

RIVER OAKS CROSSING SPECIFIC PLAN

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SECTION 1. EXECUTIVE SUMMARY

The City of Oakley and Oakley Redevelopment Agency sponsored the preparation of this Specific Plan to guide future development of the Cline Property, located along the north side of Main Street, east of Bridgehead Road at the City's westerly entrance from Highway 160. Pursuant to policies adopted as part of the City of Oakley 2020 General Plan, this Specific Plan establishes a set of commercial uses for the property, provides standards for site development and future building improvements, and identifies roadway and infrastructure improvements to serve the 76-acre project area.

The River Oaks Crossing Specific Plan includes an approved Development Plan (A), as shown in Figure SP1-1, which identifies planned commercial building envelopes, internal site circulation, pedestrian plaza, landscape and parking areas, and planned intersection improvements to provide access to the site. Development Plan A is designed to implement policies contained in the Oakley 2020 General Plan calling for community-serving retail uses at a key entrance to the City.

A range of initial land use concepts were developed in the planning process, and reviewed by the City / Redevelopment Agency at a series of public workshop meetings held on December 8, 2003, April 26, 2004 and October 22, 2005. These initial concepts were refined through further technical analysis, and through additional comments received during the scoping meeting for the environmental review process held on November 12, 2003. The land use plan was again refined in 2006 through input from City staff and property owners, and after focused technical studies completed as part of the environmental impact analysis.

Key Components of Development Plan

The approved Development Plan and the implementation policies presented in this specific plan incorporate the following:

Major Retail Envelopes: Accommodation of 3-4 principal building sites for Major Retail uses. The maximum aggregate floor area approved for Major Retail Uses, as shown in Table SP 1-1, is 520,000 square feet.

Secondary Retail Envelopes: Secondary Retail and service uses, including shops, restaurants, and an optional hotel use accommodating up to 100 rooms (40,000 square feet), strategically distributed over the site, providing for a total maximum of 170,000 square feet. The building forms depicted in Development Plan A are illustrative; smaller buildings may be aggregated and re-distributed, as provided for in Section 7.3.

Approved Building Square Footage: Combined retail uses totaling a maximum of 690,000 square feet, with an overall floor area ratio (FAR) of approximately 0.21. As discussed in Section 3, Development Plan A reflects

690,000 square feet of aggregate commercial uses, along with a corresponding balanced parking and circulation design. The acceptable range of aggregate building area for the specific plan site is from 630,000 to 690,000 square feet, as shown in Table SP 1-1.

Circulation Improvements: Internal circulation, parking, and project entry improvements, reflected in Development Plan A are designed to accommodate overall project needs, while coordinating with anticipated traffic growth and improvements to Bridgehead Road and Main Street. As outlined in Section 4, transportation system improvements include new signal lights at five locations within or fronting the site, including: (a) the Bridgehead Road entrance; (b) the new intersection to be created opposite Sandy Lane; (c) the Main Street project entrance located mid-block between Sandy Lane and Live Oak Avenue; (d) the Main Street project entrance located to the east of Live Oak Avenue; and (e) an internal intersection located north of Main Street on Live Oak Avenue (subject to future extension of Live Oak Avenue north into the DuPont Property). In addition, the project will provide modifications to the Main Street / Live Oak Avenue intersection which the City is currently in the process of signaling, and will also contribute to modifications planned for a number of other signal lights and intersection improvements, including the signal light at Main Street and Bridgehead / Neroly Road.

Live Oak Avenue Extension: An extension of Live Oak Avenue is planned as part of the Specific Plan, consistent with policies reflected in the Oakley 2020 General Plan. Live Oak Avenue will extend north from the new Main Street intersection, and connect with the principal east-west project driveway at a new intersection. Development Plan A reflects a northerly continuation of Live Oak with a future elevated over-crossing of the adjoining railroad tracks, to provide for access through the DuPont Property to the north of the specific plan site. Alternative Development Plan B addresses the internal circulation, parking and building opportunities to be considered for the Cline Property, in the event that Live Oak Avenue is not extended north into the DuPont Property. This alternative is intended exclusively to address internal site planning issues, and may only be implemented if it is subsequently determined through a General Plan-level analysis that the extension of Live Oak Avenue is not necessary. Signalization of the internal project driveway intersection at Live Oak Avenue is only needed at such time as Live Oak Avenue is extended north into the DuPont Property.

Purpose of Alternative Development Plan B: Any amendment of the Oakley 2020 General Plan that eliminates the extension of Live Oak Avenue north from Main Street over the railroad tracks shall automatically permit implementation of Alternative Development Plan B. While eliminating the elevated extension of Live Oak Avenue over the tracks may provide additional space for Secondary Retail pads, in no event shall the total floor area of development in the Specific Plan area exceed 770,000 square feet.

Site Improvements and Development Standards: The Specific Plan identifies planned on- and off-site improvements, and establishes development standards and design guidelines providing for a range of features, including public plazas and water-features, placement and design of future retail buildings, separated access and screening of primary truck loading facilities, facility maintenance, and development phasing. These Standards also include provisions for energy conservation and waste reduction.

Approved Land Uses: The uses diagrammed in Development Plan A (Figure SP 1-1) are further summarized in Table SP 1-1, the Summary of Specific Plan Land Use Alternatives. As further discussed in Land Use Section 3 and analyzed in the project EIR, a range of land use alternatives were considered for the project. Development Plan A represents a balanced parking and circulation design, which distributes both Major and Secondary Retail uses in an arrangement which is responsive to the policies advanced in this Specific Plan and adopted in the Oakley 2020 General Plan. This approved design is intended to retain the flexibility to accommodate aggregate building area increases or reductions, consistent with Table SP 1-1. Development Plan A also incorporates several site planning options, and a range of permitted and conditionally permitted uses which are further described in Section 3.4.

Structure of Document

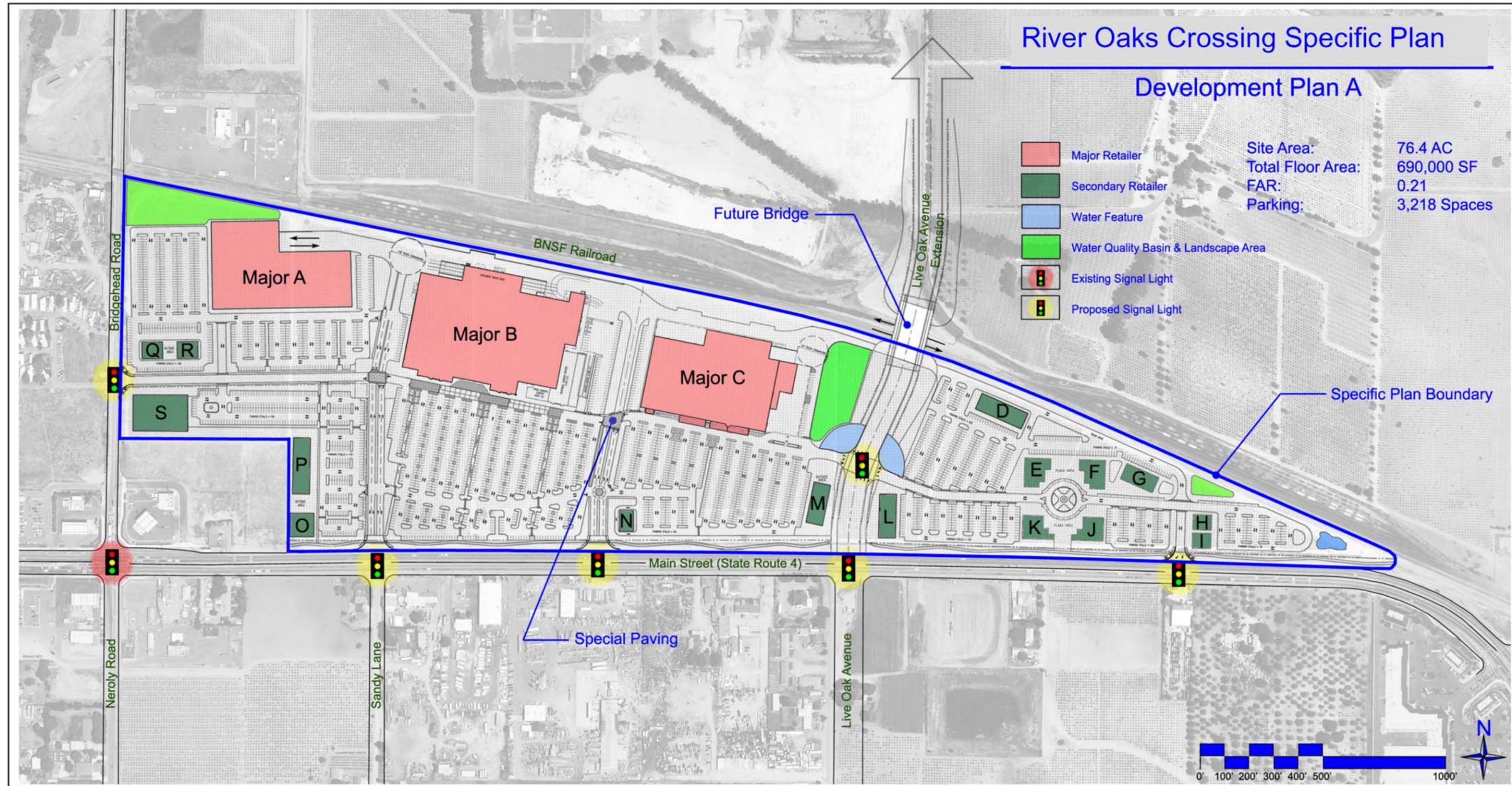
The River Oaks Crossing Specific Plan has been organized into seven sections. As required by Government Code Section 65451, this document consists of a diagram (the approved Development Plan) and policies which provide for the location and improvement of commercial land uses, and for supporting infrastructure and services. Introduction Section 2 reviews the local and community planning context and purpose of the Specific Plan, and discusses the relationship of this document to the City's Zoning Ordinance and General Plan policies. Section 3, Land Use, describes existing conditions and surrounding land uses, introduces the Development Plan and the permitted and conditional uses, and discusses overall development potential and employment opportunities. Section 4 provides a discussion of internal site circulation in relationship to a much larger transportation system network. The Circulation section provides for planned widening of Main Street, extension of Live Oak Avenue through the site, and accommodation of pedestrian, bicycle and public transportation within the project area. Section 5 identifies the range of public services and infrastructure needed to serve the project area, and Section 6 provides further details of development phasing, building envelopes, and standards to guide site improvements and future building designs. The final chapter of this document (Section 7) identifies strategies for implementation, including coordination with future individual building entitlements, zoning requirements, quality maintenance of improvements, and administration of the Specific Plan. Section 7.3 provides standards for administrative and Planning

Commission-level authorization to grant minor modifications to the approved Development Plan.

Preparation of Tiered Project-Level Environmental Impact Report

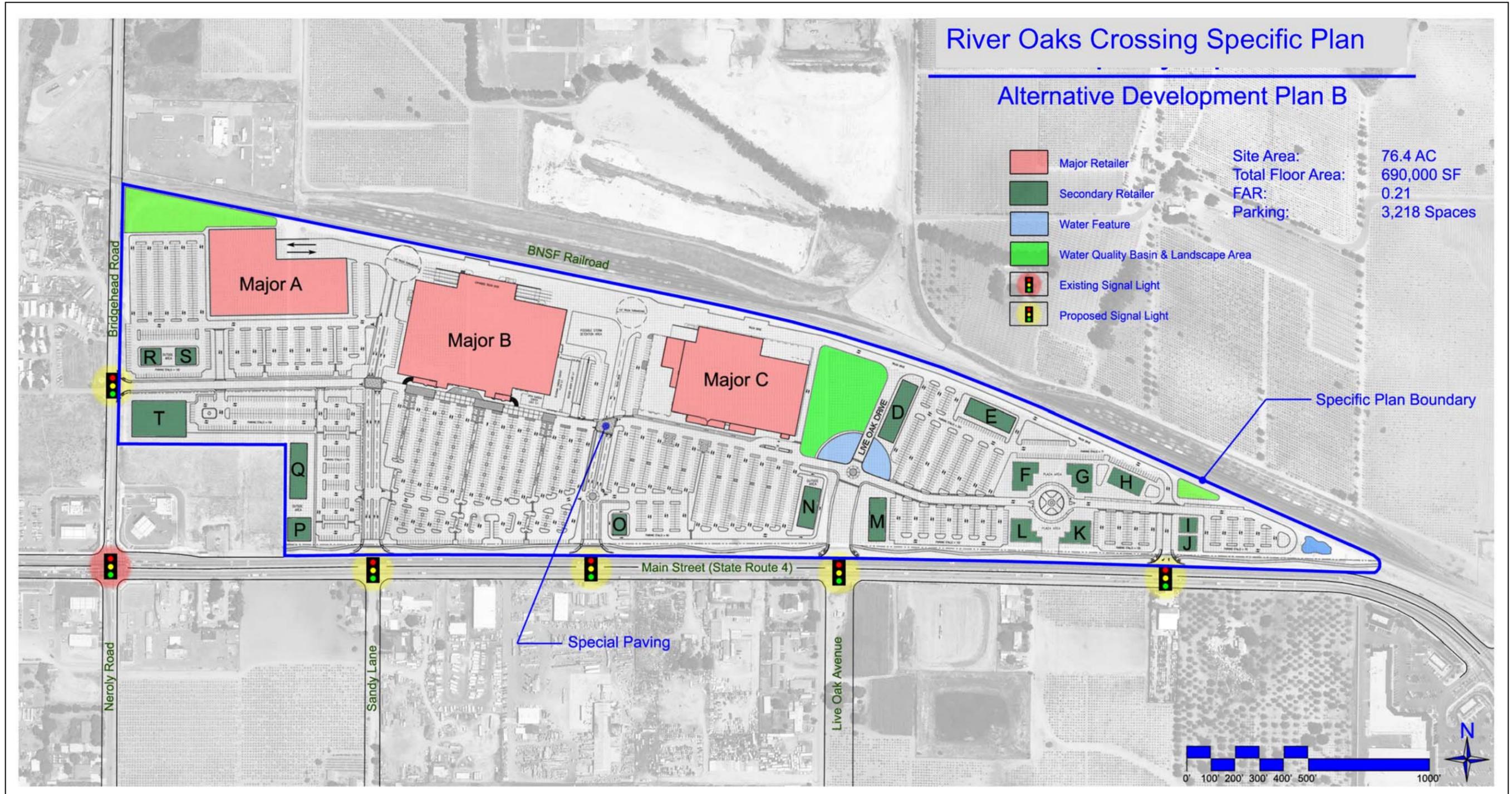
Because future development contemplated in the Specific Plan could have a variety of potentially significant environmental effects, as initially outlined in the Initial Study (attached to the Notice of Preparation), a project-level Environmental Impact Report has been concurrently prepared to accompany the Specific Plan. The EIR has been prepared in accordance with the requirements of the California Environmental Quality Act (CEQA) and analyzes contemplated development and uses of the project site in relationship to current and planned future cumulative development within Oakley and the surrounding area. This project EIR tiers from the certified Oakley 2020 General Plan EIR, and is supported by a series of in-depth studies, which are separately bound in a Technical Appendix. The Specific Plan incorporates a number of design features, development standards, planned improvements and policies that serve to avoid or substantially lessen the environmental effects identified in the Draft EIR.¹

¹ Throughout this document, the term “Draft EIR” and “DEIR” may be interchanged. Both are used for the term “Draft Environmental Impact Report”.



Source: Richard T. Loewke, AICP.

FIGURE SP 1-1: Development Plan A



Source: Richard T. Loewke, AICP.

