followed by a discussion of Specific Plan consistency with the General Plan.

9.1 LAND USE ELEMENT

9.1.1 GENERAL POLICIES

2.1.1 Promote a combination of employment and residential uses that provide both jobs and housing for Oakley residents.

The East Cypress Corridor Specific Plan provides for the development of up to 5,759 residential dwelling units in a variety of housing types. Approximately 93 acres of commercial land uses and 37 acres of commercial recreation land uses are provided for within the Specific Plan which will offer jobs for local residents.

2.1.2 Consider the fiscal impacts of development in order to ensure the City has adequate financial resources to fund community projects and programs.

A fiscal impact study has been prepared for the East Cypress Corridor Specific Plan evaluating the planned level of development for the Specific Plan Area and the funding and financing mechanisms available and proposed to implement the Specific Plan to ensure that the City will have adequate resources.

2.1.3 Promote commercial and residential development that supports the small town character of Oakley. Key elements include scale of buildings, landscaped open areas within projects and safe and accessible multi-use trails.

The East Cypress Corridor Specific Plan is designed to support the small town character of Oakley by combining neighborhoods and commercial services to create a liveable community with a human scale of development oriented to

pedestrian activities providing connectivity among neighborhoods, parks, open space, and commercial areas. Development regulations and design guidelines included within the East Cypress Corridor Specific Plan promote a building scale in keeping with the small town character of Oakley. Residential development within the Specific Plan Area is served by a network of multi-use trails to be developed on top of the engineered levees and within an open space corridor connecting to a system of sidewalks and bike trails located within the rights of way of streets within the community. The trail system connects residents to commercial uses, schools, open space, natural habitat areas, and public recreational areas within and surrounding the community.

2.1.4 Preserve open space areas, of varying scales and uses, both within development projects and at the City's boundary.

Within the Specific Plan Area, approximately 119 acres of natural wetlands/dunes areas are reserved with the potential for additional acreage to be reserved as development occurs. Approximately 164 acres of public open space will be reserved within public utility corridors extending throughout the community. Approximately 159 acres of lakes will be provided as private open space amenities within the community.

2.1.5 Ensure a strong physical connection to the Delta and the waterfront, including convenient public access and recreational opportunities.

The open space and trails system, as well as the vehicular circulation system, to be developed within the Specific Plan Area connects residential areas to the Delta and the waterfront providing public access to recreational opportunities. The trail system provides connection to future trails leading to the Dutch Slough Community Park

being planned by the City and the future Wetland Restoration Project.

2.1.6 Assure that development in the City pays for its share of the cost of necessary public services and facilities.

The developers of the East Cypress Corridor Specific Plan will be required to pay appropriate City adopted fees for public services. Additionally, the developers of the Specific Plan Area will construct all infrastructure improvements necessary to serve the project, the new engineered levee system to protect the community from 100 year flood events, a fire station site within the Specific Plan Area to serve the community, and will provide sites for development of elementary and middle schools.

2.1.8 Discourage development that results in land use incompatibility. Specifically, require buffers between uses where appropriate and discourage locating sensitive uses (residential) adjacent to existing potentially objectionable uses or locating potentially objectionable uses adjacent to sensitive uses.

The Specific Plan includes a comprehensive set of development standards and design guidelines to ensure that adequate buffers are provided between planned new residential land uses and gas well sites, wetlands, and archaeological sites.

2.1.10 When considering large scale development projects, the City may, at its discretion, authorize a Specific Plan (SP) or Planned Unit Development (PUD) approach that allows flexibility within a project area. Under this approach, the distribution of land uses may vary from the land uses designated in the Land Use Diagram. The SP/PUD approach shall not allow either an overall greater development density than allowed under the

Land Use Diagram, or a combination of uses that undermine the overall intent of the project area as established under the General Plan policies and Land Use Diagram.

The City has authorized the East Cypress Corridor Specific Plan to establish a land use pattern to be adopted by the City as the General Plan land use designation for the area pursuant to approval of a GPA. The GPA will permit flexibility in the distribution in the land uses assigned for the East Cypress Corridor Expansion Area but will not allow a greater overall development density than allowed under the Land Use Diagram. The General Plan establishes a total of 5,763 residential dwelling units permitted within the Specific Plan Area. The East Cypress Corridor Specific Plan permits up to 5,609 residential dwelling units with an additional 150 which could be developed under specific conditions as described in the Specific Plan for a total of 5,759 dwelling units.

9.1.2 RESIDENTIAL POLICIES

2.2.2 Require that new development be generally consistent with the scale, appearance and small town character of Oakley.

The East Cypress Corridor Specific Plan includes a land use plan and circulation plan along with development regulations and design guidelines intended to create livable community in keeping with the small town character of Oakley as follows:

- Preserves and enhances natural habitat.
 - Facilitates mobility through biking and walking.
 - Offers opportunities for informal neighborhood interaction through a system of passive and active park and recreational areas.
 - Offers diverse architectural design of a high quality.
- Provides connectivity among neighborhoods through a network of sidewalks and multi-use trails.
 - Offers a diversity and choice of housing types and opportunities for a variety of lifestyles and economic segments of the marketplace.
 - Provides commercial and service retail opportunities connected to residential areas through a network of bicycle and pedestrian trails.
 - Provides schools within walking distance for students.

2.2.3 Protect existing residential areas from intrusion of incompatible land uses and disruptive traffic, to the extent reasonably possible.

New land uses proposed for the East Cypress Corridor Specific Plan Area are compatible with existing land uses located within the Specific Plan Area. Existing arterial and collector roadways will be improved to General Plan standards to convey traffic to and from the Specific Plan Area without disruption to existing residential land uses located along Dutch Slough, Sandmound Boulevard and East Cypress Road.

2.2.3 Promote in areas where different land uses abut one another, land use compatibility by utilizing buffering techniques such as landscaping, setbacks, screening and, where necessary, construction of sound walls.

The development standards and design guidelines within the Specific Plan include setback, screening, and buffering requirements to promote land use compatibility within the community. Where necessary, sound walls will be constructed in accordance with the recommendations of an acoustical analysis approved by the City.

2.2.1.2 Restrict or require increased setbacks for residential development proposed and adjacent to industrially or agriculturally designated or developed land to minimize conflicts.

Development of the Specific Plan Area includes the development of a comprehensive engineered levee system which will serve as a buffer between proposed new residential development and existing industrially and agriculturally designated properties which will likely remain in place after build out of the community.

2.2.1.3 All residential development should be required to construct and dedicate to the City and pay impact and other fees that represent their respective fair share of necessary public services and facilities.

As part of the development of the East Cypress Corridor Specific Plan, all required infrastructure including roadways, water, sewer, and drainage facilities will be constructed by the developers and dedicated to the City or appropriate agency. Impact fees, as required, will be paid to the City for public services.

9.2 COMMERCIAL POLICIES

2.3.6 Neighborhood commercial centers should be central to the neighborhood area they serve. Adequate access, compatibility with surrounding uses and consistent design with a community theme are necessary. These centers should maximize access for bicycles and pedestrians.

The East Cypress Corridor Specific Plan land use plan includes an approximately 40 net acre Village Center centrally located at the key intersection of Bethel Island Road and East Cypress Road. A second commercial center is proposed at the intersection of Bethel Island Road and Sandmound Boulevard. These com-

mercial centers will be served by arterial and collector roadways which will include the construction of bikeways and sidewalks as part of their development.

2.3.9 Ensure that, to the extent feasible, business areas are provided with adequate pedestrian, bicycle and parking facilities.

Commercial development within the Specific Plan Area will be required to provide parking in accordance with the City of Oakley Zoning Ordinance. As part of the development of these centers bicycle parking will be provided in accordance with City requirements.

2.3.13 Require that all commercial developments construct, dedicate land to the City and pay impact and other fees that represent their respective fair share of necessary public services and facilities.

As part of the development of the East Cypress Corridor Specific Plan, all required infrastructure including roadways, water, sewer, and drainage facilities will be constructed by the developers and dedicated to the City or appropriate agency. Impact fees, as required, will be paid to the City for public services.

2.3.14 Require landscaping in conjunction with commercial development projects that enhances the character and quality of the project and its immediate vicinity.

Development regulations and design guidelines for commercial areas within the Specific Plan Area require landscape improvements within setback areas, parking lots, and on-site common areas to enhance the character and visitor experience of these areas.