FIGURE SP 1-2: Alternative Development Plan B

Table SP 1-1 Summary of Specific Plan Land Uses				
Land Uses²	Development Plan A¹		Alternative 1 (Lower Intensity)	
Major Retailers ³				
	A	120,000	A	100,000
	B	230,000	B	230,000
	C	170,000	C	170,000
Subtotal Major Retailers		520,000		500,000
Secondary Retailers ⁴				
	D	25,000	D	5,000
	E	12,000	E	9,000
	F	12,000	F	9,000
	G	9,000	G	7,000
	H	4,000	H	4,000
	I	4,000	I	4,000
	J	8,000	J	6,000
	K	8,000	K	6,000
	L	10,000	L	8,000
	M	10,000	M	8,000
	N	5,000	N	4,000
	O	7,000	O	4,000
	P	16,000	P	8,000
	Q	5,000	Q	4,000
	R	5,000	R	4,000
Subtotal Secondary Retailers		140,000		90,000
Hotel ⁵	S	30,000	S	40,000
Total Floor Area		690,000		630,000
Floor Area Ratio (FAR)		0.21		0.19
Land Use Emphasis	Mixed Retailing			Expanded Hotel and Restaurants

Notes:

1 Development Plan A and Alternative Development Plan B are substantially similar in land use mix; Plan B is likely to result in a slightly more efficient land use pattern and minor increases in square footages, as a result of eliminating the Live Oak overpass; both options are diagrammed approx. 690,000 SF in aggregate building area. Section 7.3 outlines procedures for minor modifications to the approved Development Plan.

2 All land uses are subject to administrative verification of consistency with the approved Development Plan; as well as Architectural Review in accordance with Section 7.1. Uses shall conform to the standards and list of approved uses as outlined in Section 3.4.

3 See Specific Plan Section 3.3 for an explanation of Major and Secondary Retail land uses.

4 The overall focus of retail and related uses within the Specific Plan vary by alternative. The project objectives as described in Specific Plan Section 1 summarize the land use emphasis reflected in the Development Plan.

5 Development Plan A as shown in Figure SP 1-1 includes an option for a hotel use of up to 75-100 rooms. This use is subject to certain restrictions and special mitigation measures as outlined in Specific Plan Section 3.3, and may be substituted for a similar amount of retail square footage, or enlarged to 100 rooms as provided for in Alternative 2.

Table SP 1-2 Summary of Specific Plan Parking, Circulation, and Development Standards		
	Development Plan	Alternative 1 (Lower Intensity)
Total Gross Floor Area	690,000 SF	630,000 SF
Parking ¹		
Retail Uses (4.5 per 1,000 SF plus turnover factor)	3,119	2,788
Restaurant Uses	Limited to 10% of GFA	Limited to 10% of GFA
Hotel ²	99	132
Required Parking Supply	3,218 parking spaces	2,920 parking spaces
Building Height Maximum ³	40 feet	40 feet
Floor Area Ratio (FAR) ⁴	0.21	0.19
Setbacks to Public Streets ⁵	20 feet	20 feet
Signalized Intersections ⁶	6	6

Notes:

1 Overall parking supply has been determined according to ITE standards as outlined in Specific Plan Section 4, based on 4.5 spaces per 1,000 gross square feet of retail space, plus 1.25 space per hotel room, with an additional minimum 5% vacancy rate for turnover. This requirement exceeds the City Code minimum requirements outlined in Specific Plan Section 4.6.

2 A hotel use is included in each Alternative with square footages ranging from 30,000 to 40,000 SF, and between 75 and 100 rooms (See Table SP 1-1). The alternatives provide for optional retail space in lieu of the hotel use.

3 Maximum retail building height may be varied for a hotel use of up to 4 floors, and for architectural features designed in accordance with Specific Plan Section 6.5 (Policy e.14).

4 Floor Area Ratio limitations assume one-story retail buildings, and exempt hotel rooms on floors above the ground level. Two-story retail buildings may be considered as part of the Architectural Review process, subject to the prescribed FAR limitation, overall GFA, and building height limit.

5 Interior and rear setbacks to be determined by the Building Envelopes as approved on the Development Plan. These Envelopes shall be consistent with the Development Plan, as outlined in Specific Plan Section 3.3. In addition to the front street right-of-way setback, an average-width landscape planter requirement of 25 feet is required.

6 The intersection signal lights may be phased as provided for in Specific Plan Section 6.1. The "future signal" located at the easterly project entrance may be deferred until such time as development occurs on the south side of Main Street at this location. See Section 4 for further details.

SECTION 2. INTRODUCTION

2.1 Planning Context

The River Oaks Crossing Specific Plan focuses on a 76.4-acre site situated on the north side of Main Street, east of Bridgehead Road, and south of the BNSF Railroad line. As shown in Figure SP 2-1, the project site is situated at the westerly entrance to the City of Oakley from Main Street, adjoining Highway 160. The 76.4-acres include Assessors Parcels 037-040-015 and 037-040-007, and excludes the existing service station and drive-through coffee bar located at the northeast corner of Bridgehead Road and Main Street. No building improvements exist on the property, which is currently farmed.

Preparation of the Specific Plan was authorized by the City of Oakley / Redevelopment Agency in 2003, to fulfill three primary objectives:

- (a) Establish a set of complementary commercial uses for the property suitable for strengthening the City / Agency tax base while also generating local employment opportunities;
- (b) Quantify and define standards for site development and future building improvements targeted at maximizing market opportunities while minimizing adverse environmental effects; and
- (c) Identify needed roadway and infrastructure improvements, and related services, appropriate to serve planned development within the project area.

As further discussed below, the Specific Plan project area is located entirely within the City of Oakley's incorporated boundaries, and currently classified on the Oakley 2020 General Plan for Commercial use. The site is also located within the City's Redevelopment Area Amendment No.1 boundaries.



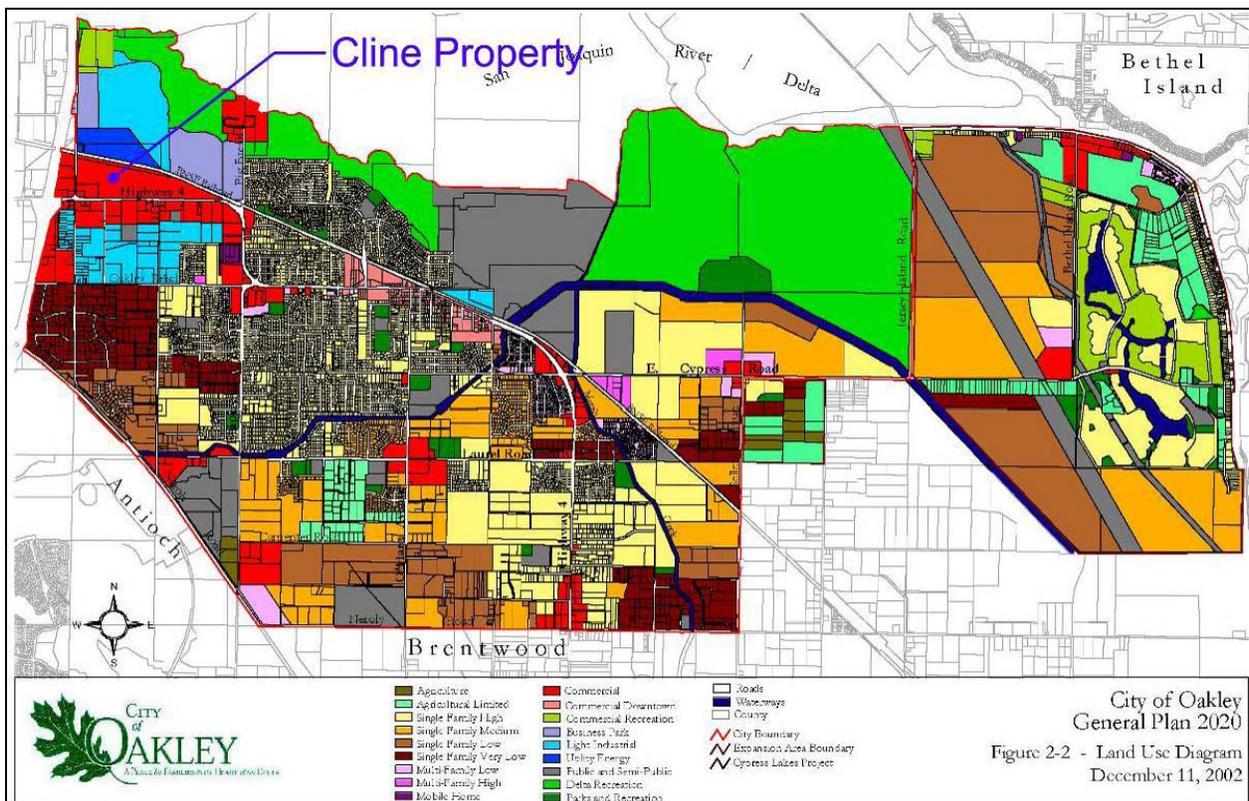
Figure SP 2-1: Specific Plan Location Map

2.2 Purpose of Specific Plan

The Specific Plan establishes detailed land use and development policy for the entire project site, and directs the remaining steps to be followed in the entitlement process before construction can begin, all in accordance with the Oakley 2020 General Plan. As provided for in Sections 65450-65457 of the California Government Code, following adoption of the Oakley 2020 General Plan, the City Council and Redevelopment Agency called for preparation of a specific plan to provide for the systematic implementation of General Plan policies affecting the Cline Property. The Specific Plan includes both a Development Plan and policies to implement and supplement the plan. These policies provide for the location and improvement of commercial land uses, and for supporting infrastructure and services. A Draft tiered project-level Environmental Impact Report (DEIR) was prepared concurrently with the draft Specific Plan; certification of a Final EIR is a prerequisite for adoption of the Specific Plan.

All future actions to be taken by the City of Oakley and Oakley Redevelopment Agency, including capital improvement projects, subdivision maps, and land use and zoning permits must be consistent with the Specific Plan. The Specific Plan includes a set of development standards and design guidelines to establish a common architectural theme and to coordinate the placement of buildings on the property. It is intended that if future commercial building projects are found to be consistent with the approved Development Plan and the policies outlined in this Specific Plan, they will be subject to Architectural Review, as provided for in Section 7.1. No further environmental documentation will be required for development applications consistent with this Specific Plan.

Future applications for building and related construction permits will be subject to a finding of consistency with the adopted Specific Plan, including the approved Development Plan. As provided in Section 7.3, minor changes to the Development Plan may be approved by the Community Development Director or Planning Commission. Other changes to the Development Plan, Development Standards or policies outlined in the Specific Plan must be considered by the City as formal amendments to the Specific Plan.



Source: City of Oakley 2020 General Plan.

Figure SP 2-2: Oakley 2020 General Plan Land Use Diagram

2.3 Relationship to Zoning, General Plan and Redevelopment Area Plan

The City of Oakley adopted its first comprehensive General Plan in December of 2002. The new General Plan covers the time horizon of 2002 through 2020. This action was supported by advance preparation and certification of a comprehensive program-level EIR (SCH No. 2002042134). The adopted Oakley 2020 General Plan Land Use Diagram is shown in Figure SP 2-2. Both the General Plan and supporting environmental analysis anticipated development of the 76.4-acre Cline Property with “Commercial” land uses. The current “Heavy Industry” zoning will be amended as part of the Specific Plan approval process to the “Specific Plan No. 2” (SP-2) District, which will be concurrently adopted for purposes of implementing the Specific Plan. The Specific Plan includes a Development Plan which will be concurrently approved. Individual applications for Architectural Review will be processed by the City in conjunction with all future individual development projects on the site. These applications must be consistent with the adopted Specific Plan in order to qualify for expedited processing as provided for in Section 7.

Figure SP 2-3 provides an enlarged view of the current General Plan land use designations on the Cline Property and adjoining sites. The “Commercial” land use designation applies both to the Specific Plan project area and other properties to the south (across Main Street), and to the west (across Bridgehead Road). Other properties to the north and south of the Specific Plan site are classified for “Utility Energy”, “Business Park” and “Light Industrial” uses. These energy and industrial use designations apply to the former DuPont Chemical Plant property to the north of the railroad tracks, and to properties extending between the frontage of Main Street and Oakley Road on the south. Highway 160 forms the City’s westerly boundary with Antioch, and Big Break Road forms the easterly edge of the study area. Existing residential uses located further to the east of the study area (shown in Figure SP 2-3) were evaluated in the planning process and carefully considered in the concurrent environmental evaluation.

Figure SP 2-4 identifies the current Zoning District boundaries on and directly adjoining the project site. The Cline Property, as well as adjoining properties north of the railroad right-of-way, and those fronting on the west side of Bridgehead Road, are all currently zoned Heavy Industry (“HI”). Single-Family Residential (“R-6”) zoning currently applies to property on the north side of Main Street, east of the Cline Property, and Planned Unit Development (“P-1”) zoning occurs to the south of Main Street. Additional property located southwest of Main Street and Neroly Road is zoning Light Industry (“LI”). The Specific Plan project would result in the establishment of “SP-2” Zoning on the 76.4-acre project site.

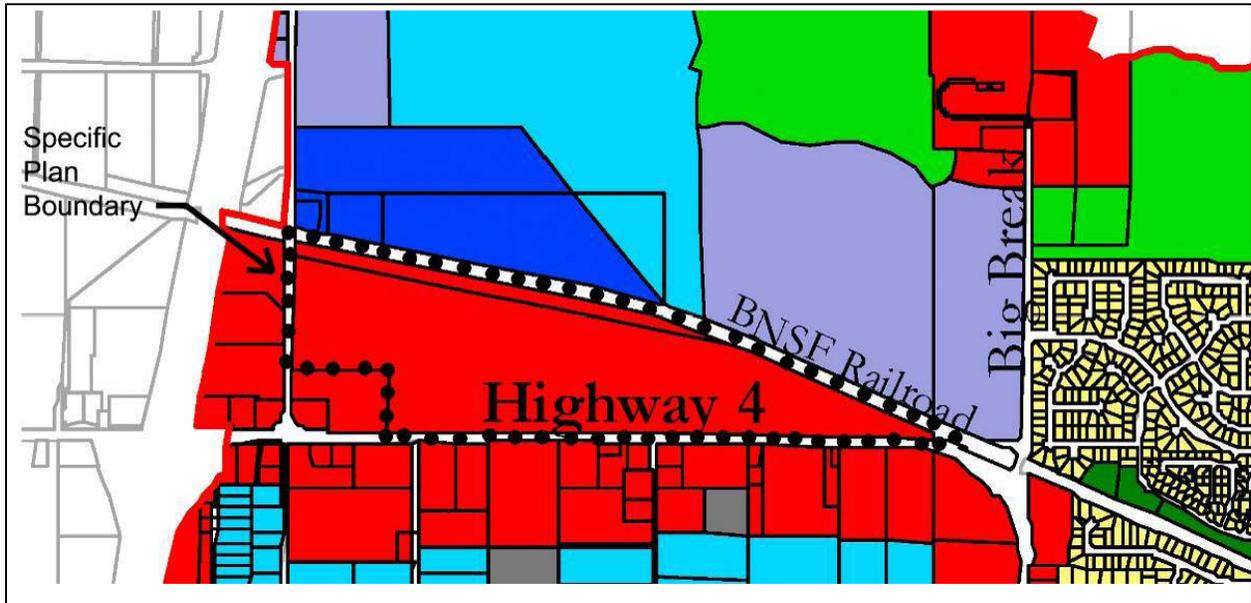
The Specific Plan serves the function of providing detailed guidelines for development of the project site with land uses and according to development standards which are consistent with overall policy direction provided through the General Plan. In this context, the Specific Plan provides for implementation of General Plan policies at a

coordinated and focused project level. The accompanying project-level EIR tiers from the certified General Plan EIR and incorporates a summary of applicable General Plan policies, and examines the consistency of corresponding components of this Specific Plan.

The 76.4-acre Specific Plan site was added to the Oakley Redevelopment Area in 2001, as part of Amendment No. 1. As shown in Figure SP 2-5, the Amendment No. 1 boundaries extended north beyond the Specific Plan site, to also include the Cline Vineyards (northeast of the Specific Plan site) and the DuPont Property (north of the Specific Plan site and west of the Cline Vineyards). Consideration of Amendment No. 1 included review of overall economic development policies for the community, in relationship to agricultural resources.

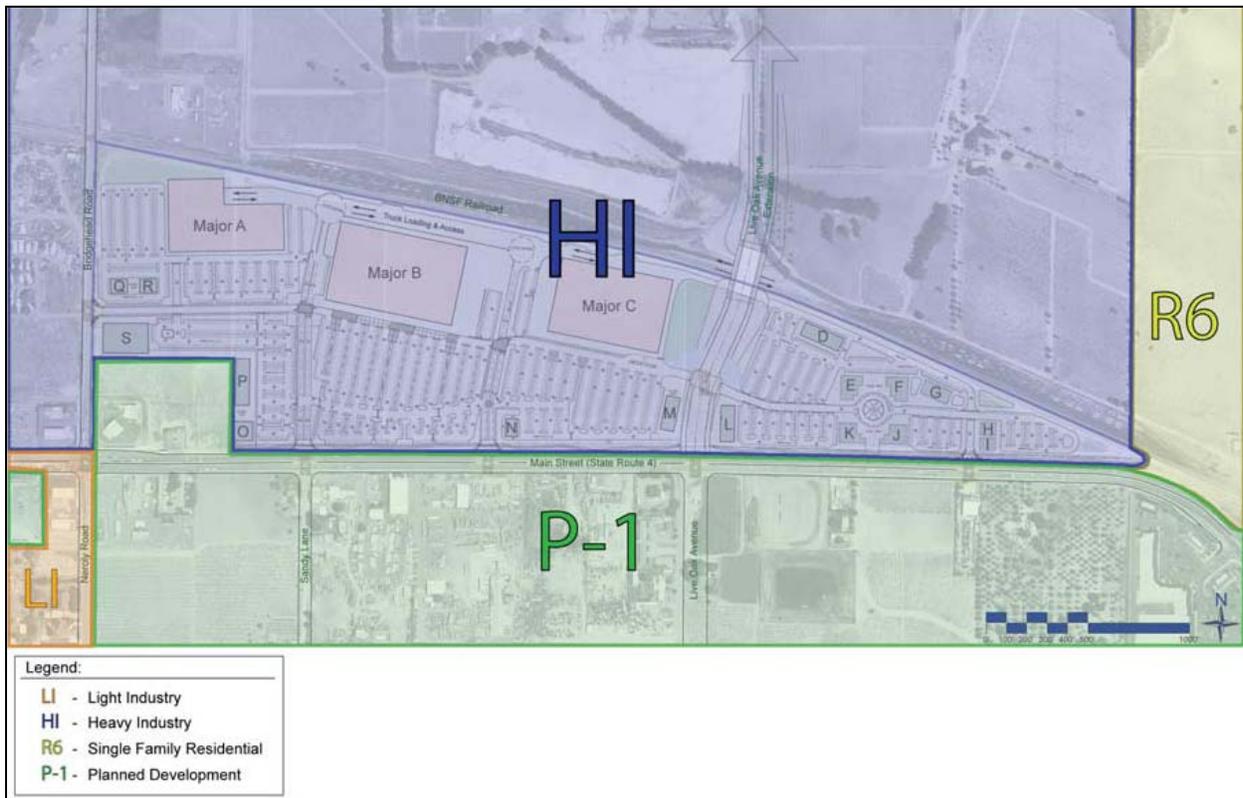
As discussed in the Project Draft EIR, the adopted amendment included direction for preservation of 15 acres of vineyard northeast of the Specific Plan boundary, on a portion of the Cline Vineyards.

Policies adopted for implementation of the Redevelopment Plan call for development of the 76.4-acre Specific Plan site with commercial uses. These policies are reflected in the current Oakley 2020 General Plan land use classification for the Specific Plan site.



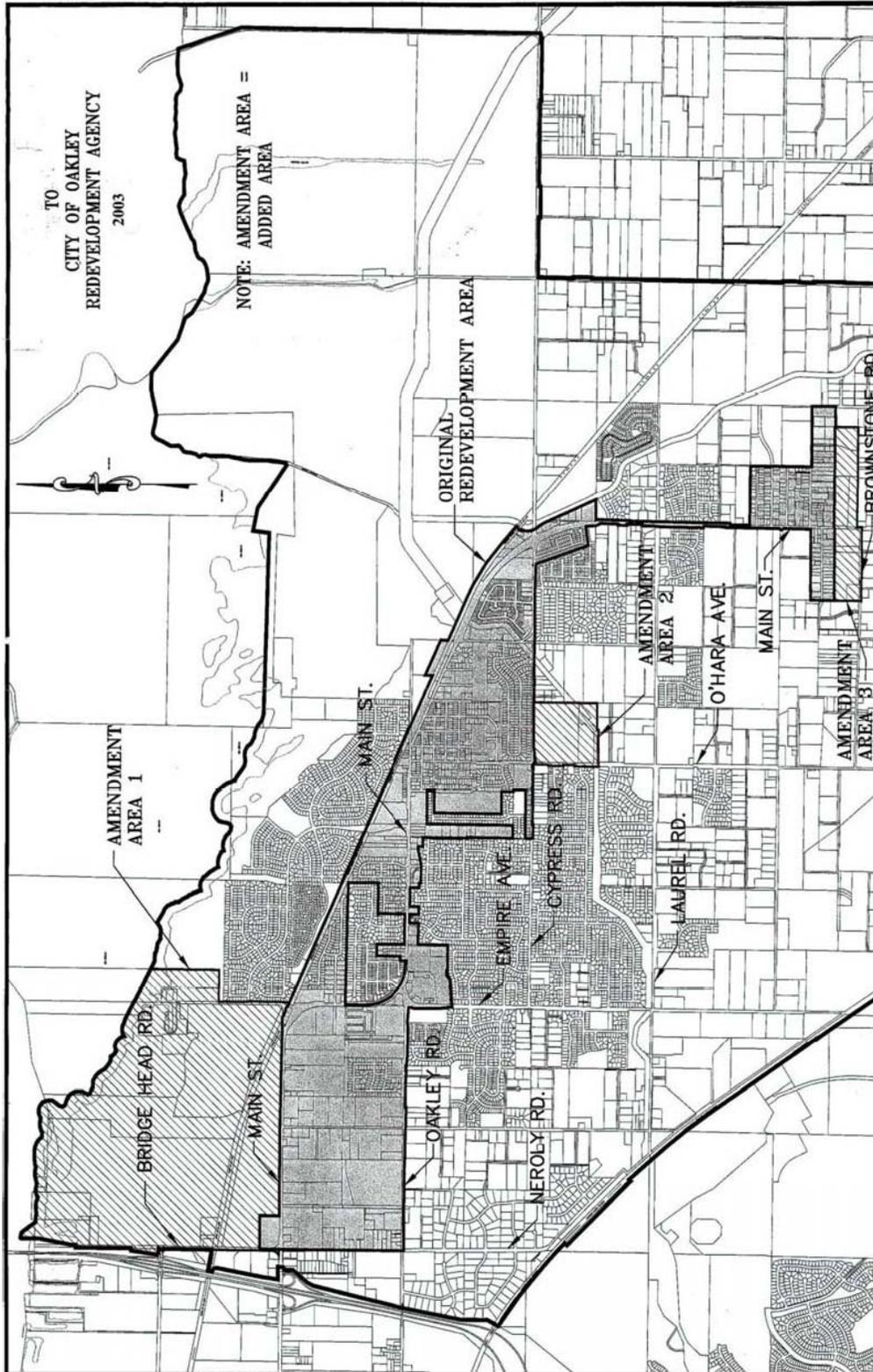
Source: City of Oakley 2020 General Plan.

FIGURE SP 2-3: Oakley 2020 General Plan Land Use Designations



Sources: Oakley Community Development Department; Richard T. Loewke, AICP.

FIGURE SP 2-4: Oakley Zoning District Boundaries



Source: Oakley Community Development Department

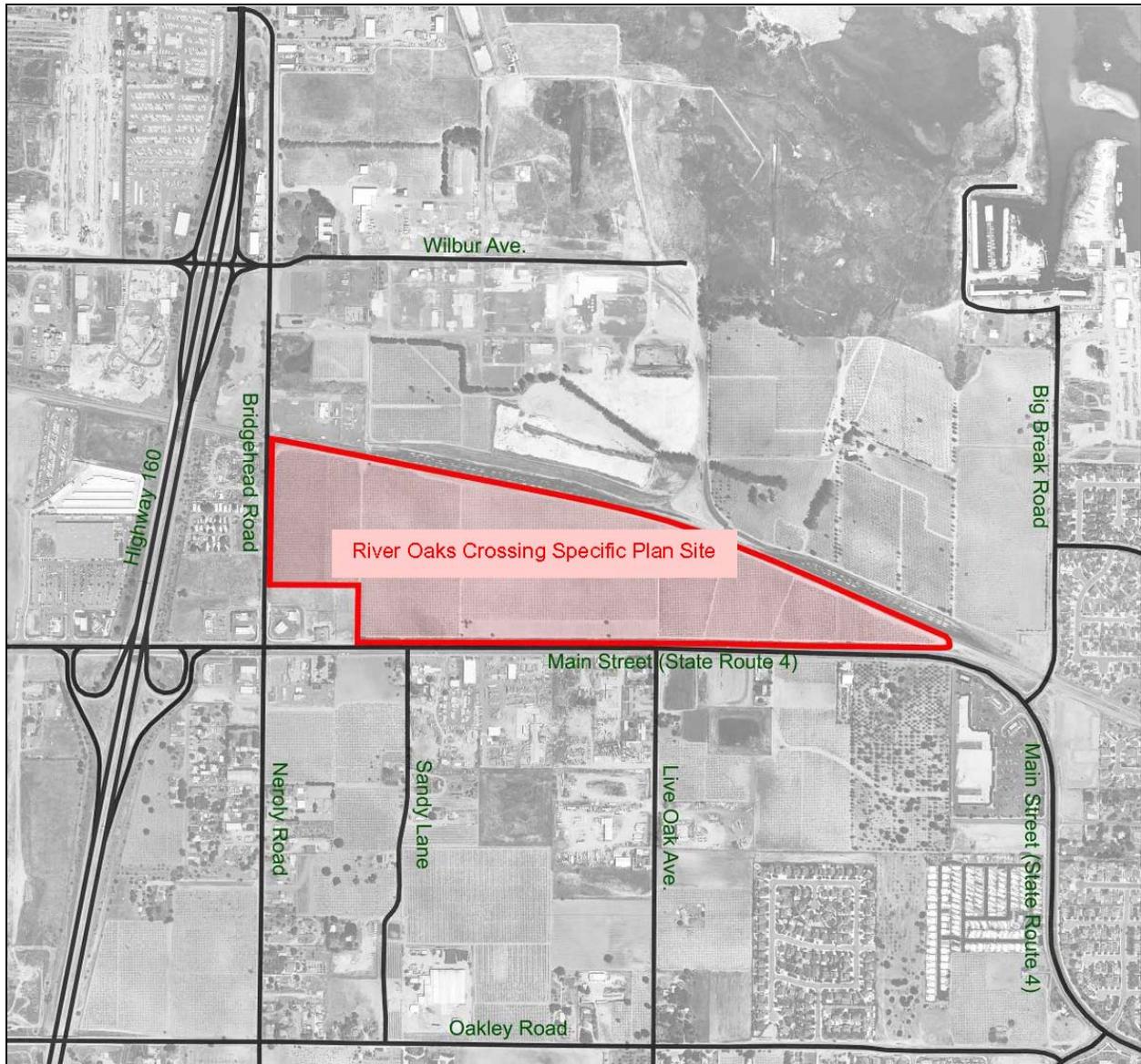
FIGURE SP 2-5: Oakley Redevelopment Plan Area Amendment No. 1

SECTION 3. LAND USE

3.1 Existing Conditions

The 76.4-acre Specific Plan project site is situated on the north side of Main Street (current State Route 4) between Bridgehead Road and Big Break Road, immediately east of State Route 160 (as shown in Figure SP 2-1 above). The relatively flat site has continuous access to Main Street, a four-lane arterial roadway that currently extends through central Oakley to eastern Contra Costa County. Main Street functions as the principal east-west arterial from Highway 160 east through the City of Oakley. The General Plan calls for Main Street to be widened to six (6) lanes through the study area. Improvements to Main Street are proceeding independently of the Specific Plan process. As of June 2007, the improvement program for Main Street is proceeding through final engineering design. The City of Oakley is currently installing a traffic light at the intersection of Main Street and Live Oak Avenue, adjoining the project site.

The Specific Plan project site also has frontage on the east side of Bridgehead Road, a two-lane local collector street. As shown in aerial view Figure SP 3-1, Bridgehead Road is currently signalized at Main Street, southwest of the site. The Cline Property extends east from Bridgehead Road nearly one mile, and is bordered along its entire northerly property line by the Burlington Northern & Santa Fe (BNSF) Railroad line. The site has a triangular shape, with increasing site depth moving from east (Big Break Road end) to west (Bridgehead Road end). No buildings exist on the site, which is cultivated with grapes.



Source: Richard T. Loewke, AICP.

FIGURE SP 3-1: Aerial View of Project Area

The narrow easterly end of the Specific Plan site is visible looking west along Main Street near the intersection of Big Break Road, as shown in Figure SP 3-2 below. Prominently visible along the public perspective from Main Street are a row of power poles and overhead utility lines along the frontage of the project site. These facilities will be placed underground as Main Street is widened and the project site develops.



Source: Richard T. Loewke, AICP.

FIGURE SP 3-2: Site View Looking West along Main Street at Big Break Road

Future development within the Specific Plan project site will be prominently visible to drivers traveling westbound along Main Street. A water feature will be incorporated into the landscape design at the easterly corner of the site visible in the photograph above.

Figure SP 3-2 also exposes the grade difference between Main Street and the easterly end of the project site. As outlined in Section 5, on-site drainage facilities will need to pick up surface flows, and carry them north along Big Break Road.

Access to the project site will be provided through a series of signalized intersections along Main Street, Bridgehead Road and an extension of Live Oak Avenue. The easterly-most access point is situated roughly halfway between Big Break Road and Live Oak Avenue.



Source: Richard T. Loewke, AICP.

FIGURE SP 3-3: Site View Looking Northwest from East End of Specific Plan Area

The BNSF Railroad line forms a substantial physical barrier along the entire north boundary of the project site as shown in Figure SP 3-3. The railroad tracks represent a constraint to access north into the DuPont property; however, as shown in Figure SP 1-1 and discussed in Section 4 below, Live Oak Avenue is identified in the Circulation Element of the General Plan to be extended north through the project site and over the tracks. Development Plan A incorporates this northerly circulation connection, with a ramp and landing to accommodate a future bridge structure. Alternative Development Plan B examines the site planning opportunities available on the project site, in the event that an elevated crossing of the railroad tracks is subsequently found unnecessary.

Existing railroad noise and aesthetic impacts also represent a substantial challenge to the design of a shopping environment conducive to pedestrian orientation and use of public space. The Development Plan provides truck access and loading along the northerly end of the site adjoining the railroad right-of-way. Public plazas and building entrances will be separated from the tracks, and oriented toward Main Street.



Source: Richard T. Loewke, AICP.