9.3 OPEN SPACE POLICIES

2.6.1 *Provide public access to the Delta and the waterfront wherever appropriate and feasible. Typically, such access should be unobstructed to the public by foot or bicycle and, where appropriate, by horse, automobile and/or boat.*

The open space and trails system, as well as the vehicular circulation system, planned within the Specific Plan Area connect residential areas to the Delta and the waterfront providing public access to recreational opportunities. A portion of the proposed trail system will be a multi-use trail for pedestrians, bicycles and equestrians.

2.6.2 *Preserve, enhance and/or restore selected existing natural habitat areas, as feasible.*

Within the Specific Plan Area approximately 119 acres of natural wetlands/dunes areas are reserved with the potential for additional acreage to be reserved as development of Planning Area 6 occurs.

2.6.4 *All public recreational areas and facilities shall be accessible by a publicly maintained road.*

All parks and public recreational areas developed within the East Cypress Corridor Specific Plan will be served by public roadways.

2.6.5 *Development shall not be permitted on lands designated by FEMA as flood prone until a risk assessment and other technical studies have been prepared and have shown that the risk is acceptable.*

Except for limited areas of high ground at elevations greater than seven feet, the entire Specific Plan Area is within a one percent chance floodplain designated by the FEMA. The source of flooding for this floodplain is the

adjacent Sacramento-San Joaquin Delta. The base flood elevation of 7.0 feet is the current estimate of the one percent chance flood event in the sloughs adjacent to the Specific Plan Area. Development within the Specific Plan Area is permitted subject to the completion of construction of a comprehensive engineered levee system which meets the FEMA requirements for 100-year flood protection.

2.6.6 *All approved entitlements and ministerial permits shall conform to the requirements of the Floodplain Management Ordinance, which are incorporated into this General Plan by reference.*

The East Cypress Corridor Specific Plan and all subsequent entitlement approvals shall conform to the requirements of the Floodplain Management Ordinance.

9.4 TRAILS POLICIES

2.7.1 *Promote a comprehensive trail program throughout the Oakley community and give preference to developments that incorporate the design of trails, including trails of neighboring communities where feasible, and associated open space into their design.*

The East Cypress Corridor Specific Plan incorporates the development of an extensive network of pedestrian, bicycle, and multi-use trails throughout the community within open space corridors, on top of the engineered levee system, and within the rights of way of public roadways.

9.5 COMMUNITY CHARACTER AND DESIGN POLICIES

2.8.7 *Residential neighborhoods and adjoining land uses should be connected by streets and multi-use trails, as appropriate.*

Fragmentation of neighborhoods is strongly discouraged.

The Specific Plan Area is afforded pedestrian, bicycle and vehicular connectivity through the planned street and trails system connecting residential neighborhoods, commercial centers, schools, parks, and open space corridors, and lakes.

2.8.9 Commercial development should provide opportunities for interaction between patrons and pedestrians. Examples include storefront display windows, sidewalk cafes and exterior seating and pedestrian scale signage.

Development of the Village Center and the commercial land use at the corner of Bethel Island Road and Sandmound Boulevard will incorporate pedestrian friendly features such as plazas, gathering spots, and seating areas in accordance with the commercial design guidelines adopted by the City of Oakley.

2.8.11 Monument or similar signs should be provided at appropriate gateways to residential districts, commercial areas or other significant landmarks.

The East Cypress Corridor Specific Plan includes a plan for entries and establishes a hierarchy of signage and monumentation for community and neighborhood entries.

9.6 SIGNAGE AND MONUMENTATION POLICIES

2.9.2 Require the construction of project monumentation to clearly identify the location and entry to major residential and commercial development projects, as deemed appropriate. Such monumentation shall be generally consistent with Figures 2-6 through 2-8 of this Element.

The East Cypress Corridor Specific Plan includes a plan for entries and establishes a hierarchy of signage and monumentation for community and neighborhood entries.

2.9.3 Monumentation at the entry to major commercial projects shall generally be constructed at intersections with arterial streets or, in some cases, at entry roads connecting to collector streets. Design of monuments shall generally be consistent with Figures 2-6 through 2-8 of this Element.

The Specific Plan requires that a master sign program be approved by the City to establish the hierarchy and location of signage within the commercial areas of the community.

9.7 CIRCULATION ELEMENT

9.7.1 ROADWAYS POLICIES

3.1.1 Strive to maintain Level of Service D as the minimum acceptable service standard for intersections during peak periods (except those facilities identified as Routes of Significance).

Intersections within the East Cypress Corridor Specific Plan will function at a Level of Service D or better.

3.1.8 Mitigate conflicts between new roadway improvements and existing rural roadways when identified conflicts threaten public health, safety and welfare.

To mitigate conflicts between new roadway improvements and existing rural roadways within the East Cypress Corridor Specific Plan Area, a new alignment for East Cypress Road will be constructed to enable the existing rural East Cypress Road to function as a frontage road. Additional circulation improvements, as recom-

mended in the project EIR and as required by the City Engineer, will be constructed to mitigate traffic conflicts at Bethel Island Road and Sandmound Boulevard. The project will participate in the construction of a new bridge at Bethel Island Road and Rock Slough.

9.7.2 BICYCLES AND PEDESTRIANS POLICIES

- 3.2.1 *Provide maximum opportunities for bicycle and pedestrian circulation on existing and new roadway facilities.*
- 3.2.2 *Enhance opportunities for bicycle and pedestrian activity in new public and private development projects.*
- 3.2.4 *Design new roadway facilities to accommodate bicycle and pedestrian traffic. Includes Class I, II or III bicycle facilities as appropriate. Through the Design Review process, provide sidewalks on all roadways, except in cases where very low pedestrian volumes and/or safety considerations preclude sidewalks.*

The East Cypress Corridor Specific Plan proposes the development of an extensive bicycle trail system to include Class I, Class II and Class III bike trails and a network of sidewalks and off-street pedestrian trails which provide connectivity throughout the entire community and to the surrounding area.

9.7.3 NEIGHBORHOOD TRAFFIC MANAGEMENT POLICIES

- 3.4.1 *Direct non-local traffic onto collector streets and arterials.*

The circulation plan for the East Cypress Corridor Specific Plan consists of a hierarchy of streets including the arterial and collector streets of East Cypress Road, Jersey Island Road

and Bethel Island Road. These arterial streets surrounding the Specific Plan Area will be improved to form the backbone circulation system to move traffic to and from the Specific Plan Area.

- 3.4.2 *Maintain traffic speeds and volumes on neighborhood streets consistent with residential land uses.*
- 3.4.3 *Provide adequate capacity on collector and arterial streets to accommodate travel within the City.*

The arterial and collector roadways to be developed as part of the East Cypress Corridor Specific Plan will be improved to the ultimate right of way recommended in the City's General Plan in order to accommodate the projected traffic volumes for these streets.

9.8 GROWTH MANAGEMENT ELEMENT

9.8.1 GROWTH CONTROL POLICIES

- 4.1.1 *Prior to approval of all projects, demonstrate that traffic levels of service and performance standards outlined in the Circulation Element will be maintained or that a funding mechanism and timeline has been established which will provide the infrastructure to meet the standards.*

The circulation system for the East Cypress Corridor Specific Plan has been designed to maintain the levels of service established for each roadway and intersection as identified in the General Plan.

4.1.2 Analyze conformity with the growth management standards for all development projects, such as subdivision maps or land use permits.

Subdivision maps and additional land use permits will be required for development to proceed within the Specific Plan Area following adoption of the East Cypress Corridor Specific Plan. These development approvals will be required to implement the infrastructure plans and development standards and guidelines approved by the City for the Specific Plan Area consistent with the Specific Plan.

4.1.4 Require from all new development, including major modifications to existing development, the construction of all necessary on- or off-site infrastructure and public services needed to serve the project in accordance with City standards.

All necessary infrastructure and public services needed to serve the East Cypress Corridor Specific Plan will either be constructed or funded through impact fees by the project developers.

4.1.5 Direct growth toward areas with existing infrastructure.

Growth within the East Cypress Corridor Specific Plan Area can be accommodated pursuant to the installation of infrastructure and provision of public services as planned for in the Specific Plan.

9.8.2 FACILITIES FINANCING POLICIES

4.2.1 Permit development only when financing mechanisms are in place or committed which assure that adopted performance standards for public facilities will be met.

The East Cypress Corridor Specific Plan includes standards for implementation of the Specific Plan to include alternative funding and financing mechanisms for provision of public facilities.

4.2.2 Ensure that all future development projects provide public infrastructure and public services that fully serve the needs of the project and address any impacts created by such projects.

4.2.3 Ensure that new development does not adversely affect public facilities or services.

All necessary infrastructure and public services needed to serve the East Cypress Corridor Specific Plan will either be constructed or funded through impact fees by the project developers.

4.2.4 Ensure that future development projects are included in special districts (i.e. lighting, landscaping, etc.) when applicable.

The City will create a landscape and lighting district for the East Cypress Corridor Specific Plan Area for maintenance of common area improvements including but not limited to common area landscaping, parks and street lighting within the Specific Plan Area.

4.2.5 Require new development to pay its fair share of the cost of existing public facilities and services it utilizes, as appropriate, based upon the demand for these facilities and services that can be attributed to new development.

Developers of the East Cypress Corridor Specific Plan will pay impact fees to pay for their fair share of the cost of services and public facilities required to support the development.

4.2.6 Require new development to pay all costs of upgrading existing public facilities, constructing new facilities or expanding services that are needed to serve new development.

As part of the development of the East Cypress Corridor Specific Plan, upgrades to existing roadways and other infrastructure facilities within the Specific Plan Area will be constructed. New water, sewer, and drainage facilities will be installed, a fire station site will be developed for the community, public parks, trails, and open space will be developed, and elementary and middle schools sites will be reserved to serve the community.

4.2.7 Financial impacts of new development on public facilities and services should generally be determined during the project review process and may be based on the analysis contemplated under the growth management program (Land Use Element) or otherwise. As part of the project approval, specific findings shall be adopted which relate to the demand for public facilities and services and how the demand affects the service standards included in the growth management program.

A fiscal impact report has been prepared for the East Cypress Corridor Specific Plan analyzing the impact of the project on public services and facilities and how these impacts are being addressed in order to determine the growth management implications of the project. Findings resulting from the analysis will assist the City in identifying appropriate conditions of approval for projects as phased development occurs within the Specific Plan Area.

4.2.13 New development should not result in inconsistent street frontage improvements along streets adjacent to and serving the project.

A master plan for new roadways including landscape design for street frontages will be adopted for the East Cypress Corridor Specific Plan to ensure that street frontage improvements for the project are developed in a consistent manner.

9.8.3 COMMUNITY BUILDING & SERVICES POLICIES

4.3.2 Ensure that high quality library services are maintained for the residents of Oakley.

The developers of the East Cypress Corridor Specific Plan will pay the appropriate level of impact fees to ensure that the development does not negatively impact the level of service for libraries in the City.

4.3.5 Encourage the development of quality childcare and pre-school facilities in appropriate locations, especially in conjunction with park and private common areas, schools and church facilities.

Day care and pre-school facilities are considered in the list of permitted uses within the East Cypress Corridor Specific Plan enabling providers of these services to operate within the community.

4.3.6 *Encourage or develop a coordinated telecommunications systems which enhances the availability of information to the Community.*

The East Cypress Corridor Specific Plan includes standards for the provision of telecommunications systems within the new homes to be developed within the community.

9.8.4 FIRE PROTECTION AND EMERGENCY SERVICES POLICIES

4.4.2 *Require that new development pay its fair share of costs for new fire protection facilities and services.*

As part of the development of the East Cypress Corridor Specific Plan, a fire station site is reserved and will be constructed and equipped per the specifications of the fire district.

4.4.3 *Identify needed upgrades to fire facilities and equipment during project environmental review and planning activities.*

The environmental impact report prepared for the East Cypress Corridor Specific Plan will evaluate the fire facilities needs for the community and recommend measures to mitigate any potential significant impacts.

9.8.5 LAW ENFORCEMENT POLICIES

4.5.2 *Incorporate police protection standards and requirements into the land use planning process.*

The Police Department will review the East Cypress Corridor Specific Plan and subsequent subdivision maps and entitlement permit applications and recommend appropriate improvements to ensure that public safety standards are met.

4.5.7 *Set fees to ensure that each dwelling unit, business and vacant parcel pays a fair share of the cost of police services.*

The parcels within the East Cypress Corridor Specific Plan will be subject to a parcel tax to cover the cost of the project's fair share of police services.

9.8.6 PUBLIC SCHOOLS POLICIES

4.6.2 *To the extent possible, new residential development, General Plan Amendments or Rezoning shall be required to adequately mitigate impacts on primary and secondary school facilities.*

Exceptions to this requirement may be made if the Planning Agency determines that there are overriding considerations (i.e. provision of low or moderate cost housing) associated with the project.

4.6.5 *Support the development of quality schools, to the extent possible, by coordinating development review with local school districts including such activities as designating school sites and obtaining dedication of school sites. Consider supporting appropriate local fees, special taxes and bond issues intended for school construction.*

4.6.6 *Ensure that school facility impact fees are collected and work with developers and school districts to establish mitigation measures to ensure the availability of adequate school facilities.*

As part of the development of the East Cypress Corridor Specific Plan appropriate sites will be reserved for the development of three elementary schools and one middle school to serve the community. The City shall work with representatives of the Knightsen Elementary School District to explore realignment of its boundary

to facilitate service to the entire Specific Plan Area by the Oakley Unified School District.

4.6.10 To the extent possible, school facilities should be sited in conjunction with and adjacent to local parks and trails.

Schools within the East Cypress Corridor Specific Plan are located adjacent to or within close proximity to neighborhood parks and open space corridors, and trail systems.

4.6.11 Ensure that adequate land is available for acquisition of school sites within one mile of Proposed School Locations as identified on Figure 4-1, Public Facilities and Services.

The locations of the elementary and middle school site have been coordinated with each respective school district. Adequate land is available within the East Cypress Corridor Specific Plan to accommodate a public high school in the event the school district determines the Specific Plan Area is a desirable location for a new high school. The East Cypress Corridor Specific Plan provides for an option of providing a high school site in Planning Area 2 in place of a middle school site and residential sites.

9.8.7 SOLID WASTE/RECYCLING AND HAZARDOUS MATERIALS POLICIES

4.7.5 Consider solid waste disposal capacity in land use planning and permitting activities, along with other utility requirements, such as water and sewer service.

The environmental impact report prepared for the East Cypress Corridor Specific Plan will analyze the impacts of the project on solid waste disposal capacity and recommend measures, as appropriate, to mitigate any potential impacts to solid waste disposal facilities.

9.8.8 WATER SERVICES POLICIES

4.8.1 Coordinate future development with all water agencies to ensure facilities are available for proper water supply.

Water system improvements to serve the development of the East Cypress Corridor Specific Plan have been reviewed with all appropriate water agencies to ensure that adequate facilities are identified and required for construction to serve the community.

4.8.4 Ensure that new development pays the costs related to the need for increased water system capacity.

4.8.5 Ensure that water service systems be required to meet regulatory standards for water delivery, water storage and emergency water supplies.

As part of the development of the East Cypress Corridor Specific Plan new water reservoirs, mains, pumps and associated water facilities adequate to serve the community will be installed.

4.8.7 Encourage urban development within existing water district Spheres of Influence adopted by the Local Agency Formation Commission; expansion into new areas within the Urban Limit Line beyond the Spheres should be restricted to those areas where urban development can meet all growth management standards included in the General Plan.

The East Cypress Corridor Specific Plan will be served by DWD and indirectly by CCWD. Planning Areas 1, 2, 5 and a portion of Planning Area 6 are currently within the DWD/CCWD service boundaries. The remainder of the Specific Plan Area not currently within the DWD/CCWD service boundaries

will annex into the Districts concurrently with City annexation of these areas.

4.8.9 Encourage rural residences currently served by well water or an underground domestic water supply to connect to municipal water service when it becomes available. Upon connection to municipal water service, any water well(s) may be maintained for irrigation purposes only.

Water facilities to be constructed within the East Cypress Corridor Specific Plan Area are designed to accommodate connections to these facilities by existing rural residences upon annexation to the City and DWD/CCWD.

4.8.11 Identify, monitor and regulate land uses and activities that could result in contamination of groundwater supplies to minimize the risk of such contamination.

The development of the East Cypress Corridor Specific Plan includes requirements for the construction of water quality treatment facilities to NPDES standards.

4.8.12 Reduce the need for water system improvements by encouraging new development to incorporate water conservation measures to decrease peak water use.

Water conservation measures are incorporated into the East Cypress Corridor Specific Plan through the use of drought tolerant planting materials in common areas and the use of reclaimed water if available from the City of Oakley and ISD.

4.8.12 Encourage the use of reclaimed water as a supplement to existing water supplies.

The development of the East Cypress Corridor Specific Plan will include installation of separate water lines to accept reclaimed water if available from the City of Oakley and/or ISD. At this time, no reclaimed water is available.

4.8.13 All proposals for development, including requests for building permits, within 1,000 feet of the Contra Costa Canal property line shall be referred to Contra Costa Water District for comment to ascertain the District's standards for the proposed development project.