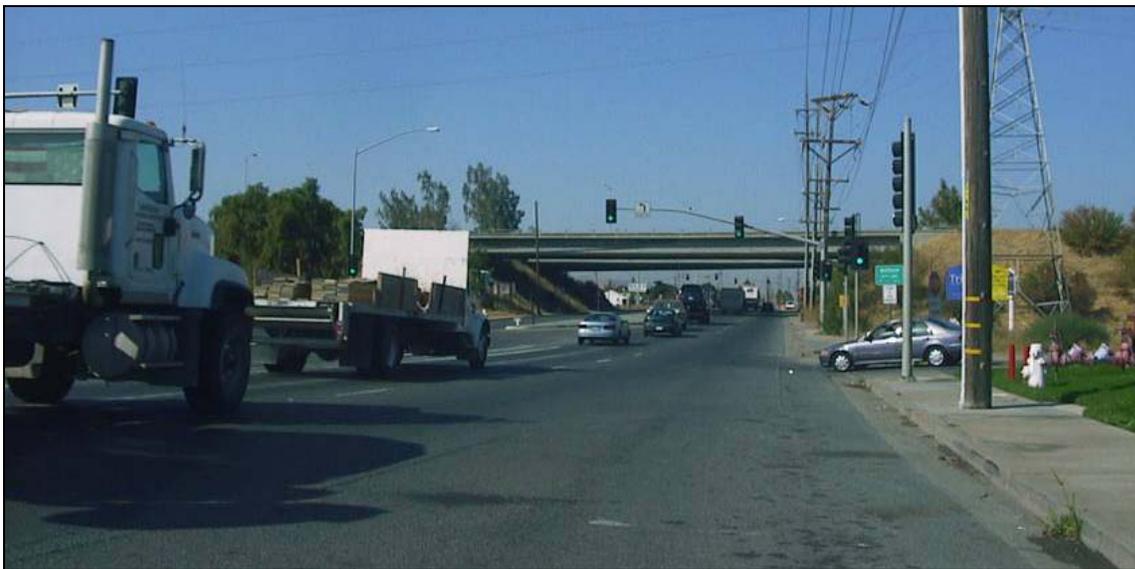
FIGURE SP 3-4: Site View Looking East from Bridgehead Road

As viewed from Bridgehead Road, the Specific Plan project site provides almost one-quarter mile of street frontage, from the railroad tracks on the north to the adjoining commercial uses at the intersection of Main Street. Circulation to Bridgehead Road is an important consideration in maximizing commercial accessibility and distributing vehicle trips. However, future access is planned to occur near the southerly end of the site, at a point well removed from the existing Bridgehead Road under-crossing.

As shown in Development Plan A (Figure SP 1-1), the only access point to Bridgehead Road will be at a new signalized intersection, located approximately 750 feet north of Main Street. As discussed in Section 4, principal truck access will be routed in and out of the site through signalized intersections along Main Street.

3.2 Surrounding Land Uses

The project site has frontage on Main Street and Bridgehead Road. A wide range of established and developing commercial uses occur along these streets. Visible in Figure SP 3-5 is the section of Main Street west of Bridgehead Road and the project site. Fast food and hotel uses exist on the north side of the street, with additional fast food and gas station uses to the south. As discussed in Section 4, this segment of roadway currently carries a substantial volume of peak-hour traffic, in relation to its design capacity. Additional travel lanes and enhanced intersection turning capacity are incorporated into the City's current Main Street widening project. The intersection of Main Street and Bridgehead Road / Neroly Road will be further upgraded over time, in order to maintain acceptable levels of service as discussed in the Draft project EIR.



Source: Richard T. Loewke, AICP.

FIGURE SP 3-5: View West Along Main Street to Hwy 1



Source: Richard T. Loewke, AICP.

FIGURE SP 3-6: View South Across Main Street

Opposite the Specific Plan project site along Main Street are a mix of existing commercial land uses that pre-date City incorporation and current zoning standards. Visible in Figure SP 3-6 is an existing salvage yard that extends east from Neroly Road on the south side of Main Street opposite the project site. These commercial uses include outdoor storage and business operations, with little frontage landscaping visible from Main Street.



Source: Richard T. Loewke, AICP.

FIGURE SP 3-7: View South Across Main Street

Further to the east along the south frontage of Main Street are additional vacant and under-developed properties planned for commercial use under the Oakley 2020 General Plan. Visible in Figure SP 3-7 above is one such property situated opposite the middle of the planned easterly entrance to the Specific Plan project site. It is at this general location that a future signalized intersection is shown on the Development Plan. Right-of-way will be dedicated on the north side of the intersection, and 50% of the anticipated costs will be collected concurrently with Specific Plan development east of Live Oak Avenue. However, the signal light improvements will be deferred until the properties on the south side of Main Street (shown above) are developed.

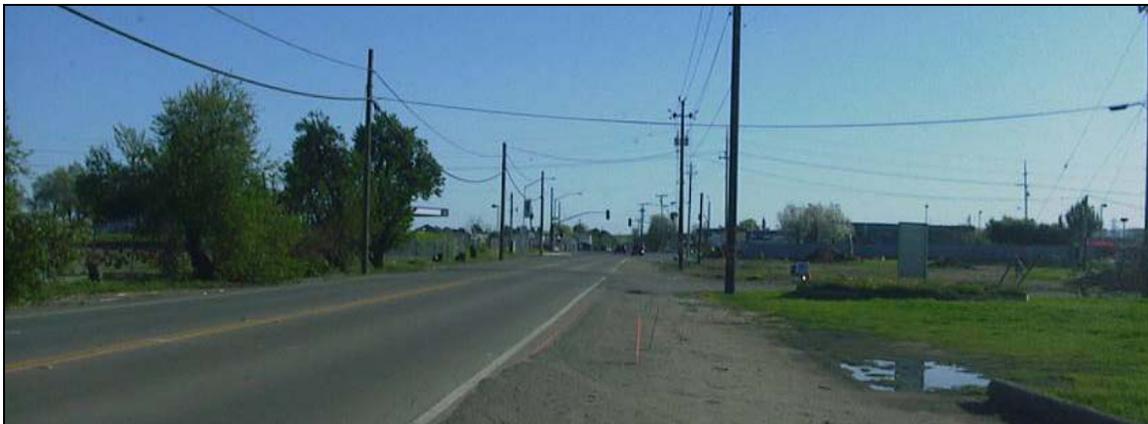
Figure SP 3-8 shows a recently completed commercial development on the south side of Main Street near the easterly end of the project site. This development includes a range of smaller retail and service uses, along with a commercial self-storage use (to the rear). Building forms include modulated store entrances within a linear structure punctuated by taller accent elements at both ends.



Source: Richard T. Loewke, AICP.

FIGURE SP 3-8: Retail Center South of Main Street Opposite Easterly End of Specific Plan Site

Looking south along Bridgehead Road along the frontage of the Specific Plan project site is the currently signalized intersection at Main Street. Existing land uses to the west of the Specific Plan site include a service station, fast-food restaurant, and hotel. A residential mobile home park and an open commercial storage use, both substantially pre-dating City incorporation, currently occupy the properties north of the railroad tracks, along the west side of Bridgehead Road.



Source: Richard T. Loewke, AICP.

FIGURE SP 3-9: View South Along Bridgehead Road Toward Main Street

A gasoline station and convenience food store occupy the northeast corner of Bridgehead Road and Main Street. Situated between this corner and the project site is a 4.46-acre site for which the City has approved a minor subdivision application (MS 98-0016) to create four separate commercial parcels. Uses within the currently developing commercial subdivision include an approved restaurant, car wash, and motel. A drive-through coffee bar exists along the Main Street frontage of this adjoining site. Recent entitlements for this site provided for relocation of the coffee bar use to its current

location, in order to accommodate the restaurant pad. These adjoining properties are also classified for Commercial use under the General Plan, as shown in Figure SP 2-3.

Additional vacant and under-developed properties extend along the west side of Bridgehead Road, north of the BNSF railroad line. This section of land between the railroad tracks and Wilbur Avenue (situated between Bridgehead Road and Highway 160) is situated within the City of Antioch and is planned for Light Industrial uses. The Delta Diablo Sanitary District currently operates a pumping station within a portion of this property in the City of Antioch.



Source: Richard T. Loewke, AICP.

FIGURE SP 3-10: View South Along Bridgehead Road at Railroad Crossing

Visible in Figure SP 3-11 is the existing intersection of Bridgehead Road and Wilbur Avenue, along with the ramps to and from northbound Highway 160. This intersection and the adjoining Highway 160 ramps currently carry a limited amount of traffic from existing Oakley and adjoining Antioch industrial areas. However, future industrial growth contemplated in the Antioch and Oakley General Plans, will add considerably to peak-hour traffic demands at this location. Additional improvements to the capacity of the Wilbur / Bridgehead Road intersection and the adjoining Highway 160 ramps will be needed as the area develops (See Section 4 for further discussion of cumulative growth potentials).



Source: Richard T. Loewke, AICP.

FIGURE SP 3-11: Wilbur Avenue / Bridgehead Road Intersection and Hwy 160 Ramps

North of the railroad line are vacant properties owned by the DuPont Chemical Company, along with other privately owned properties cultivated with grapes (as shown in Figure SP 3-12). These adjacent properties extend from Bridgehead Road on the west to Big Break Road on the east. As shown in Figure SP 2-3 above, these properties are classified in the General Plan for Industrial, Business Park and Utility / Energy uses. It is anticipated that DuPont Chemical will prepare and submit plans for development of these properties in the near future.



Source: Richard T. Loewke, AICP.

FIGURE SP 3-12: View North from Project Site

3.3 Development Plan

Existing environmental factors, design constraints, and market opportunities have been taken into consideration in preparing the Development Plan. The purpose of the Development Plan is to identify the physical relationships between approved Primary and Secondary Retail buildings, circulation and parking improvements, landscape areas and surrounding properties. Whereas the specific building footprints and sizes may vary as provided for in the Specific Plan, the Development Plan provides an overall framework for relationships of buildings and required site improvements. The approved Development Plan responds to the project objectives outlined in Section I, and incorporates the following design features, opportunities and constraints:

- ***Drainage and Hydrology:*** The site is very flat, with slopes averaging between one-half of one percent to almost 4 percent. The easterly half of the site drains to the northeast, while the westerly half drains to the southwest. The nearest 100-year flood plain extends across the northerly end of the adjoining DuPont site, but does not affect the Specific Plan project area.
- ***Live Oak Avenue Alignment:*** The City contracted with Mark Thomas and Company (MTCO) to develop a range of possible options for crossing the BNSF Railroad corridor, north into the DuPont property. The MTCO over-crossing scenario swings Live Oak Avenue slightly to the east as it crosses the Specific Plan project site, offering the greatest flexibility in terms of local access, cost, and possible phasing. Development Plan A includes the option of an extended bridge structure which opens a portion of the project site for internal east-west truck and service vehicle circulation immediately south of the railroad right-of-way. The optional second bridge structure would add some cost to the project, but would also enhance internal access by allowing trucks to circulate between the east and west portions of the site, independently of customer traffic. Section 4 provides a further discussion of site circulation and the Live Oak Avenue alignment.
- ***Access to Main Street:*** Despite extensive opportunities for access to the one-mile long frontage on Main Street, future intersections and driveways must be limited, in an effort to preserve the capacity of the roadway to accommodate ultimate peak-hour volumes at full build-out of the City (year 2020 horizon as defined in the Oakley 2020 General Plan). In addition to the future signalized intersection at Live Oak Avenue, the Specific Plan project must incorporate a limited number of dispersed connections to maximize the opportunity for coordinated intersections with existing and/or planned streets to the south.
- ***Number and Size of “Major” and “Secondary” Tenants:*** Existing market data, as compiled through the City’s office of Economic Development and further analyzed in the Retail Market Impact Analysis,² suggests very strong support for

² The Retail Market Impact Analysis is included as Appendix H to the Draft EIR.

3-4 large-scale (or Major) retail uses within the Specific Plan project site. Such uses can benefit from convenient access to both current and future residents and employees in Oakley, via access from Main Street and Highway 160 / SR 4. In addition, a range of moderately-sized (Secondary) retail uses can be accommodated on the site to complement and share shopping trips with the Major Retailers. These Secondary Retail uses can include restaurants, specialty retailers, service uses and potentially an additional hotel use. The Secondary Retail uses can enrich the shopping experience, add diversity to the mix of retail uses, and entice shoppers to the site as a retail destination, rather than for single-purpose shopping. Major Retailers are classified for purposes of this Specific Plan process as uses with a gross floor area in excess of 100,000 square feet.

Uses planned for this sub-regional and community-serving center are not expected to significantly overlap with the range of goods and services currently offered and planned for the Oakley Downtown Area (to the east along Main Street). The proposed Specific Plan uses are intended to draw from a larger customer base from the entire City, including anticipated growth areas identified in the Oakley 2020 General Plan.

As part of the Specific Plan environmental evaluation process, an economic impact analysis was prepared to determine what effect the Major and Secondary Retail land uses proposed for the Specific Plan area would have both individually and cumulatively on competing facilities within the marketplace. The analysis (included as Appendix H of the DEIR Technical Appendix) assess the economic effects of development proposed under the Specific Plan, including the impacts of supercenter type uses, and identifies where these effects might result closure of existing businesses and sustained vacancy of existing buildings. Chapter 3.16 of the Draft EIR further evaluates these effects in relation to existing programs proposed mitigation measures, in order to determine whether a negative impact on the physical environment of Oakley and the surrounding communities could result, as potentially manifested by urban decay and physical deterioration of existing retail centers.

The economic analysis shows³ that Oakley's population experienced an average annual growth rate of 4.2% between 2000 and 2005. The City's rate of growth is expected to result in an average population increase of 2.9% over the ten-year period between 2005 and 2015, and 3.1% between 2010 and 2020, resulting in a population of approximately 48,836 in 2020. The analysis also shows that Oakley's estimated 2005 mean household income of \$83,000 will continue to grow at an estimated annual rate of 1.2% over the next 5 years. Employment growth within Oakley's business park areas north (DuPont) and south of the Specific Plan project area will contribute to further demand for goods and

³ Updated in May 2008 and presented to the City Council as part of Final EIR.

services. The analysis concludes that these changes will contribute to new demand.

- **Mitigation of Railroad Visual and Noise Constraints:** The railroad line is a visually prominent feature of the site. As analyzed in the Draft EIR and in Technical Appendix D, rail traffic contributes significantly to the local ambient noise environment. To shield the public and shoppers from the negative influence of rail traffic, the Specific Plan provides for visual landscape screening, and earth mounding and building mass to block noise penetration. The challenge is to achieve these goals without creating a linear or “strip” appearance to building designs and placement. The Development Plan shows a preferred approach to accomplishing this objective.
- **Options for Avoidance of “Strip” Appearance:** Alignment of the buildings in an orderly row, backing up to the railroad right-of-way is efficient, and serves to screen the tracks from public view. However, this option diminishes the attractiveness of the site, and can lead to creation of a “sea” of parking viewed from Main Street. A significant challenge in the Specific Plan process is the creative placement of building envelopes and employment of design standards, to create more manageable spaces without blocking the visibility of Major Retailers from Main Street.

Development Plan A, shown in Figure SP 1-1, responds to the challenges by balancing accommodation of Major and Secondary Retail site opportunities. The building envelopes shown in Figure SP 1-1 reflect a distribution within the site, and an internal relationship that provides for balance in circulation, parking availability, site visibility and screening of the railroad right-of-way.

Three building envelopes have been reserved for Major Retailers west of the Live Oak Avenue extension. As reflected in Table SP 1-1, the building envelopes range in potential GFA from 100,000 to 230,000 square feet, with an aggregate maximum of 520,000 square feet. Major Retail Envelope A is located at the westerly end of the site, partially screened by existing and recently approved retail uses on the adjoining properties at the northeast corner of Bridgehead Road and Main Street. Building Envelope A is best suited to a use requiring convenient access and proximity to parking, but one that is less dependent on building identification from the arterial roadway. Major Retail Envelopes B and C offer greater building visibility, together with convenient access and distributed parking. The building envelopes are arranged in a configuration that affords access for truck deliveries from the north, and provides for substantial screening of the railroad tracks. These envelopes are reserved for large-format warehouse, high-volume retail operations and supercenter type uses.

Approximately sixteen Secondary Retail envelopes are to be distributed across the site to provide opportunities for complementary restaurants, specialty retailing and commercial services. As reflected in the Development Plan, and detailed in Table SP 1-1, these smaller retail buildings are intended to accommodate specialty retailers,

restaurants, and a hotel use with up to 100 rooms (a range of from 75 to 100 rooms are provided for in Table SP 1-1). As addressed in the accompanying EIR, a hotel use within the Specific Plan project site is considered sensitive to both existing and future noise sources. The principal sources of noise impact are the existing railroad and planned truck deliveries on site.

As further discussed in Section 6, the Secondary Retail envelopes will be aggregated around the principal project entry points, in order to frame portions of the site into smaller spaces, with more individual character. As arranged in the Development Plan, these Secondary Retail envelopes have been extended to the perimeter of the site, in an effort to improve the visual exposure of individual uses, while preserving adequate “windows” of visibility to the Major Retail envelopes at the north end of the site. Although subject to further refinement at the time of building permit application and through the Architectural Review process described in Section 7.1, phased development of the site is expected to adhere to the design guidelines outlined in Section 6 and the relationships described in this section.

The individual building envelopes and overall aggregate retail floor space shown in the Development Plan are supported by a balanced circulation system and parking supply. As further described in Section 4, the on-site circulation system affords access to various uses on the site via five intersections which will ultimately be signalized as development within and adjoining the Specific Plan area is completed, along with a sixth existing intersection (Live Oak at Main Street) where signal modifications will be performed concurrently with development.

The driveway opening furthest east is designated as a “future” signalized intersection. Specific Plan development will provide necessary right-of-way dedication and fund half of the signal light costs. This intersection will be completed, and the signal light will be installed, concurrently with development on the opposite side of Main Street.

These controlled access points are compatible with plans for widening of Main Street to 6 travel lanes, and for installation of a raised central median in the roadway. As detailed in Table SP 1-1, parking is specified (using ITE standards) on the basis of 4.5 spaces per 1,000 square feet of gross floor area and 1.25 spaces per hotel room, plus an overall minimum turnover factor of 5 percent. This is more conservative than the City’s Zoning Ordinance standard. A total of up to 10 percent of the aggregate GFA within the project is assumed for occupancy by restaurant uses. Building envelopes and parking facilities shown in the Development Plan also reflect a distribution of facilities with balanced parking demand between Major and Secondary Retail uses, both east and west of Live Oak Avenue.

3.4 Approved Land Uses and Building Areas

The range of retail uses approved for all Major and Secondary Retail building envelopes identified in the Development Plan include all uses listed as Permitted Uses under Section 9.1.504(b) of the Oakley Municipal Code (Retail Business), together with the

following additional uses, all subject to the standards and square footage limitations provided for herein:

- (a) A hotel use of up to 100 rooms
- (b) Major appliance sales
- (c) Department store uses
- (d) Building materials and hardware sales
- (e) Tire sales and service uses
- (f) Additional non-residential uses of a compatible nature which may be approved by conditional use permit as provided for in Section 9.1.504(c).

The following uses are not permitted within the SP-2 District and are prohibited from occupying any portion of the Specific Plan area:

- (g) Supercenter uses (i.e., A store that exceeds 100,000 square feet in size with at least 10% of the sales area dedicated to non-taxable merchandise. Membership and club stores are excluded from this definition.

Hours of operation may be no later than midnight, except for the following uses, or with the review and approval of the City Council: Hotel, drug stores with 24-hour pharmacy, self-service gasoline pumps (market restricted to midnight), full-service grocery stores, and sit-down and self-service restaurants. All other uses are restricted to operating no later than midnight. At any time the City Council may review the hours of operation for any use and either restrict or allow different hours of operation, as deemed appropriate by the City Council.

Hours of operation will also be reviewed by the City Council with each Architectural Review application, at which time the City Council may further restrict or allow different hours of operation.

Table SP 1-1 includes the identification of land use categories including Major and Secondary Retailers, including a possible hotel use. This table identifies two variations on land use intensity, identified as Alternatives 1 and 2, representing the approved range of buildout square footage within the Specific Plan project area. These alternative floor space allocations are included in the environmental analysis as increased and reduced intensity options. All the environmental impact analyses (including traffic, noise, air quality, retail impact, water supply assessment and related studies) are based on the aggregate maximum of 770,000 square feet. Alternative 1 retains the three Major Retailers shown in the Development Plan, but assumes slightly smaller footprints, and incorporates a 24% smaller aggregate amount of Secondary Retailers (130,000 compared to 170,000 square feet), for a reduced aggregate floor area of 630,000 square feet. This Lower Intensity Alternative equates to an overall FAR of 0.19, and is considered a minimum development threshold for land use intensity on the Specific Plan project site that still meets the project's objectives.

The Development Plan represents a desirable design and land use mix concept that maximizes retail sales and employment opportunities with minimized environmental effects. It is the intent of this Specific Plan to retain sufficient flexibility to accommodate a variety of retail uses, subject to Architectural Review and verification of consistency with the approved range of Development Plan uses and building sizes.

3.5 Development Potential and Job Production

The 76.4-acre Specific Plan project site is geographically centered within a largely underdeveloped portion of the community, which is currently planned for intensive commercial and industrial development. Estimates of total floor area and employment were developed based on build-out conditions for each of the following future sites within the larger study area surrounding the Specific Plan project boundary. These estimates are consistent with the General Plan 2020 projections.

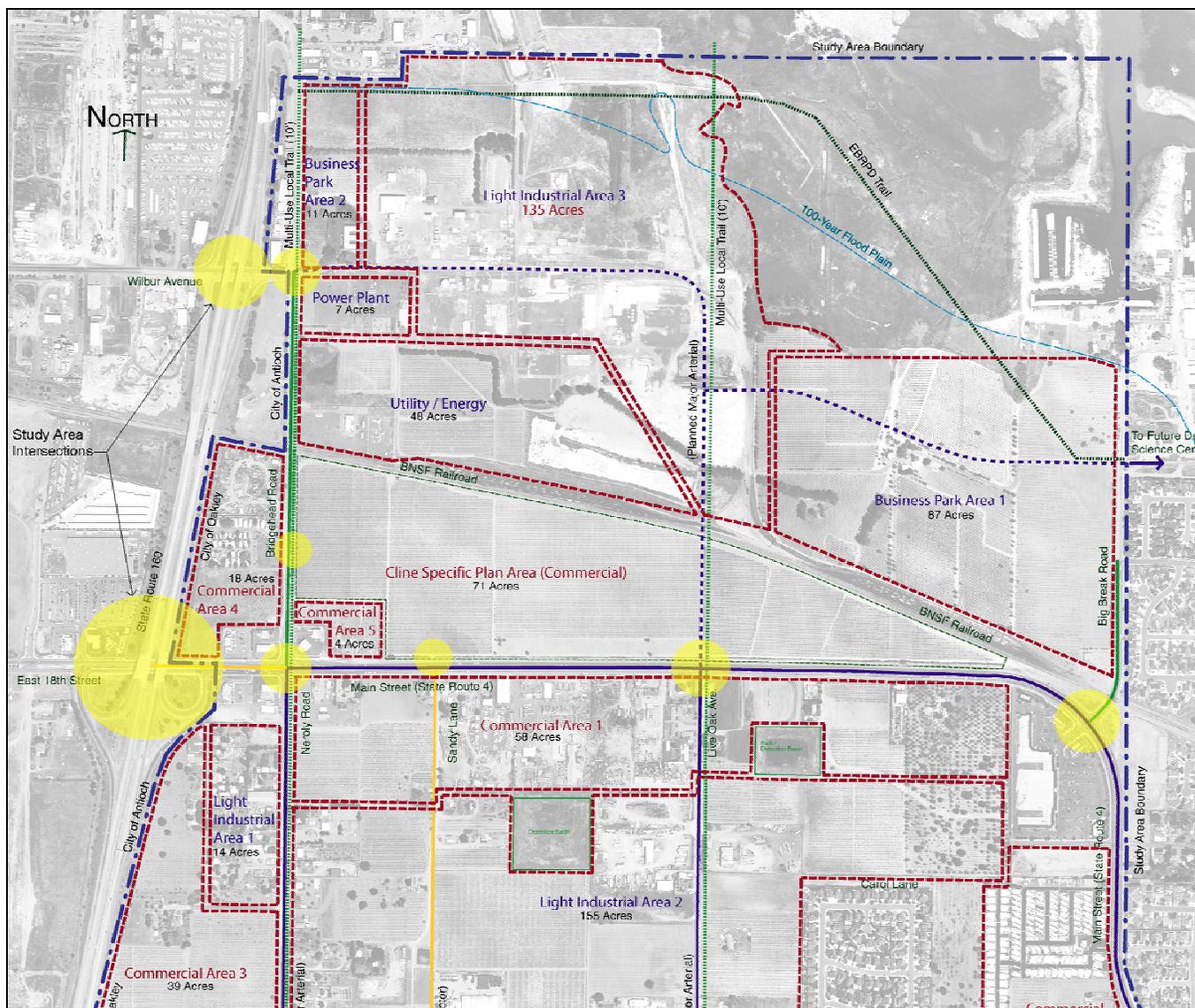
- (a) DuPont Business Park Area 1
- (b) DuPont Business Park Area 2
- (c) DuPont Light Industrial Area 3
- (d) Power Plant Site
- (e) DuPont Utility / Energy Site
- (f) Commercial Area 1
- (g) Commercial Area 2
- (h) Commercial Area 3
- (i) Commercial Area 4
- (j) Light Industrial Area 1
- (k) Light Industrial Area 2

Figure SP 3-13 provides an overall view of the northwest portion of Oakley which includes the Specific Plan project area, showing acreages for all of the land uses identified in the Oakley 2020 General Plan. This map includes a table which summarizes, by land use category, the ultimate buildout FAR, total Building Area, and total Employment within this larger study area. These assumptions were tested for conformity with General Plan build-out conditions, and were also built into the accompanying DEIR analysis.

The assumptions used in developing the buildout projections for the study area are as follows:

1. *Land Use Classifications:* All land use classifications were taken directly from the current General Plan Land Use Element, with the following exceptions: (a) the 7-acre “Power Plant” area shown on the DuPont property was identified in a more recent master development concept plan provided by DuPont; (b) the remaining 48-acre area shown at the southwest corner of the DuPont property was classified in the General Plan as “Utility / Energy”, but has been treated in the table as a potential Business Park site for purposes of assessing future development potential. The Cline Property is classified in

2. *Development Areas on DuPont Property:* Total development area accounted for within the DuPont property reflects an estimated gross land area, excluding assumed potentially protected wetlands. The wetlands boundary line conforms to the General Plan Land Use Element, and does not reflect analysis of field conditions on the DuPont property. The northerly light industrial area includes substantial acreage located within the 100-year floodplain.
3. *Property West of DuPont:* North of the BNSF rail line and west of DuPont are two small sites for which no additional development is anticipated. The property between Wilbur Avenue and the rail line is within the City of Antioch and currently accommodates the Bridgehead Sanitary Sewer Pump Station and force main. The 2004 Antioch General Plan classifies this area as “Business Park”; it is anticipated that the majority of this small site will remain in public ownership and utility use. North of Wilbur Avenue is a developed narrow strip of land classified under the Oakley 2020 General Plan as “Light Industrial”. No significant additional development is anticipated on this occupied property.
4. *Other Excluded Areas:* The dotted red sub-area boundary lines shown in Figure SP 3-13 correspond to areas of anticipated future commercial and industrial development, and exclude the existing and approved developments within these areas for purposes of projecting net increases in employment and development.



Source: Richard T. Loewke, AICP.

FIGURE SP 3-13: Anticipated Study Area Land Use and Development

General Plan Land Use	Area (Acres)	FAR (SF/Acre)	Building Area (SF)	Building Area/Job	Total Jobs
Light Industrial	301	.25	3,278,000	750	4,370
Business Park	98	.30	1,281,000	350	3,660
Power Plant	7	.25	76,000	1,000	80
Commercial	206	.25	2,243,000	500	4,490
Utility/Energy (Bus. Park)	48	.25	523,000	350	1,490
Total	660		6,991,000		14,090

Note: All land use classifications are taken from 2020 Oakley General Plan. Land area calculations are based on gross site area, less pre-existing uses assumed to remain. Floor Area Ratios (FARs) and Building Area per Job ratios are based on comparable developed projects in Eastern Contra Costa County. These ratios are less than the maximum ratios as prescribed in the General Plan and Zoning Ordinance.

5. *Developable Acreage Calculations:* All calculations are rounded off to the nearest whole acre, and are further adjusted in the following instances to exclude a portion of the acreage for retained existing land uses: (a) the 14 net acres shown in Light Industrial Area #1 were adjusted to reflect retention of 20% of the existing residential and/or industrial uses; (b) the 58 net acres shown in Commercial Area #1 were adjusted to reflect retention of 20% of existing commercial and public uses; (c) the 18 net acres shown in Commercial Area #4 were adjusted to reflect retention of the recently completed hotel use; and (d) Business Park Area 1 is expected to include a reservation of 15 acres for preservation of producing grape vines, as called for in Redevelopment Plan Amendment Area 1 (see DEIR Technical Appendix L). The calculated 155 developable acres within Light Industrial Area #2 include a 7-acre area reserved on the General Plan for “Public and Semi-Public” uses and an existing Antioch School located at the intersection of Live Oak Avenue and Oakley Road. Acreage figures shown in the Land Use and Development Table in Figure SP 3-13 reflect the sum of all sub-area calculations. The 76-acre Cline Property was included under the Commercial classification in the Land Use and Development Table.

6. *Floor Area Ratios:* The Land Use and Development Table includes a set of specific floor area ratios used to calculate the total gross floor area of future development within each land use category. These FARs were developed as averages, based on a review of representative development projects approved within Eastern Contra Costa County over the past 5 years, and adjusted slightly upward to take into consideration policies contained in the City’s General Plan that encourage a higher intensity of development.

7. *Projected Employment:* The calculation of future buildout employment levels is based on the land use category acreage multiplied by the average building area per job. These employment density factors were developed based on a combination of the Antioch-Oakley Jobs Housing Opportunity Zone Program Proposal and the Oakley 2020 General Plan EIR analysis. These estimates reflect the future employment levels in planned commercial, light industrial and business park projects within the study area. As noted in the table, the FAR and employment density factors used in this study were designed to accurately reflect anticipated future development, and are slightly lower than the maximum policy entitlements in the General Plan.

8. *Use of Data:* Figure SP 3-13 includes a forecast of net added building and employment within the study area, compared to current conditions. Full development is assumed to occur by 2020. Building square footage and employment figures were used in conjunction with standardized ITE trip generation rates to determine future traffic levels generated within the study area. This information is helpful in determining peak traffic volumes for certain local intersections (marked by yellow circles on the map) within the study area and identified in the EIR.
9. *Approved But Not Yet Completed Development:* Planning efforts for the Cline Property took into consideration the following development projects that have been entitled, but not yet constructed within the study area boundaries:
 - a. *Lodge Property:* Located at the northeast corner of Main Street and Bridgehead Road, the Lodge Property is situated behind the existing gas station. This project involves a subdivision of 4.6 acres into 4 parcels to accommodate two convenience restaurants, an existing drive-through coffee bar, a motel and a carwash.
 - b. *Delta Science Center:* In October of 2001 the EBRPD approved the Big Break Regional Shoreline Land Use Plan, for a 1,648-acre section of shoreline located immediately northeast of the study area, with access from Big Break Road. This project involves four phases of development. Phase 3 includes creation of the Delta Science Center, a joint venture of EBRPD, Contra Costa County, the Contra Costa County Community College District, Cal State University Hayward and the Ironhouse Sanitary District. Located immediately north of the driveway access from Big Break Road, the Center portion of the project would include provision of 250 parking spaces, the trips from which would contribute to peak hour traffic at study area intersections (according to the MND prepared for this project, which included a traffic analysis by Fehr & Peers).
 - c. *Industrial Buildings on Live Oak Avenue, North of Oakley Road:* City entitlements were recently issued for construction of two speculation light industrial buildings of 8,000 square feet each on the west side of Live Oak Avenue, approximately ¼ mile north of Oakley Road.
 - d. *Single-Family Homes:* 48 additional homes are nearing completion on the north side of Oakley Road between Live Oak Avenue and Empire Avenue east of the existing residential neighborhood.
 - e. *Tire Sales Facility:* A 20,000 square foot commercial facility has been recently completed on the property immediately south of Carol Lane and the shopping center, west of Main Street (Les Schwab project). This business involves the sales and installation of tires.

In addition to the foregoing identified local projects, the traffic, noise, air quality, fiscal and other impact analyses have taken into consideration all additional approved but not

yet completed development projects within the City and adjoining communities, as well as those future projects anticipated as a result of local General Plan buildout.

SECTION 4. CIRCULATION

4.1 Circulation System

Development on the Cline Property is planned to consist of between 630,000 and 770,000 square feet of commercial uses spread over the 76.4-acre Specific Plan project area. As shown in the Development Plan and discussed in the preceding section, these uses will be accessed from Bridgehead Road, Main Street and the future extension of Live Oak Avenue.