The East Cypress Corridor Specific Plan and EIR have been reviewed by the Contra Costa Water District.

9.8.9 WASTEWATER SERVICES POLICIES

4.9.1 Coordinate future development with the ISD to ensure facilities are available for proper wastewater disposal.

The ISD has analyzed the proposed level of development for the East Cypress Corridor Specific Plan to identify the extent to which new facilities will be required to serve the community.

4.9.4 Reduce the need for sewer system improvement by requiring new development to incorporate water conservation measures which reduce flows into the sanitary sewer system.

Water conservation measures designed to reduce flows into the sanitary sewer system will be incorporated into the design of new homes planned for the East Cypress Corridor Specific Plan.

9.8.10 DRAINAGE FACILITIES POLICIES

4.10.1 Work cooperatively with Contra Costa County Flood Control and Water Conservation District (CCCFCWCD) to ensure and enhance flood protection in the City of Oakley.

4.10.2 Pursue and achieve compliance with all regional, State and Federal regulations related to flood control, drainage and water quality.

4.10.3 Recognize the unique flooding constraints of the areas north and east of the Contra Costa Canal.

Except for limited areas of high ground at elevations greater than seven feet, the entire Specific Plan Area is within a one percent chance floodplain designated by FEMA. The source of flooding for this floodplain is the adjacent Sacramento-San Joaquin Delta. The base flood elevation of 7.0 feet is the current estimate of the one percent chance flood event in the sloughs adjacent to the Specific Plan Area.

To bring the Specific Plan Area out of the floodplain, an extensive system of engineered levees will be developed to meet FEMA requirements for 100-year flood protection.

4.10.6 Develop new drainage facilities and/or improvements to existing facilities to provide additional recreational or environmental benefit, where possible.

As part of the drainage improvements for the East Cypress Corridor Specific Plan, a system of lakes will be developed for storm water detention and cleansing purposes. These lakes will also provide an open space amenity to the community with the potential for limited recreational purposes.

4.10.10 In conjunction with Contra Costa County Flood Control and Water Conservation District (CCCFCWCD), develop and approve a Drainage Master Plan in the Cypress Corridor Area and the Cypress Corridor Expansion Area.

The East Cypress Corridor Specific Plan includes a master plan of drainage improvements to serve the entire community. This master plan of drainage has been developed in conjunction with the CCCFCWCD.

Mitigation Measure 3.9-2 To maintain long-term water-quality objectives for the lakes, the City shall require a comprehensive Lake Management Plan (LMP) for all individual projects that will construct lake features. The plan shall clearly identify the management activities that are needed, the anticipated costs of conducting the required activities and the funding source to implement the LMP. Wherever practical, the City of Oakley shall own the lakes and associated infrastructure and shall be the entity responsible for implementing the LMP. The Lake Management Plan shall be approved by the Building Department prior to the issuance of a final grading permit for the lake.

4.10.12 In conjunction with Contra Costa County Flood Control and Water Conservation District (CCCFCWCD), develop flood control plans and identify discharge points for unincorporated areas annexed to the City of Oakley. Ensure that flood control implementation and maintenance are performed.

Flood control plans which include a master plan of levees and lakes to be constructed within the community are being developed in conjunction with CCCFCWCD.

9.9 OPEN SPACE AND CONSERVATION ELEMENT

9.9.1 AGRICULTURAL RESOURCES POLICIES

- 6.1.2 *Reduce the negative impacts resulting from urban uses and neighboring agricultural uses in close proximity.*
- 6.1.4 *Incorporate parks, open space and trails between urban and agricultural uses to provide buffer and transition between uses.*

Buffers between existing agricultural uses and new urban uses within the East Cypress Corridor Specific Plan will be provided through the development of a new engineered levee system which will include a multi-use trail system on the tops of the engineered levees.

9.9.2 AIR QUALITY POLICIES

- 6.2.4 *Ensure location and design of development projects so as to conserve air quality and minimize direct and indirect emissions of air contaminants. The project EIR prepared for the East Cypress Corridor Specific Plan recommends mitigation measures to minimize direct and indirect emissions of air contaminants during construction phases and long term operations.*

9.9.3 BIOLOGICAL RESOURCES POLICIES

- 6.3.1 *Encourage preservation of important ecological and biological resources as open space.*

The East Cypress Corridor Specific Plan preserves approximately 119 acres of natural wetlands/dunes as part of the project. Additional wetlands/dunes will be identified for preservation as development of Planning Area 6 occurs.

- 6.3.2 *Develop open space uses in an ecologically sensitive manner.*

Open space within the East Cypress Corridor Specific Plan utilizes existing utility corridors and includes the development of lakes with the dual purpose of serving as NPDES facilities.

- 6.3.3 *Use land use planning to reduce the impact of urban development on important ecological and biological resources identified during application review and analysis.*
- 6.3.5 *Encourage preservation and enhancement of Delta wetlands, significant trees, natural vegetation and wildlife populations.*
- 6.3.6 *Encourage preservation of portions of important wildlife habitats that would be disturbed by major development, particularly adjacent to the Delta.*

The Specific Plan land use plan identifies approximately 119 acres of wetlands/dune area to be reserved as a natural habitat area. Approximately 105 acres of the wetlands/dune area are located within Planning Area 1. Approximately 10 acres of wetlands are also located within Planning Area 2, and approximately 4 acres are reserved as part of the development plan approved for Planning Area 5, the Summer Lake South development. These areas will be preserved in the natural condition as part of the project. As development of Planning Area 6 occurs in the future, additional wetlands/dune areas will be set aside as habitat area. The 105 acre wetlands/dune area to be reserved within Planning Area 1 as natural habitat is located within the San Joaquin River flood plain. Wetlands/dune areas adjacent to the Dutch Slough will be left in a natural condition. Areas south of the existing non-engineered levee historically used for agriculture will be enhanced and returned to a naturalized condition.

Public access to this area will be limited and the area will be managed for the purpose of habitat mitigation according to a Mitigation and Monitoring Plan (MMP) adopted as part of the certification of the East Cypress Corridor Specific Plan EIR. The MMP will provide guidance on managing and monitoring the mitigation habitat. The wetlands/dune area will be preserved and enhanced in perpetuity for the following species and habitats:

- Wetlands and waters
- Riparian habitat and trees
- Alkali meadow and grassland
- Special-status plants
- Special-status invertebrates: dune insects and vernal pool fairy shrimp
- Special-status reptiles: giant garter snake, western pond turtle, and silvery legless lizard
- Special-status birds: burrowing owl, Swainson's hawk, and other nesting raptors
- Special-status mammals: bats

9.9.4 CULTURAL RESOURCES POLICIES

6.4.1 Preserve Areas that have identifiable and important archaeological or paleontological significance.

Archaeological sites within the Specific Plan Area will be avoided by development to the extent possible. Where archaeological sites cannot be avoided they will be incorporated into parks and open space. Where archaeological sites cannot be avoided or incorporated into parks, greenspace or open space, these sites will be treated with a layer of chemically stable soil prior to the construction of facilities on top of them. Types of facilities which will be permitted to be constructed on top of archaeological sites include tennis courts, parking lots, or similar facilities and school playgrounds.

9.9.5 OPEN SPACE RESOURCES POLICIES

6.6.2 Establish buffers from adjoining land uses to protect the natural open space resources in the City.

The East Cypress Corridor Specific Plan includes guidelines for buffering of natural open space areas and habitat from development.

6.6.4 Where feasible and desirable, major open space components shall be combined and linked to form a visual and physical system in the City.

Open space consists of the utilization of utility corridors and canals improved for multi-use trail and passive recreational use forming connects between the community and the surrounding area.

9.10 PARKS AND RECREATION ELEMENT

9.10.1 GENERAL PARKS AND RECREATION POLICIES

7.1.10 Consider multiple uses for open space land (i.e. land use buffer zones and green-ways for trails and linear parks, flood control basins for basin and park joint use and school sites for neighborhood/community park joint use).

The East Cypress Corridor Specific Plan utilizes existing utility corridors as an open space network providing a greenway connection throughout the community. Lakes planned for the community will also serve as flood control and water quality basins.

7.1.19 Require all development to dedicate parkland and/or pay in-lieu, as well as pay impact fees sufficient to meet the added demand for parkland facilities.

The East Cypress Corridor Specific Plan includes approximately 437 acres as parks, lakes, and active open space. New parks consist of public community parks, and neighborhood parks. Open space consists of the preservation of utility corridors and canals improved for multi-use trail and passive recreational use. Public and private lakes, including private beach clubs, provide open space amenities and recreational opportunities for residents.