The 660-acre study area shown in Figure SP 3-13 above, extends roughly from the City limit to the north, SR4 and SR160 to the west, Oakley Road to the south, and Big Break Road/Main Street/Empire Avenue to the east. Major roadways in the study area include SR 160, Main Street, Neroly Road/Bridgehead Road, Oakley Road, and Empire Avenue. Other minor roadways include Wilbur Avenue, Sandy Lane, Live Oak Avenue, and Big Break Road. The following is a summary of these roadways and their relationship to the Specific Plan project area:

- SR 160 is a north-south highway that extends through the study area, west of the project site. This roadway serves as a major route connecting Oakley to the Antioch Bridge and Sacramento County to the north, and to the SR 4 freeway to the west. SR 160 typically has two lanes in each direction, narrowing to one lane per direction north of the Antioch Bridge toll plaza.
- Main Street (SR 4) is an east-west arterial extending from an interchange with SR 160 on the west to Brentwood and Stockton on the southeast. In the study area, Main Street typically provides two lanes in each direction with a two-way center left-turn lane. Access to the project site would be provided on Main Street.
- Neroly Road/Bridgehead Road is a two-lane north-south roadway connecting Oakley to Brentwood and borders the west side of the project site. Site access would be provided from Bridgehead Road.
- Oakley Road is a two-lane east-west minor arterial that connects Oakley to Antioch. It extends from west of SR 160 in Antioch eastward to its terminus at Empire Avenue.
- Empire Avenue is a major north-south roadway in the study area, providing connections between Brentwood and Oakley. In the study area, shown in Figure SP 3-13, Empire Avenue contains up to 6 travel lanes with enhanced turning capacity at Main Street.
- Wilbur Avenue is a minor east-west roadway to the north of the project site, with an interchange on SR 160. Wilbur Avenue provides access to industrial and residential areas and extends westward into Antioch.

- Sandy Lane is a minor north-south roadway between Main Street and Oakley Road that serves primarily local residents. It is unpaved, except where it intersects Main Street. Access to the project site is proposed as the north leg of the Sandy Lane/Main Street intersection.
- Live Oak Avenue is a two-lane roadway extending from Main Street in the north to Neroly Road in the south. An extension of Live Oak Avenue as a major arterial is anticipated to be constructed through the project site and into industrial areas to the north with a grade-separated crossing at the railroad tracks. Project site access would be provided on the Live Oak Avenue extension.
- Big Break Road is a minor north-south roadway providing access to a large residential development and the Oakley Marina. There is an at-grade railroad crossing just north of Main Street at the BNSF tracks.

4.2 Transportation System Network

The traffic and circulation analysis prepared as part of the Oakley 2020 General Plan in 2002 identified a number of roadway improvements ultimately needed within the vicinity of the Specific Plan project area. These include: (a) Opening of SR 4 Bypass, with ramp connections between the Bypass and SR 4; (b) Extending Live Oak Avenue north through the project site; (c) Upgrading Sandy Lane to a paved roadway and signaling Main Street / Sandy Lane; and (d) Widening of Main Street to six lanes throughout the study area.

The additional traffic demand associated with the proposed Specific Plan project has been analyzed in the Draft EIR, in relationship to the following seven current and future condition scenarios:

- *Existing* – Existing conditions based on existing traffic volumes obtained from counts and existing roadway geometries.
- *Near Term No Project* – Near term (Year 2010) forecast conditions based on near term future approved developments and roadway improvements.
- *Near Term Plus Project* – Near Term conditions plus project-related traffic.
- *Cumulative No Project* – Future (Year 2030) forecast conditions based on the City of Oakley General Plan buildout and all planned roadway improvements.
- *Cumulative Plus Project* – Future (Year 2030) forecast conditions with General Plan Buildout plus project-related traffic.



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PROJECT LOCATION AND STUDY INTERSECTIONS

FIGURE SP 4-1: Study Area Intersections

Figure SP 4-1 identifies the 29 key intersections that have been evaluated under all of the current and future demand conditions. They include the following:

1. Wilbur Avenue/Minaker Drive
2. Wilbur Avenue/Viera Avenue
3. Wilbur Avenue/SR 160 Southbound Ramps
4. Wilbur Avenue/SR 160 Northbound Ramps
5. Wilbur Avenue/Bridgehead Road
6. East 18th Street/Hillcrest Avenue
7. East 18th Street/Viera Avenue
8. East 18th Avenue/Phillips Lane
9. Main Street/SR 160 Southbound Ramps
10. Main Street/SR 160 Northbound Ramps
11. Main Street/Bridgehead Road/Neroly Road
12. Main Street/Sandy Lane
13. Main Street/Live Oak Avenue
14. Main Street/Big Break Road
15. Oakley Road/Neroly Road
16. Oakley Road/Live Oak Avenue

17. Oakley Road/Empire Avenue
18. Main Street/Empire Avenue
19. Main Street/Vintage Parkway
20. Main Street/O'Hara Avenue
21. Cypress Road/Empire Avenue
22. Cypress Road/Main Street
23. Neroly Road/Live Oak Avenue (future)
24. Live Oak Avenue/Laurel Road (future)
25. Laurel Road/Empire Avenue
26. Bridgehead Road/Project Driveway (future)
27. Main St./Project Driveway (future)
28. Main St./Project Driveway East (future)
29. Project Internal Intersection Live Oak (future)

Following is an aerial and photographic (eye-level) view of the eleven existing intersections located in closest proximity to the project site. These figures reflect current conditions that form the basis for analysis of impacts and mitigation measures contained in Chapter 3.2 of the Draft EIR. The complete Transportation Impact Analysis is presented in Technical Appendix C.



Source: Richard T. Loewke, AICP.

FIGURE SP 4-2: Wilbur Avenue / SR 160 Southbound Ramps (Photo & Aerial View)



Source: Richard T. Loewke, AICP.

FIGURE SP 4-3: Wilbur Avenue / SR 160 Northbound Ramps Adjoining Bridgehead Road (Photo & Aerial View)



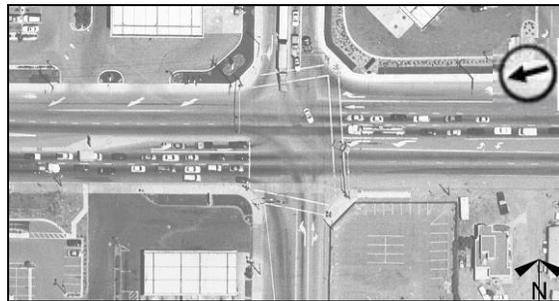
Source: Richard T. Loewke, AICP.

FIGURE SP 4-4: Main Street / SR 160 Southbound On-Ramp (Photo & Aerial View)



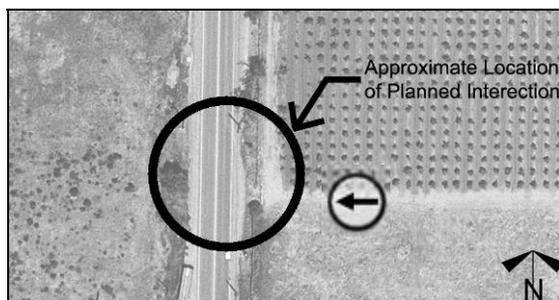
Source: Richard T. Loewke, AICP.

FIGURE SP 4-5: Main Street / SR 160 Northbound Off-Ramp (Photo & Aerial View)



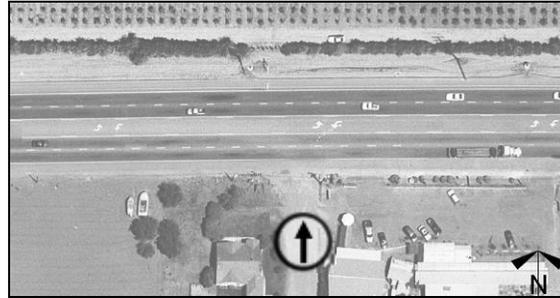
Source: Richard T. Loewke, AICP.

FIGURE SP 4-6: Main Street / Bridgehead Road / Neroly Road (Photo & Aerial View)



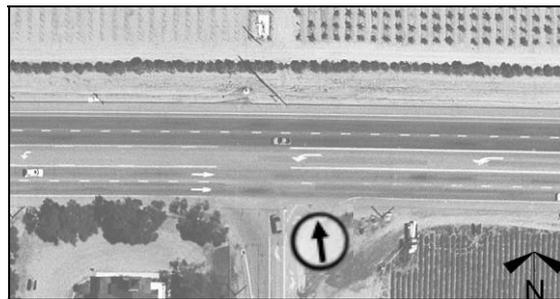
Source: Richard T. Loewke, AICP.

FIGURE SP 4-7: Bridgehead Road / Project Driveway (Photo & Aerial View)



Source: Richard T. Loewke, AICP.

FIGURE SP 4-8: Main Street / Sandy Lane (Photo & Aerial View)



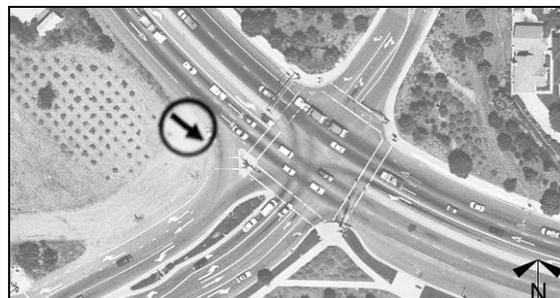
Source: Richard T. Loewke, AICP.

FIGURE SP 4-9: Main Street / Live Oak Avenue (Photo & Aerial View)



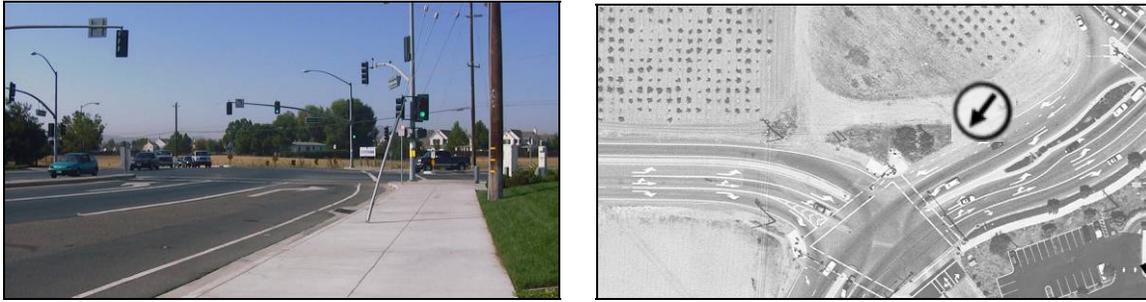
Source: Richard T. Loewke, AICP.

FIGURE SP 4-10: Main Street / Big Break Road (Photo & Aerial View)



Source: Richard T. Loewke, AICP.

FIGURE SP 4-11: Main Street / Empire Avenue (Photo & Aerial View)



Source: Richard T. Loewke, AICP.

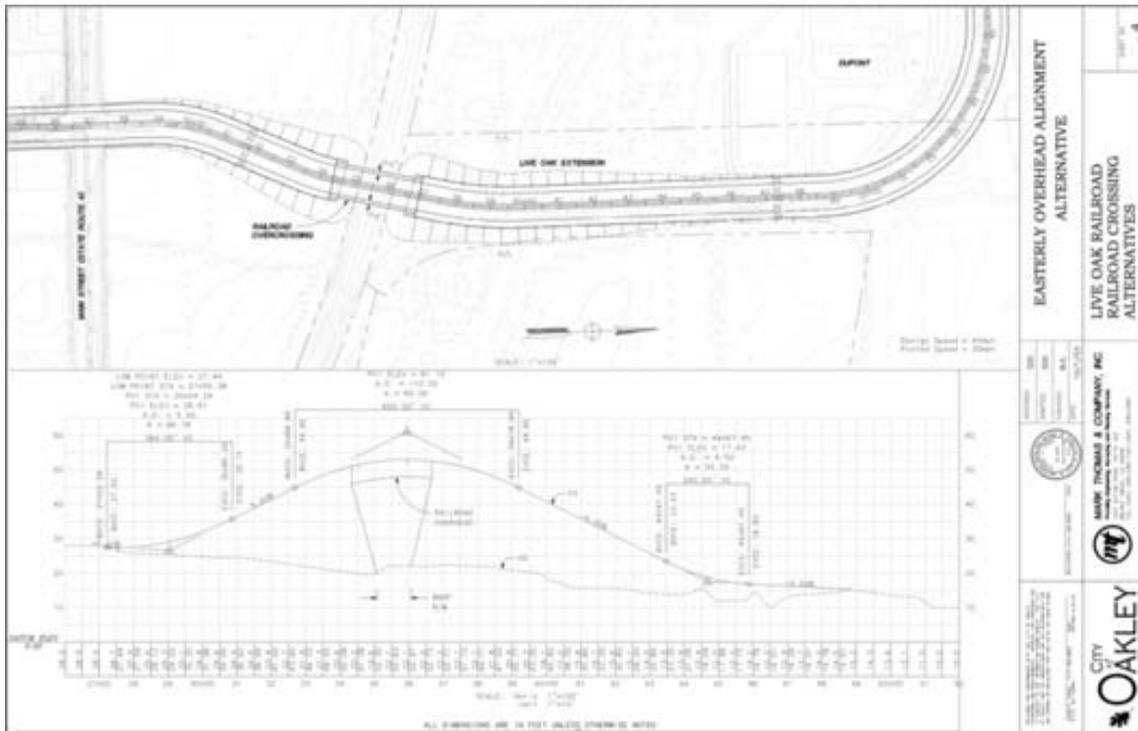
FIGURE SP 4-12: Oakley Road / Empire Avenue (Photo & Aerial View)

4.3 Live Oak Avenue Extension

The City is currently proceeding with improvement plans to widen and signalize Live Oak Avenue at Main Street. As part of this process, the City contracted with the engineering firm of Mark Thomas and Company to work with the Specific Plan team to design options for future extension of Live Oak Avenue north, through the Specific Plan area and into the DuPont property. As shown in the General Plan Circulation Element, Live Oak is eventually planned to loop through the DuPont Property and connect with Wilbur Avenue (see Figure SP 4-13).

The alignment for Live Oak Avenue, as represented in Figure SP 4-13, includes a slight easterly alignment within the Specific Plan project area, and an elevated over-crossing of the railroad tracks. This alignment minimizes future impact on spur tracks within the DuPont property (as visible in Figure SP 1-1), while maximizing the on-site space availability for Major Retail users in the Specific Plan project area. As represented in the Development Plan, this Live Oak Avenue alignment includes the potential for two bridge spans. The first would extend directly over the railroad tracks, while the optional adjoining second span which would facilitate truck movement along the northerly project boundary.

Internal vehicular movement is accommodated in Development Plan A through an internal intersection on Live Oak Avenue, approximately 350 feet north of Main Street. This intersection would require signalization at the time of extension into the DuPont Property.



Source: Mark Thomas & Co.

FIGURE SP 4-13: Plan and Profile of Live Oak Overcrossing of Railroad Tracks (Second Bridge Structure Not Shown)

The Specific Plan project includes construction of the earthen embankment and roadway segment south to Main Street, together with a right-of-way reservation, whereas the bridge structures will be an assigned obligation of future development on the DuPont property. Such contemplated future development north of the specific plan project area may also involve additional modifications to the Live Oak Avenue / Main Street signal, in order to efficiently accommodate all turning movements.