9.10.2 COMMUNITY PARKS, PLAYFIELDS AND RECREATION CENTERS POLICIES

7.2.1 Develop 3.0 acres of community park per 1,000 residents (corrected to a requirement of 2.0 acres of community park per 1,000 residents per City’s Park and Recreation Master Plan) for all new residential subdivisions either through land dedication or in-lieu fees.

The East Cypress Corridor Specific Plan will provide approximately 41 acres of community parks to serve the Specific Plan Area and 600 acres of neighborhood parks.

7.2.4 Locate a community park generally within 1 mile of almost all Oakley residents. Parks should be located on a major arterial or thoroughfare, where impact to surrounding residential neighborhoods is minimized. If the community park should abut residential areas, those uses common to neighborhood parks should be used as buffers. Wherever possible, incorporate commu-

nity parks and trails that are a part of the Citywide trail network.

The East Cypress Corridor Specific Plan includes the provision of a site for a community park. A centrally located community park will be within 1 mile of almost all of the proposed development. It will be served by East Cypress Road and Bethel Island Road and is accessible through the network of trails to be constructed within the open space system, on the engineered levees and within streets connecting the park to the entire community.

7.2.5 Design community parks to have a minimum size of 10 acres, comfortably 15 to 20 acres with an ideal size of 40 to 50 acres. As recreation activities will drive the design of the community park, these parks should host formal and organized recreation tournaments, and should meet adult recreation opportunities, which generally requires larger fields and therefore larger sites.

The community parks proposed within the East Cypress Corridor Specific Plan total approximately 41 acres in size. Improvements to the community parks will be provided in accordance with City requirements depending on the level and type of recreational activities planned for the park by the City.

7.2.8 Design community parks to contain features that serve the community at large and provide economies of scale. Allocate at least 65% of the land to be available for active recreation. Appropriate features include:

- *Multiple play fields for organized play with lighting of some fields*
- *Multiple play courts*
- *Separate play areas for both school age and pre-school children*

- *Special features such as a skate park or playground with water play*
- *Areas for special events such as an amphitheater or festival activities*
- *Group picnic as well as individual picnic areas*
- *Restrooms and concessions*
- *Parking*
- *Equipment storage*

The community parks proposed within the East Cypress Corridor Specific Plan will be developed in accordance with City requirements depending on the level and type of recreational activities planned for the park by the City.

9.10.3 NEIGHBORHOOD PARKS POLICIES

7.3.3 Provide for 2.0 acres of developed neighborhood park per 1,000 residents (corrected to a requirement of 3.0 acres of neighborhood park per 1,000 residents per City's Park and Recreation Master Plan) from all new residential subdivisions through Land Dedication In-lieu fees and/or Park Impact fees.

The East Cypress Corridor Specific Plan provides approximately 68 acres of neighborhood parks.

7.3.6 Front at least 50% of a park's frontage onto a public street. For perimeters not bound by a street, woodlands, creeks, agricultural uses or significant open space features are desired over backyard fences. Where backyard fences are unavoidable, they should be screened through the use of trees and shrubs. Surrounding buildings should have windows and entries onto the park.

Almost all of the neighborhood parks planned for the East Cypress Corridor Specific Plan front onto public streets. Some neighborhood parks are also located adjacent to the public open space corridor and will be served by the multi-use trail planned within this open space corridor.

7.3.6 Design neighborhood parks to conserve natural features including creeks, heritage trees and significant habitats. However, parkland dedicated for private active recreation should not have biological and/or ecological restrictions on land usage.

Neighborhood parks within the East Cypress Corridor Specific Plan are located outside of significant habitat areas.

7.3.8 Locate neighborhood parks no more than 1/4 mile walking distance for most residents. Attempt to avoid major street crossing for most residents to access a neighborhood park.

All the neighborhood parks planned for the East Cypress Corridor Specific Plan are accessible to residents by means of local residential streets and bike and pedestrian paths. Most residents will be within 1/4 mile of a neighborhood park.

7.3.9 Design and locate neighborhood parks based on a preferred size of 5 to 6 acres with a minimum size of 2 acres. The park size of 5 to 6 acres would allow for the incorporation of lawn play areas of sufficient size to accommodate informal field sports.

All of the neighborhood parks within Planning Areas 1-4 are greater than 2 acres in size.

9.10.4 SPECIAL PURPOSE FACILITIES POLICIES

7.4.2 Provide special purpose facilities as determined by community need and support, either as separate facilities or combined with neighborhood or community parks (providing for appropriate separation of uses):

- *Off-leash dog area or parks*
- *Equestrian center and arena*
- *Trails*

The East Cypress Corridor Specific Plan provides an extensive network of multi-use trails and bicycle trails for use by pedestrians, equestrians and bicyclists. The trail system links neighborhoods, parks, schools, commercial centers and open space areas within the community.

7.4.6 Restrict or prohibit public access to certain open space and shoreline areas as needed for preservation purposes.

Approximately 119 acres of natural wetland/dunes area will be preserved within the East Cypress Corridor Specific Plan. Public access to these areas will be restricted. Observation points may be provided near areas where habitat is less sensitive.

9.10.5 TRAILS POLICIES

7.5.1 Construct trails to provide transportation, exercise and connection to nature and leisure opportunities for Oakley residents.

The East Cypress Corridor Specific Plan provides an extensive network of multi-use trails and bicycle trails for use by pedestrians, equestrians and bicyclists. The trail system links neighborhoods, parks, schools, commercial centers, and open space areas within the communi-

ty and ultimately to the Delta, Bethel Island and the recreational activities associated with these locations.

7.5.2 Construct short feeder trails to connect proposed developments to the regional trail system.

The East Cypress Corridor Specific Plan will provide numerous links to the City of Oakley trail system to be located within East Cypress Road and Bethel Island Road. The Specific Plan Area trails will connect to the proposed East Bay Regional Park District (EBRPD) regional trail to Rock Slough.

7.5.4 Provide public greenbelt corridors along major arterials of not less than 40 feet in width.

A landscape setback of 40 feet is provided along the north side of East Cypress Road west of Bethel Island Road and along the west side of Bethel Island Road north of East Cypress Road. For most of its length, Bethel Island Road is adjacent to open space or levee to the east.

7.5.6 Construct trails, whenever possible, for multiple uses (i.e. pedestrian, bicycle and equestrian).

The trail system planned for the East Cypress Corridor Specific Plan includes multi-use bicycle/ pedestrian/equestrian trails and multi-use bicycle/pedestrian trails.

7.5.7 Whenever possible, separate activities (i.e. pedestrian, bicycle and equestrian) of multi-use trails, by providing easements on each side of major arterials, to provide safe resolution of potential conflicts between users, animals and traffic.

The multi-use trails provided for bicycle/equestrian/pedestrian use are located on the tops of

the engineered levee system which is separated from streets within the Specific Plan Area. Other multi-use trails located within open space corridors are street separated where possible.

7.5.8 Construct trails, whenever possible, to be accessible to persons with disabilities.

The trail system planned for the East Cypress Corridor Specific Plan will be accessible to persons with disabilities wherever feasible. Levee trails will be accessible at roadway crossings, and by means of ADA compliant ramps.

7.5.10 Construct pedestrian trails to have a surface width of 6-8 feet (emergency and service vehicle accessible) providing sufficient space for two people to walk abreast.

The pedestrian trail system planned for the East Cypress Corridor Specific Plan will have a minimum paved width of 6 feet.

7.5.11 Construct Class 1 bicycle trails to have a surfaced width of 8 feet with shoulders of 2 feet to allow riders to pass safely.

Class I bicycle trails within the East Cypress Corridor Specific Plan will have a surfaced width of at least 8 feet, with 2 foot shoulders.

7.5.12 Provide clearance over trails of not less than 7 feet for pedestrian and bike trails, and not less than 9 feet for equestrian trails.

All trails within the East Cypress Corridor Specific Plan will be clear of any overhead obstructions.

7.5.13 Provide trail fences, directional signage, gates and bollards to protect the safety of trail users and adjacent properties. Provide equestrian trails to connect sta-

bles or ranchette development with regional trails.

The East Cypress Corridor Specific Plan includes design criteria for the development of trails within the Specific Plan Area specifying the types of edge treatment between the trail and adjacent uses to protect the safety of trail users and adjacent properties. Equestrian trails planned for the East Cypress Corridor Specific Plan will connect to regional trails.

9.11 HEALTH AND SAFETY ELEMENT

9.11.1 GEOLOGY AND SEISMIC HAZARDS POLICIES

8.1.2 Include a thorough evaluation of geologic, seismic and soils conditions at risk in all significant land use decisions (General Plan amendment, rezoning, etc., affecting 10 acres or more).

The EIR prepared for the East Cypress Corridor Specific Plan includes thorough evaluation of geologic, soils and seismic conditions of the Specific Plan Area, and recommends appropriate land use and site planning measures accordingly.

8.1.3 Require design of structures for human occupancy for satisfactory performance under earthquake conditions.

The project will comply with State of California earthquake construction standards and requirements.

9.11.2 FLOOD HAZARDS POLICIES

- 8.2.1 *Applications for development at urban or suburban densities in 100-year floodplain areas where it is a serious risk to life and property (see Figure 8-3) shall demonstrate appropriate solutions or be denied.*
- 8.2.3 *Buildings in urban development near the shoreline of the Delta and in flood prone areas shall be protected from flood dangers, including consideration of rising sea levels.*
- 8.2.4 *Habitable areas of structures near the shoreline of the Delta and in flood prone areas shall be sited above the highest water level expected during the life of the project, or shall be protected for the expected life of the project by levees of an adequate design.*

Measures to bring the East Cypress Corridor Specific Plan Area out of the 100-year floodplain include the construction of an extensive system of engineered levees to be developed to meet FEMA requirements for 100-year storm event flood protection.

- 8.2.5 *Rights-of-way for levees protecting inland areas from tidal flooding shall be sufficiently wide on the upland side to allow for future levee widening to support additional levee height.*

The proposed engineered levee system to be developed as part of the East Cypress Corridor Specific Plan will be approximately 150 feet wide at the base, with maintenance/access roads provided on both sides.

- 8.2.12 *In order to protect lives and property, intensive urban and suburban development shall not be permitted in reclaimed*

areas subject to 100-year flooding, unless flood protection in such areas is constructed. Typically, levees shall meet the standards of the U.S. Army Corps of Engineers, although “Dry Levees” that supplement existing levees may be allowed at the discretion of the City.

- 8.2.13 *Levees shall be properly engineered and designed to ensure protection against earthquakes, tsunamis and seiches.*

The majority of the levee system to be developed as part of the East Cypress Corridor will be constructed as an engineered earthen berm levee with approximately 150 feet at the base. The levee system will be constructed to meet the FEMA requirements for an urban levee system.

9.12 NOISE ELEMENT

9.12.1 PROTECTION OF RESIDENTS FROM HARMFUL NOISE POLICIES

- 9.1.1 *New development shall use the land use compatibility table shown in Figure 9-1 and the standards contained within Tables 9-1 and 9-3 for determining noise compatibility.*

The EIR prepared for the East Cypress Corridor Specific Plan utilized all General Plan standards and criteria in the evaluation of noise impacts and identification of noise mitigation measures for the planned community.

- 9.1.2 *New development of noise-sensitive uses shall not be allowed where the noise level due to non-transportation noise sources will exceed the noise level standards of Table 9-1 as measured immediately within the property line or within a designated outdoor activity area (location is at the discretion of the Community Development Director) of*

the new development, unless effective noise mitigation measures have been incorporated into the development design to achieve the standards specified in Table 9-1.

The EIR prepared for the East Cypress Corridor Specific Plan identifies noise impacts to residential units constructed adjacent to gas well sites resulting from drilling activities associated with gas well sites. Mitigation measures will be identified within the Final EIR to reduce noise levels resulting from drilling activities to residential units to an acceptable level, consistent with the City's gas drilling ordinance.

9.1.5 Noise created by new transportation noise sources shall be mitigated so as not to exceed the levels specified in Table 9-3 at outdoor activity areas or interior spaces of existing noise-sensitive land uses.

9.1.6 It is anticipated that roadway improvement projects will be needed to accommodate build-out of the General Plan. Therefore, existing noise-sensitive uses may be exposed to increased noise levels due to roadway improvement projects as a result of increased roadway capacity, increases in travel speeds, etc. It may not be practical to reduce increased noise levels consistent with those specified in Table 9-3. Therefore, as an alternative, the following criteria may be used as a test of significance for roadway improvement projects:

- Where existing traffic noise levels are less than 60dB Ldn at the outdoor activity areas of noise-sensitive uses, a +5dB Ldn increase in noise levels due to roadway improvement projects will be considered significant; and*
- Where existing traffic noise levels range between 60dB Ldn and 65 dB*

Ldn at the outdoor activity areas of noise-sensitive uses, a +3dB Ldn increase in noise levels due to roadway improvement projects will be considered significant; and

- Where existing traffic noise levels are greater than 65dB Ldn at the outdoor activity areas of noise-sensitive uses, a +1.5dB Ldn increase in noise levels due to roadway improvement projects will be considered significant.*

9.1.7 Where noise mitigation measures are required to achieve the standards of Tables 9-1 and 9-3, the emphasis of such measures shall be placed upon site planning and project design. The use of noise barriers shall be considered a means of achieving the noise standards only after all other practical design-related noise mitigation measures have been integrated into the project.

The EIR prepared for the East Cypress Corridor Specific Plan identifies noise impacts to adjacent residential units along portions of East Cypress Road and Bethel Island Road. Mitigation measures to include the construction of noise barriers varying in height from 6 feet to 8 feet are recommended at these locations to mitigate noise levels to an acceptable level.

9.13 HOUSING ELEMENT

10.1 Provide adequate sites, including land suitable for Multifamily rental housing, manufactured housing, housing for agricultural employees, emergency shelters, and transitional housing, to accommodate the City's share of regional housing needs through appropriate zoning and development standards.

10.1.1 Maintain an adequate supply of appropriately zoned land with available or planned public services and infrastructure to accommodate the City's projected housing needs for all income levels and for special needs groups.

The adoption of the East Cypress Corridor Specific Plan will result in the rezoning of approximately 1,542 acres of land for residential development at densities ranging from 1.0–12.0 per acre, and the provision for adequate infrastructure and community facilities to serve the residential development.

10.1.1 Promote development of affordable housing located in close proximity to services, shopping, and public transportation.

The East Cypress Corridor Specific Plan includes residential development areas planned in close proximity to new commercial services and shopping provided in a centrally located Village Center in Planning Area 1, a commercial site provided in the northerly portion of the Specific Plan Area in Planning Area 6, and a local serving retail center proposed for Planning Area 2.

10.1.3 Encourage the development of affordable housing to occur concurrently with the production of market-rate development. Such affordable dwellings shall be of the same quality of workmanship as market rate units and shall be spread throughout the development project as appropriate.

Approval of the East Cypress Corridor Specific Plan will provide approximately 1,542 acres of residentially zoned land in a range of densities to promote the development of a variety of housing types to address the needs of a wide variety of economic segments of the community. All residential development within the East Cypress Corridor Specific Plan will be master

planned so that all residences are developed with the same high quality of workmanship.

10.2 Encourage the development and provision of adequate housing stock to meet the needs of very low, low, and moderate-income households and special needs groups.

10.2.1 Facilitate and encourage the construction of housing affordable to very low, low, and moderate income households consistent with the City's identified housing needs.

Approval of the East Cypress Corridor Specific Plan will provide approximately 1,542 acres of residentially zoned land in a range of densities to promote the development of a variety of housing types available to address the housing needs of various economic segments of the market place. Within Planning Area 6, an affordable housing site may be available when the levee in this location is moved north.

10.2.2 Consider increased discretionary density bonus and/or an additional incentive that is higher than required by State law or other incentives to projects providing affordable units in addition to those required to receive a density bonus under State law.

The adoption by the City of the proposed Zoning Ordinance containing provisions for application of a residential unit density bonus will make available a tool for development of affordable housing within the East Cypress Corridor Specific Plan Area.

10.2.4 Increase access to homeownership by providing first-time homebuyer assistance for very low, low, and moderate-income households.

The development of residential units within the East Cypress Corridor Specific Plan will result in residences available to first time and move up homebuyers.

10.2.5 Ensure long-term affordability of very low and low income housing.

Low and very low income housing developed within the East Cypress Corridor Specific Plan utilizing public financing, funding and or other subsidies will abide by the terms for long term affordability as established in the terms of development funding, financing, and/or subsidy mechanisms.

10.2.6 Support energy conserving programs in the production and rehabilitation of affordable housing to reduce household energy costs.

Residential development within the East Cypress Corridor Specific Plan will incorporate energy conserving devices wherever feasible and in accordance with applicable City and State requirements.

10.2.7 Encourage housing opportunities to include, but not limited to, agricultural workers, homeless, seniors, single person headed households, large families, and persons with disabilities.