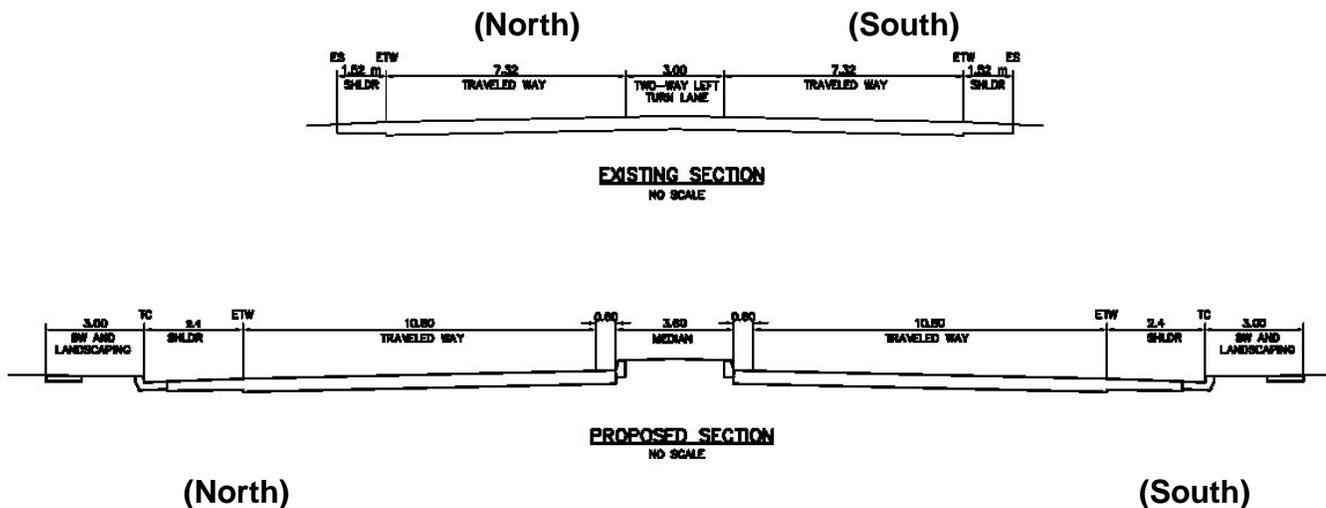
A second site plan, Alternative Development Plan B, was prepared and included in the Specific Plan to identify opportunities for redistribution of planned buildings and site improvements within the boundaries of the project area, in the event that the General Plan is amended in the future to eliminate a crossing of the railroad tracks using Live Oak Avenue. Such a determination is beyond the scope of this Specific Plan and the accompanying EIR, and would require a separate General Plan-level analysis of land use intensities on the DuPont Property and corresponding circulation alternatives.

4.4 Main Street Widening

The City contracted with the engineering firm of Nolte Associates to complete design work on a planned widening of Main Street from 4 to 6 lanes through the Specific Plan project area. As depicted in the preliminary cross-sections (Figure SP 4-14), this independent roadway widening project will include a raised center median. The median

will include landscaping and also provide for left-turn pockets at project area intersections.

The Development Plan accommodates a planned dedication of 20 feet along the entire length of the Cline Property. This dedication is required in order to accommodate the additional travel lanes and median. The Specific Plan project will contribute to the cost of this widening project through payment of traffic impact mitigation fees, and will fund the fronting curb, gutter and sidewalk improvements (along with associated utility improvements described below).



Source: City of Oakley.

FIGURE SP 4-14: Main Street Cross Sections

4.5 Frontage Roadway Network Improvements

Roadway network improvements may be funded and implemented in one of two ways. For improvements that are currently included in the City’s Five-Year Capital Improvement Project (CIP), it may be possible for the project to contribute its fair share through payment of the Transportation Impact Fee Program. Improvements that are not currently included in the City’s CIP may need to be added to the CIP or installed by the developer at the time of building permit issuance. Depending on the timing of the impact (i.e. near-term or cumulative/long-term) the most feasible funding and implementation mechanism for the each improvement may differ. This Specific Plan summarizes the improvements identified in the traffic analysis, and their likely timing for implementation; however, the Final EIR and Mitigation and Monitoring Program that will need to be certified and adopted by the City Council will determine the actual funding and implementation mechanism for each individual improvement identified in this Specific Plan and the Draft EIR.

The traffic analysis shows that four of the study area intersections currently operate at unacceptable levels without the addition of project traffic. These include Main Street /

Bridgehead Road / Neroly Road, Main Street / Live Oak Avenue, and Oakley Road / Neroly Road, and Laurel Road / Empire Avenue (See Figures SP 4-1, SP 4-6, SP 4-9 and SP 4-11). Currently funded roadway improvement projects will provide service level corrections at the following intersections:

- (a) Main Street / Live Oak Avenue: Signal light to be added.
- (b) Laurel Road / Empire Avenue: Signal light to be added.

The analysis indicates that prior to mitigation, with the addition of project traffic and other background traffic growth by the year 2010, poor operations at the intersection of Main Street / Bridgehead Road / Neroly Road would be exacerbated, and that unacceptable conditions would also occur at the Wilbur Avenue/SR 160 SB Ramps, Main Street / Sandy Lane, Oakley Road / Live Oak Avenue, and at Neroly Road / Live Oak Avenue. These conditions will be relieved through inclusion in the Specific Plan project of the following improvements:

- (c) Southbound Bridgehead Road Approach at Main Street: Addition of a second exclusive left-turn lane to provide one exclusive right-turn lane, one through lane, and two left-turn lanes.
- (d) Main Street / Sandy Lane: Construction of a traffic signal at the intersection (See Figure SP 4-8).
- (e) Oakley Road / Live Oak Avenue: Construction of a traffic signal at the intersection, and provision of exclusive left-turn lanes on all approaches.
- (f) Neroly Road/Live Oak Avenue: Construction of a traffic signal at the intersection, along with exclusive left-turn lanes on the northbound and southbound approaches.

The Wilbur Avenue/SR 160 SB Ramps would remain at unacceptable conditions; however, the intersection would not satisfy peak hour signal warrant standards with the addition of project-generated traffic. Thus, the proposed project would not cause a significant impact at this intersection under Near Term With Project conditions.

The planned extension of Live Oak Avenue to Main Street is already budgeted and proceeding as a City-sponsored improvement, and will be completed with a new signal light prior to completion of the Specific Plan project (See Figure SP 4-9).

The Specific Plan project is expected to be phased such that full buildout may not occur for a period of approximately 10 years. The Draft EIR and traffic analysis indicate that additional unacceptable level of service declines would occur under cumulative conditions with the project at the intersections of Wilbur Avenue/Viera Avenue, Wilbur Avenue / SR 160 SB Ramps, Wilbur Avenue / SR 160 NB Ramps, Wilbur

Avenue/Bridgehead Road, Oakley Road / Neroly Road, Main Street / Empire Avenue, Laurel Road / Empire Avenue, and Main Street / Neroly Road / Bridgehead Road. In order to avoid significant effects of traffic under the various long-term (cumulative) conditions with inclusion of the Specific Plan project, the following additional improvements are contemplated:

- (g) Wilbur Avenue / SR 160 Southbound Ramps, Wilbur Avenue / SR 160 Northbound Ramps, and Wilbur Avenue / Bridgehead Road: Installation of traffic signals at these intersections. Due to their proximity, these three intersections should be signalized at the same time and signal timings and phasings should be coordinated.
- (h) Oakley Road / Neroly Road: Construction of a traffic signal and provision of exclusive left-turn lanes on all approaches to this intersection.
- (i) Main Street / Empire Avenue: Intersection service level can be improved to acceptable conditions through implementation of the following three improvements: (1) Add a second exclusive left-turn lane on the westbound approach of the intersection, (2) convert the exclusive southbound right-turn lane at the Oakley Road/ Empire Avenue intersection to a shared through/right-turn lane, and (3) coordinate signal phasing and timing at the Main Street/Empire Avenue and Oakley Road/Empire Avenue intersections.
- (j) Laurel Road / Empire Avenue: The service level of this signalized intersection can be improved to acceptable conditions by adding a second exclusive left-turn lane and one exclusive right-turn lane on the eastbound approach and adding an exclusive right-turn lane on the southbound approach.
- (k) Main Street / Neroly Road / Bridgehead Road: Mitigation of the unacceptable conditions at this intersection can be achieved by converting the second exclusive left-turn lane to a share left-turn/through lane on the northbound approach.

4.6 Internal Site Circulation and Parking

As shown in Development Plan A (Figure SP 1-1), the site would be accessible from Bridgehead Road, Main Street and Live Oak Avenue. A signal-controlled full access point is proposed on Bridgehead Road, on Main Street opposite Sandy Lane, at the mid-block location between Sandy Lane and Live Oak Avenue, and eventually, at the easterly intersection located between Live Oak Avenue and Bridgehead Road.

The easterly opening on Main Street adjoining Secondary building Envelopes H and I will function adequately as a right-in/right-out driveway to serve the needs of the Specific Plan project. In order to accommodate long-term access for future development on the south side of Main Street, this access point will be signalized. Specific Plan

development will therefore provide for dedication of necessary public right-of-way and funding for 50% of the cost for a deferred signal improvement (as determined by the City Engineer).

In addition, a fully signalized intersection will be provided on the Live Oak Avenue extension, between Main Street and the railroad tracks at such time as Live Oak is extended north into the DuPont Property. This signal light will require inter-connection with a modified signal light on Main Street and Live Oak Avenue. The number of access points is sufficient to meet expected demand, based on incorporation of mitigation measures identified in the Draft project EIR.

An internal east-west roadway is proposed to extend the length of the site. It will extend through the center of the site, and will be signal controlled where it crosses Live Oak Avenue. An additional undercrossing of the future elevated section of Live Oak Avenue will be provided along the northerly property line, in order to facilitate the movement of trucks and service vehicles. Truck traffic will be routed into and out of the site from the signal-controlled intersections at Sandy Lane, the intersection immediately east of Sandy Lane, and the intersection east of Live Oak Avenue. Circulation of truck traffic onto Bridgehead Road and Live Oak Avenue will be avoided.

The proposed site layout will result in high pedestrian volumes crossing the major internal roadway between the Major Retailers and parking areas. Consequently, the internal roadway has been designed with traffic calming features to maintain reduced vehicle speeds. These include roundabouts, special pavement treatments, and pedestrian enhancements, such as sidewalk curb extensions and pedestrian-scale lighting.

Following are several examples of traffic calming features that Development Plan A (Figure SP 1-1) has incorporated, which are referenced in the Design Guidelines (Section 6.5).



Source: Richard T. Loewke, AICP.

FIGURE SP 4-15: Example of Roundabout

Note that one-way flow of traffic to the right around each leg of the intersection is designed to moderate traffic speed and enhance visual experience.



Source: Richard T. Loewke, AICP.

FIGURE SP 4-16: Example of Use of Decorative Paving

Distinctive texture, pattern and pigmentation of pedestrian crossings serves to identify crosswalks.



Source: Richard T. Loewke, AICP.

FIGURE SP 4-17: Example of Curb Extensions and Signage

Narrowing street width at the location of designated pedestrian crossings (by removing parking) can help to improve crosswalk visibility. The addition of carefully placed signage can further enhance pedestrian safety.



Source: Richard T. Loewke, AICP.

FIGURE SP 4-18: Example of Illuminated Street Surface Markers

Push Button-activated lighting is one more tool to heighten drivers' awareness of the presence of pedestrians at a crosswalk. The recessed surface-mounted lights flash on and off for a predetermined period of time, once the button is pushed.

Together, these features (with examples shown in Figures SP 4-15 through SP 4-18), will serve to facilitate safe and convenient pedestrian movement between retail buildings and parking areas on the site.

To provide adequate parking, the site will include enough parking stalls to meet peak parking demand plus an additional turnover factor. The addition of a minimum 5 percent turnover factor (in addition to the number of spaces needed to satisfy estimated ITE peak demand) will allow visitors to easily find an available parking stall and reduce the amount of on-site circulation that could lead to congestion and excessive queuing. Peak parking demand has been estimated using ITE's Parking Generation, including that needed for supercenter type uses pursuant to ITE Code 813 (2nd Edition). Peak conditions for the combination of uses identified in the Development Plan, and listed in Table SP 1-1 are expected to occur on the weekend and would result in a demand for approximately 3,218 parking stalls, based on the 690,000 gross floor area reflected in Development Plan Figure SP 1-1. This requirement of the Specific Plan exceeds the City's standard requirement for one space per 250 square feet of floor area in conventional centers. The Development Plan parking supply may be adjusted up or down, based on the parking standards set forth on Table SP 1-2. Based on the scale of this project, the Specific Plan development standards allow flexibility to accommodate up to an aggregate 10% of the floor space in the project for restaurant uses (consistent with the ITE peak demand plus turn-over analysis).

4.7 Public Transportation, Bicycle and Pedestrian Access

There are limited bicycle and pedestrian facilities currently within the project study area. In the vicinity of the project, bicycle lanes exist on Empire Avenue and portions of Vintage Parkway. The Oakley 2020 General Plan (adopted December 2002) and East County Bikeway Plan (November 2001) propose several new facilities to be constructed in the future. These include bike lanes planned for Main Street, Big Break Road, and Oakley Road. Frontage improvements along the Cline Property will incorporate the planned bike lane.

Local multi-use trails are proposed along the railroad right-of-way to the north of the project site, and along Neroly Road/Bridgehead Road and Live Oak Avenue. A network of regional trails, which would be maintained by the East Bay Regional Parks District, is also proposed in the vicinity of Big Break Road and along the water frontage.

Tri-Delta Transit currently operates four local bus routes and two express commuter routes within the specific plan project area as described below.⁴ Development of the Specific Plan will provide bus pullouts and bus shelters on Main Street adjacent to the project site to provide pedestrian access between the bus stops and the major retailers of the project.

- Route 300, the Pittsburg BART / Brentwood Park & Ride route, is a weekday express route connecting Brentwood to the Pittsburg/Bay Point BART station via Oakley and Antioch. The bus travels along Main Street adjacent to the project

⁴ All bus routes and times derived from www.trideltatransit.com.

vicinity. It has one stop in the study area near the intersection of Main Street/Big Break Road. The bus currently runs from 4:20 AM to 10:00 PM on ten- to thirty-minute headways.

- Route 383, the Oakley/Antioch/Freedom High School route, connects Oakley to Antioch and Freedom High School in Brentwood. One stop is provided in the study area near the intersection of Main Street/Big Break Road. This route is only in service on weekdays and has both clockwise and counterclockwise route patterns. The clockwise route currently stops at Main Street/Big Break Road at 7:24 AM and 7:32 AM. The counterclockwise route currently stops at Main Street/Big Break Road from 6:00 AM to 6:46:00 PM on approximately one-hour headways.
- Route 391, the BART / Pittsburg / Antioch / Oakley / Brentwood route, provides weekday service to most East County cities. In Oakley, a stop is provided near the intersection of Main Street/Empire Avenue. The bus now runs from 4:00 AM to 1:15 AM on thirty- to seventy-minute headways.
- Route 392, the BART / Pittsburg / Antioch / Oakley / Brentwood route is the weekend service of Route 391. However, no stop is provided in Oakley within the study area. The bus runs from 5:50 AM to 2:00 AM on sixty- to ninety-minute headways.
- Delta Express, the express commuter bus run by Tri-Delta Transit, presently has two routes with stops in Oakley. One route connects Oakley with the Dublin/Pleasanton BART station, where passengers can connect to a free shuttle to the Bishop Ranch Business Park. Passengers can board the bus at the Oakley Albertsons at 5:11 AM and 5:41 AM, with returning service at 5:59 PM and 6:59 PM. Another route connects to Lawrence Livermore National Lab, departing the Oakley Albertsons at 5:26 AM and 6:26 AM and returning at 5:13 PM and 6:13 PM.

SECTION 5. INFRASTRUCTURE & PUBLIC SERVICES

5.1 Municipal Services Including Police

The specific plan project site is situated within the Oakley municipal boundaries. The City currently provides police, engineering, park, maintenance and administrative services to the site. Development of the property, as contemplated in the Development Plan would result in the addition of between 630,000 and 690,000 square feet of retail and service uses. As detailed in Table SP 1-1, optional development of a hotel with between 75 and 100 rooms could replace the 30,000 - 40,000 square feet of retail uses. The hotel use would generate additional tax increment and transient occupancy tax in lieu of sales tax from Secondary Building Envelope S. The combined development potential on the site is expected, under all contemplated scenarios, to generate a net surplus of revenues to the City / Agency, after taking servicing costs into consideration.