The adoption of the East Cypress Corridor Specific Plan by the City of Oakley will result in development of a variety of housing types available to address the housing needs of various economic segments of the market place.

10.2.8 Assist very low and low income households in locating affordable housing and finding sources of assistance with housing payments and rent.

The developers of the East Cypress Corridor Specific Plan will work cooperatively with the City of Oakley to assist in the distribution of housing resources materials provided by the City to assist the location of affordable housing within the East Cypress Corridor Specific Plan Area.

10.3.3 Provide consistent and predictable policy direction for residential project applicants in the development and design standards and decision-making process.

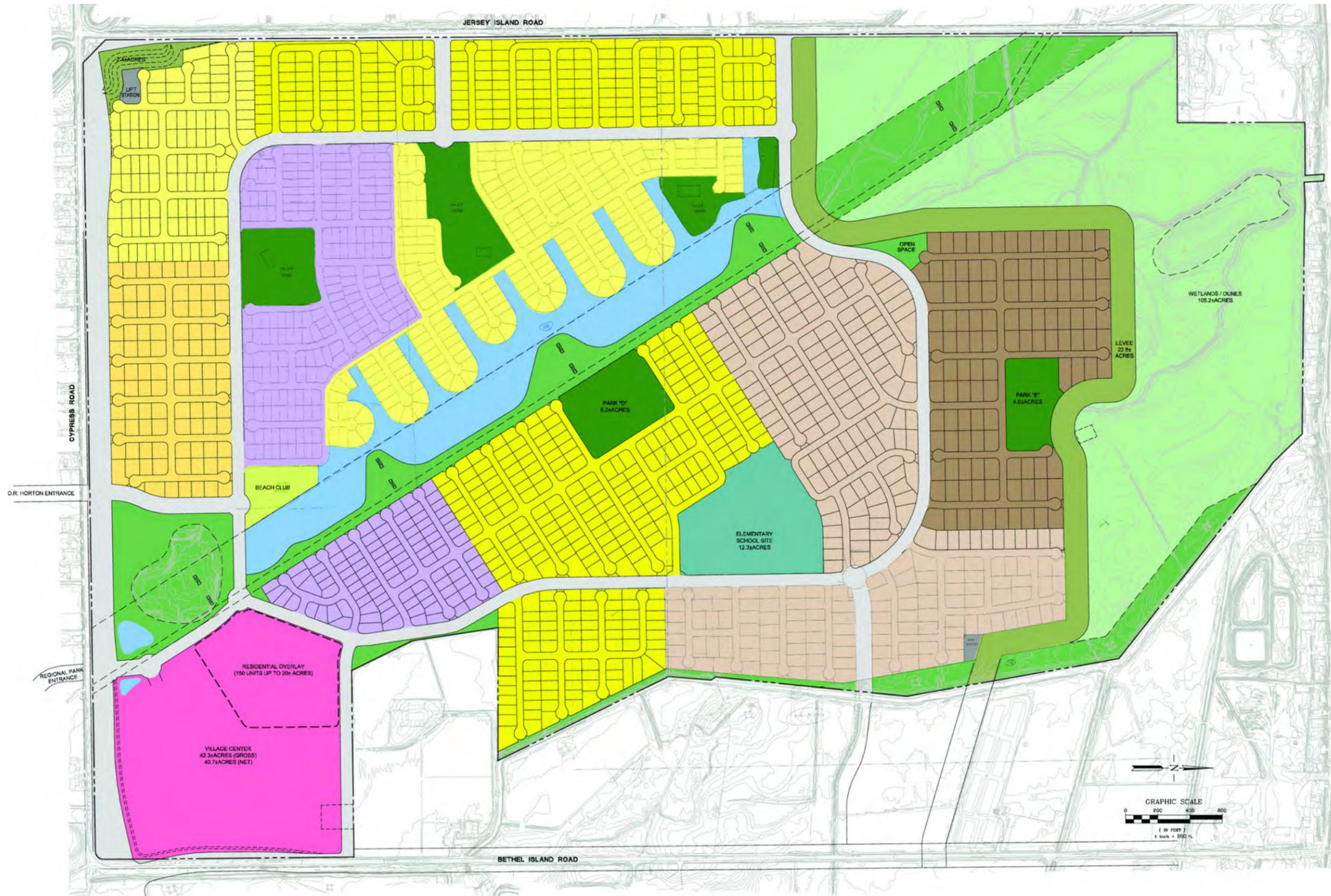
The East Cypress Corridor Specific Plan includes clear policy direction, land use and infrastructure plans, development regulations, and design guidelines to guide project builders within the Specific Plan Area.

10.4.3 Recognize mobile home parks as a source of affordable housing.

The East Cypress Corridor Specific Plan provides for existing and future mobile home development by designating a mobile home land use within the Specific Plan Area.

10.5.1 Prohibit discrimination in the sale or rental of housing to anyone on the basis of race, color, ancestry, national origin, religion, disability, sex, familial status, marital status, or other such arbitrary factors.

Sales and rental of housing within the East Cypress Corridor Specific Plan Area will adhere to Federal and local fair housing policies and guidelines.



LEGEND:

LAND USE	LOT SIZE	# UNITS	AREA (Acres)	% AREA
Single Family Residential - Medium Density	80'x 125' (10,000 sft)	119	37.69	5.36 %
Single Family Residential - Medium Density	70'x 115' (8,050 sft)	270	74.87	10.64 %
Single Family Residential - Medium Density	80'x 100' (8,000 sft)	95	24.81	3.53 %
Single Family Residential - High Density	55'x 110' (6,050 sft)	297	85.00	12.04 %
Single Family Residential - High Density	60'x 100' (6,000 sft)	464	13.76	1.94 %
Multi-Family Residential - Low Density	50'x 90' (4,500 sft)	294	50.87	7.20 %
Village Center		150*	42.33	6.01 %
Beach Club			2.58	0.37 %
School			12.35	1.75 %
Neighborhood Park			25.01	3.55 %
Open Space			55.86	7.91 %
Wetlands / Dunes			105.20	14.95 %
Levee			30.31	4.31 %
Levee			25.16	3.56 %
Streets (Cypress/Bethel Island/Entry/Loop)			54.15	7.60 %
Lift Stations			0.89	0.13 %
TOTAL		1689*	703.76	100%

*Units in the event area are private Village Center. Site is only used for housing. If a commercial entity up to 250 units of the Village Center may be residential with up to 150 units (Sports Park Site 27-2).

PARK SUMMARY

Neighborhood Parks	AREA (Acres)	NET AREA (Acres)
Park "A"	5.0	5.0
Park "B"	6.4	5.1*
Park "C"	3.4	2.1*
Park "D"	6.2	6.2
Park "E"	4.0	4.0
TOTAL	25.0	22.4

*The net area assumes that 1.2 acres will be reserved for gate walls.

PARK REQUIREMENT

- Open Space - 1 acre per 1,000 residents
- Community Park - 2 acres per 1,000 residents
- Neighborhood Park - 3 acres per 1,000 residents
- 3.28 residents per household (General Plan & ABAQ)

Lesher/Dal Porto Park Requirement:

- Open Space - 5.8 acres
- Community Park - 11.2 acres
- Neighborhood Park - 16.8 acres

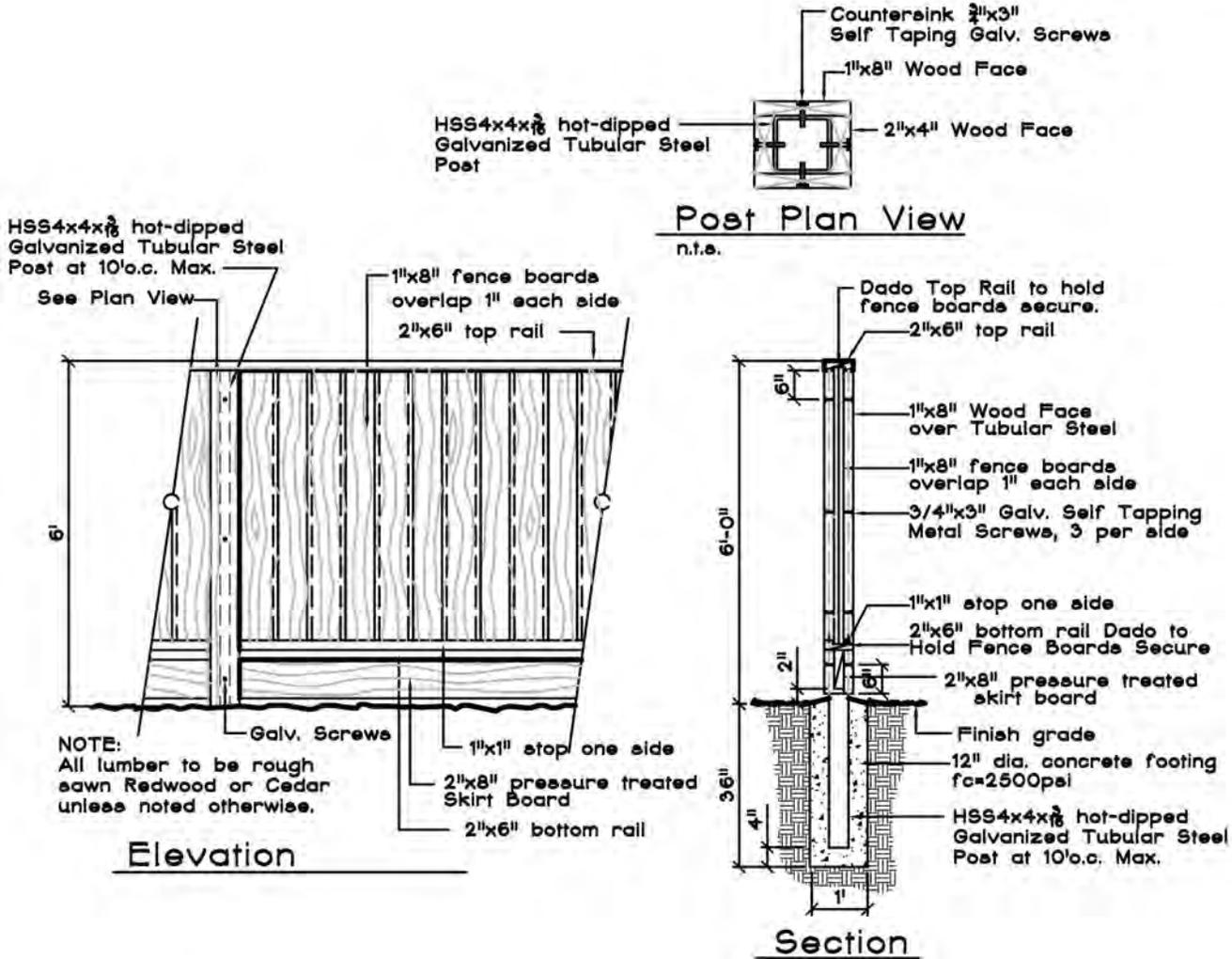
Planning Area 1
KB Home Conceptual Plan



Planning Area 3
D.R. Horton Conceptual Plan



Planning Area 4
Bethel Island, LLC Conceptual Plan



6' Wood Fence

n.t.s.

Source: HWA Landscape Architecture Site Planning

City Council Resolution No. 31-06
EXHIBIT D
[Modifications to the Specific Plan Adopted by
the City Council on March 13, 2006]

PC Recommendations:

Correct the commercial acreage to 93 acres on page 2 of the Errata Sheet for the Revised Specific Plan. The number is correct on page 1.5 of the Specific Plan.

Correct the reference to numbers of gas wells from ten to eleven on page 4 of the Errata Sheet for the Specific Plan and on page 2.11, first paragraph.

Correct page 9 of the Errata Sheet and page 3.38 of the Specific Plan, Section 3.7.2, fifth paragraph to eliminate hyphen in south-easterly and to correct “pod” to “pond”.

Page 5.86:

Modify language to provide flexibility in terms of the future service provider to the area for recycling and solid waste disposal. In Section 5.6.4 (Solid Waste) of the Specific Plan, make the following change:
Oakley Disposal provides solid waste collection services for the City and is anticipated to service the Specific Plan Area.

Page 7.133:

Modify the design guidelines in the Specific Plan to require storm drain markers that are upgraded from the standard specification to provide permanent and artistic plaques such as decorative stamped brass or cast iron. In Section 7.2.2.6 (Local Streets) add the following paragraph at the end of the section:

All storm drain inlets should include storm drain markers upgraded from the standard polyurethane material to a decorative, more durable material, such as stamped brass or cast iron, subject to the review and approval of the City Engineer.

Staff Recommendations:

Gas well sites may be enlarged or shifted based upon the site specific needs and findings as part of subsequent conditional use permits that are reviewed and approved pursuant to the Oil and Gas Chapter of the Zoning Code and the Specific Plan. It may be helpful to clarify questions and concerns expressed at the public hearing to add language to Section 4.7 of the Specific Plan to indicate the provisions of the Zoning Code, regulating Oil and Gas Drilling, shall prevail in the event of any conflict with the Specific Plan. At end of Section 4.7, staff recommends the following:

Page 4.55, paragraph added:

Gas well sites may be enlarged or shifted based upon the site specific needs and findings as part of subsequent conditional use permits that are reviewed and approved pursuant to the Oil and Gas Chapter of the Zoning Code and the Specific Plan. In the event of a conflict between the Specific Plan and the provisions of the Zoning Code that regulate Oil and Gas Drilling, the Zoning Code provisions shall control.

The Specific Plan establishes that the “Roadway Hierarchy Diagram” (Exhibit 32 in Section 7), establishes the hierarchy and general location of roadways in the Specific Plan Area. Additional clarification to Section 5.1 of the Specific Plan may be appropriate to identify that the precise alignment of roads may vary to accommodate sloping terrain, site constraints, and existing development. Staff recommends adding the following at the end of the paragraph under Section 5.1:

Page 5.69, text added:

The precise alignment of roads may vary to accommodate sloping terrain, site constraints, and existing development.

The developers continue to explore the preservation of additional wetlands by shifting development even further towards the interior. In this case, it may be beneficial to add flexibility to Section 8.5 of the Specific Plan to allow the transfer of the residential density allocation within the Specific Plan area within the density ranges indicated on the Land Use Plan to accommodate additional wetland preservation. Staff recommends inserting the following as the new third sentence in Section 8.5:

Page 8.170, sentence added:

If the development potential is reduced due to environmental constraints, the residential units may be reallocated to other Planning Areas provided densities stay within the ranges shown on the Land Use Map and the total number of dwelling units for the Specific Plan is not exceeded.

Staff has discovered that building height standards are not uniform for multi-family attached housing in the Multi-Family Land Use Areas. The design guidelines call for multifamily buildings to be articulated to create visually interesting elevations. The ability to capture articulation in vertical elements adds features such as dormers, focal points, and opportunities to create more open space and recreational amenities on the site. Staff recommends the building height for the Multi-Family Residential – Medium Density be the same as for High Density such that both have maximum height not to exceed 40 feet or 3 stories. Staff recommends on page 6.93 of the Specific Plan that in:

Footnote changed:

Table 3, Multi-Family Attached, replace footnote 2 with footnote 3 on the “Max. Height” column for the MM land use designation.

City Council Recommendations:

1) *Page 8.174 of the Specific Plan, last sentence of Section 8.10.2.2 Commercial development is changed (last two lines) as follows (similar to Section 2.72):*

“....the Village Center site may could be filed for processing with the City of Oakley.”

2) *Page 6.96*

of the Specific Plan, should be changed to revise Table 4 - minimum site area from 20 acres to 40 acres, and add a footnote 7 (which should read the same as footnote 2 in Table 1).

4) Exhibit 4, the Illustrative Land Use Master Plan will be revised to indicate proper coloring in the map for the land use designations shown in the legend.

5) *Page 5.69 of the Specific Plan, second paragraph of Section 5.1.1.1 (East Cypress road, third sentence should be revised as follows:*

This frontage road, as indicated on Exhibit 6 and Exhibit 22, will provide the residents proper access to their properties without interfering with the traffic pattern on the new East Cypress Road.

6) Page 7.101 (last bullet) , page 7.103 (fifth bullet) and page 7.104 (fifth bullet) shall all be replaced with the following:

In no case, shall any side yard setback be less than 5 feet, including any building elements such as entertainment systems and fireplaces. No encroachments shall be allowed into the 5-foot setback.

7) Page 7.119, second paragraph under Section 7.2.2.1 (East Cypress Road), revise to include the following at top of page:

“...determined by a noise study. The southern setback will be 30’ wide, allowing for a 6’ wide pedestrian trail. The rows of columnar trees will create a visual buffer to separate the existing right-of-way. The existing roadway will be further separated from the new East Cypress right-of-way by a 10’ landscape setback. The existing residential neighborhood to the south will be buffered from the traffic by a community theme wall of a minimum 6’ in height, as determined by a noise study.