Additional safety and security procedures to be implemented by the first Major Retail use in the Specific Plan area through the use of professional private security forces, include the following:

Install closed-circuit camera systems (surveillance cameras) inside and outside store buildings, monitored by security employees.

Establish a parking lot patrol, which assists customers, ensures safety and takes action to identify and prevent any suspicious activity (such as loitering and vandalism) both during the day and nighttime hours.

Establish a plainclothes patrol inside the Major Retail buildings to ensure safety and security.

Establish and implement a training program for associates to identify and report safety and security issues at the site. The Police chief shall be consulted on any such training program and have the opportunity to give input, which shall be integrated into the program.

Provide lighting in the parking areas that will ensure public safety.

Prohibit consumption of alcohol in the parking lots by having security regularly patrol the parking areas. Limit alcohol sales to the hours of 6AM to 2AM of the following day, in accordance with State Law.

Hold, at a minimum, quarterly meetings with the Police Chief, or his/her designee, regarding criminal activity on the site and the effectiveness of deterrent practices. At any time the Police Chief, or his/her designee, may recommend more frequent meetings and/or additional safety measures to be implemented by the owner(s)/manager(s) of the properties on site.

Install external security cameras to provide coverage of active areas of the site. The cameras shall be coordinated with the Oakley Police Department so that video may be uploaded by the Police Department upon request. The location and design of the cameras shall not contradict the architecture and design of the building on which they are mounted, subject to the review and approval of the Community Development Director.

Upon final design, the northern boundary of the project shall include means for visible access to the railroad tracks for safety precautions. If necessary, emergency vehicle access may be required at the discretion of the Community Development Department or Police Department.

Each architectural review shall include parking space(s) reserved for security or other emergency vehicles, as deemed necessary by the Police Department or Community Development Department.

5.2 Fire Protection

Emergency response services for this project will be provided by the East Contra Costa Fire Protection District. The District maintains a station on Second Street in the downtown area, east of the site. Plans for this project must be reviewed and approved in accordance with inter-district agreements by the Contra Costa Fire Protection District. The Development Plan identifies an arrangement of proposed buildings with full access to fire trucks and other emergency response equipment to meet District standards. Site improvements will include a system of pressurized fire flow supply lines and hydrants positioned as required by the District in accordance with California Fire Code requirements. All buildings on the site will also be equipped with automatic fire sprinklers pursuant to NFPA 13 requirements.

5.3 Sanitary Sewer

The 76.4-acre site is situated within the boundaries of the Ironhouse Sanitary District (ISD), which maintains a gravity flow trunk line within Main Street. Service to the project site will require connection to ISD facilities, subject to applicable permits and fees. ISD maintains an 8-inch sanitary sewer trunk line that flows south along the frontage of the Cline Property in Bridgehead Road, turning east and continuing along the south side of Main Street. This line increases in size to 18-inch diameter at a point just east of the southwesterly corner of the Cline Property frontage (west of Sandy Lane), and continues to run easterly to a point roughly 900 feet east of the Live Oak Avenue intersection. The 18-inch line then turns south, eventually connecting with Carol lane and Main Street at a point southeast of the Big Break Road intersection. On-site sewer collection systems will outfall into the Main Street trunk line in accordance with the overall phasing plan for future development. While more than one connection is possible, these connection points should be minimized to avoid multiple crossings of

Main Street. Additional details of sewer service facilities are addressed in Chapter 3.13 of the DEIR.

5.4 Water Supply

The site is also within the service boundaries of the Diablo Water District (DWD) and the Contra Costa Water District (CCWD). DWD maintains a system of water distribution lines throughout the City, and jointly operates a water treatment plant with CCWD. CCWD provides raw water to DWD through its Los Vaqueros storage facility and the Contra Costa Canal. The two districts jointly operate the Randall-Bold Water Treatment Plant located south of the project area on Neroly Road near Laurel Road.

A detailed Water Supply Assessment (WSA) was prepared by DWD in order to determine the ultimate water supply needs of future development within the Specific Plan area, and to confirm the reliability of supply to serve the project. The WSA has been incorporated into the project DEIR, and estimates that full development of all uses and site improvements identified in the Development Plan and quantified in Alternative 1 of Table SP 1-1 would generate the need for up to 172,000 gallons per day, or 192.7 acre-feet per year of water. The WSA has verified that the 76.4-acre Specific Plan area and its planned uses are included in the water demands identified by DWD in the District's 2005 Urban Water Management Plan (UWMP). The WSA also confirms that DWD has adequate supply sources to meet future needs under all conditions for its future planning area, including the Specific Plan project area.

Water supply to developing commercial uses within the Specific Plan area will be delivered by the following DWD water distribution facilities: (a) a 10-inch water main that extends along the south side of the site's one-mile long frontage on Main Street; (b) a 24-inch line that extends along the northerly boundary of the Cline Property from Big Break Road on the east to a point in the vicinity of Major Retail building Envelope A (roughly 800 feet east of Bridgehead Road), and then runs north under the railroad line into the DuPont Property; and (c) a 12-inch line that extends along the Bridgehead Road frontage of the Cline Property. These water supply facilities are looped and sized to accommodate both domestic and fire flow needs of developing properties in the City's northwesterly commercial and industrial area. Additional lines will be required to connect to these facilities, and related improvements may be required as determined DWD in order to adequately serve the project. Details of the infrastructure and utility service systems for the project area are provided in the Draft EIR (Chapter 3.13).

5.5 Storm Drainage

The Specific Plan development area is located within drainage area 29H. This area is currently served by a storm drain line that flows easterly within the Main Street right-of-way across the frontage of the Cline Property to a point approximately 1,500 feet west of the southeast property corner. At this point the line continues in a northeasterly direction through an easement across the Cline Property and under the railroad right-of-

way, toward an eventual outfall leading into the San Joaquin River. The on-site portion of the existing storm drain line crosses the site between planned Secondary Retail building Envelopes D and E. The detailed design of these buildings will identify the location of this pipeline, in order to avoid potential conflicts in construction, as detailed in DEIR Chapter 3.11. This line has sufficient size and depth to accept flows from development as contemplated in the Specific Plan. On-site building, parking lot and landscape areas will be served by private collections systems that must convey flows to the easterly end of the site, where they can tie into the current storm drain trunk line.

5.6 Gas, Electricity, Telephone and Cable

PG&E currently operates a 21KV primary electric supply line along the north side of Main Street on overhead poles and wires (as visible in Figure SP 3-2). Additional primary (21KV) and secondary electric service lines extend along the property's frontage on Bridgehead Road. These facilities have ample capacity to serve planned retail development within the Specific Plan project area. The City's current Main Street widening project will provide for relocation of the poles along the frontage of the Cline Property; development as authorized under the Specific Plan will be responsible for placing the Main Street and Bridgehead Road lines underground (see visible in Figure SP 3-9).

Existing gas pipelines cross the westerly end of the project site in a North-South alignment. These lines currently cross the site in the vicinity of Major Retail Envelope A. As discussed in the DEIR Chapter 3.13, either these lines will need to be realigned or the envelope for Building A will need to be adjusted, in order to avoid construction and/or operational conflicts.

SECTION 6. DEVELOPMENT STANDARDS

6.1 Development Phasing

Development within the project area may be phased in any sequence or scale consistent with the Development Plan and policies contained in this Specific Plan, subject to the following minimum standards as determined through final engineering design and approved by the City Engineer:

- (a) Provision shall be made for continuous vehicular and pedestrian access throughout the entire developed area of the site; any subdivision of the site shall provide for reciprocal parking and vehicular access easements, which may be enforced by the City / Agency.
- (b) None of the Major Retail building envelopes may be developed without concurrent and proportional frontage improvements along Main Street and Bridgehead Road, calculated on a minimum of one-third of the combined project frontages per Major Retail use.
- (c) Development of Major Retail Envelope A shall, at a minimum, require concurrent improvement of the full Bridgehead Road frontage, along with the westerly portion of the Main Street frontage to a point easterly of the Sandy Lane intersection.
- (d) Development of Major Retail Envelope B and/or C shall, at a minimum, require concurrent improvement of the entire Main Street frontage.
- (e) Development of Major Retail Envelope B and/or C shall require dedication of the full Live Oak Avenue right-of-way, and full improvements within the right-of-way from Main Street north to the first bridge abutment (including the water feature).
- (f) Development of Major Retail Envelope B and/or C shall also require improvement of the signalized intersection at Live Oak Avenue and the project east-west roadway (may be deferred until such time as Live Oak Avenue is extended over the railroad right-of-way).
- (g) Signal light improvements at the Bridgehead Road project entry intersection shall be included with development of any of the Major Retail envelopes.
- (h) Right-of-way dedication and 50% funding for the future signal light at the easterly project entrance on Main Street shall be provided concurrently with development of any project phase east of Live Oak Avenue. Full improvement of this intersection and signal light may be required by the City Engineer with development of the easterly portion of the project site, subject to partial reimbursement as properties on the south side of Main Street develop.

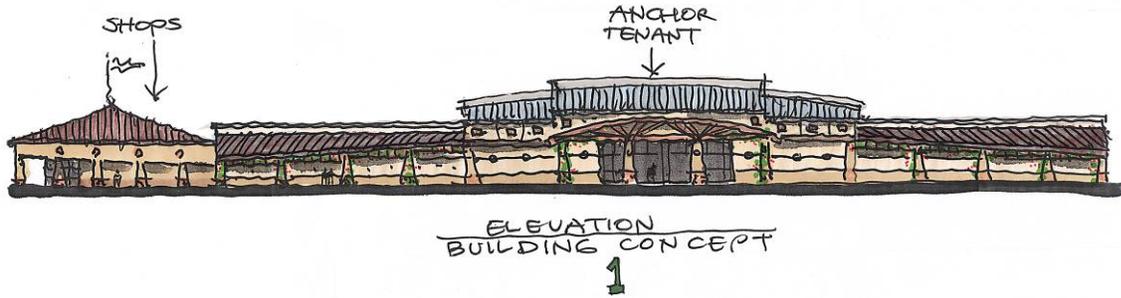
- (i) Internal circulation improvements and parking facilities shall be provided with each project phase in accordance with standards as outlined in the appropriate section of this document.
- (j) The easterly water feature shall be improved concurrently with initial development east of Live Oak Avenue.
- (k) All landscape and utility improvement plans shall be reviewed and approved by the City Engineer prior to issuance of building and site improvement permits for each phase of development.
- (l) No permit for building or site improvements may be issued until a determination of consistency with the Specific Plan has been issued by the Community Development Director. Each phase of construction shall incorporate environmental mitigation measures related to site improvements and uses.

6.2 Building Envelopes and Improvements

The Major and Secondary Retail building envelopes are identified in Development Plan A (Figure SP 1-1); the scale of these envelopes is further detailed in Table SP 1-1. The specific footprint, shape and orientation of all buildings and improvements within and adjoining the building envelopes and the surrounding site areas shall be consistent with all policies contained in this document, including the Parking, Building Height and Setback Standards summarized in Table SP 1-2 and further discussed in Section 6.4, the Design Guidelines identified in Section 6.5, and the Energy Conservation and Waste Reduction standards identified in Section 6.6. Section 3 (Land Use) of this Specific Plan contains policy options for consideration of retail and service uses on both Major and Secondary sites in the project area, including options for alternative uses at Secondary Retail Envelope S (in the event that a hotel use is not developed).

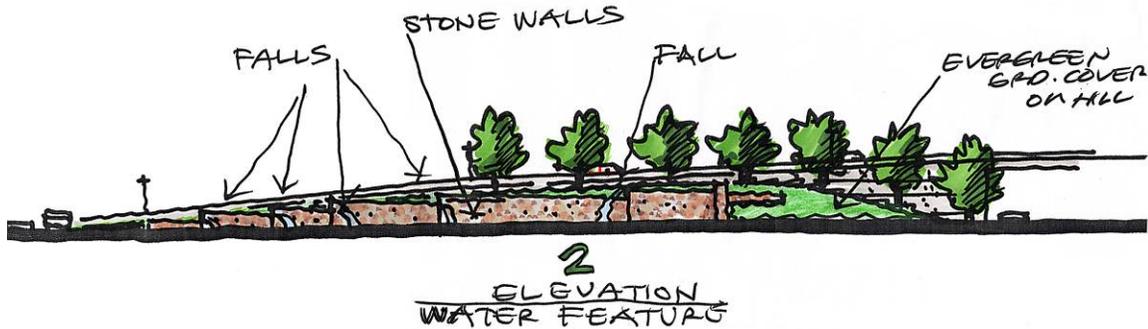
6.3 Site Improvements

All development within the Specific Plan area shall be subject to a determination of consistency with the approved Development Plan, and Architectural Review of building plans as outlined in Section 7.1. No permit for building or site improvements may be issued until a determination of consistency with the Specific Plan has been issued by the Community Development Director. Each phase of construction shall incorporate environmental mitigation measures related to site improvements and uses. Plans shall include details of building size and placement, parking supply, landscape, utility system and roadway improvements. All such improvements shall adhere to the Standards identified in Sections 6.4 through 6.6.



Source: City of Oakley.

FIGURE SP 6-1: Building Concept Elevation



Source: David Gates and Associates, Inc.

FIGURE SP 6-2: Water Feature Elevation at Live Oak Avenue



Source: Richard T. Loewke, AICP.

FIGURE SP 6-3: Simulated Aerial Perspective of Project

6.4 Parking Standards

On site parking within the Specific Plan project area will generally be provided in accordance with standards detailed in Table SP 1-2. As outlined in the Circulation Section above, this standard, as applied to the Specific Plan, calls for an aggregate of 4.5 spaces per 1,000 square feet of gross retail floor space, plus 1.25 space per hotel room, plus a minimum vacancy turn-over factor of 5% (which provides for a restaurant allowance of up to 10% of the gross floor area). As applied to the Development Plan, this translates into a total of 3,218 parking spaces for a development with an aggregate floor area of 690,000 square feet. Based on the scale of this project, flexibility is provided to accommodate up to an aggregate 10% of the floor space in the project to restaurant uses. Restaurants within the project are expected to range from coffee kiosks to a full-service dinner house, and will be distributed over the mile-long project site to complement the Major Retailers by making efficient use of shared customer shopping trips.

As further evaluated in the Circulation Section and analyzed in the project EIR, parking adequacy is determined, in part, through the provision of a supply that is sufficient to meet peak demand, plus an additional turnover factor. The addition of a minimum 5 percent turnover factor will allow visitors to easily find an available parking stall and reduce the amount of on-site circulation that could lead to congestion and excessive queuing. Peak parking demand has been estimated using ITE's Parking Generation (2nd Edition). Peak conditions are expected to occur on the weekend and would result in a demand for approximately 3,218 parking stalls, based on the 690,000 square feet reflected in Development Plan A (Figure SP 1-1). This standard exceeds the normal City Zoning Ordinance standard of four spaces per 1,000 square feet.

The parking requirements associated with the Lower Intensity Alternative (Alternative 1) follow the same parking standard of 4.5 spaces per 1,000 square feet plus 1.25 space per hotel room, with a turn-over factor, as summarized in Table SP 1-2. Each project phase will be tested at the time of application for building and site improvement permits in order to assure adequacy of parking improvements to meet phased building gross floor space requirements, as well as consistency with the adopted Development Plan and the provision of landscape improvements in accordance with the City's adopted Commercial Design Guidelines (see Section 6.5 below).

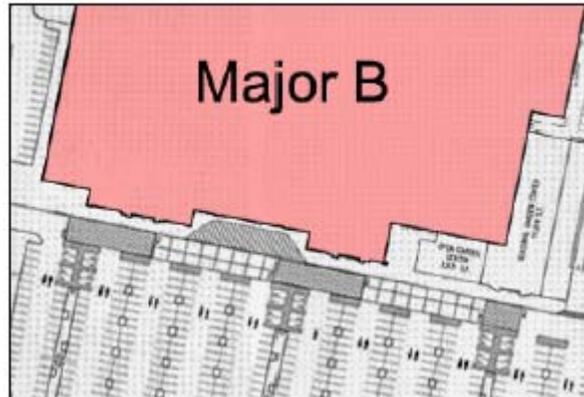
6.5 Design Guidelines

The City's Commercial and Industrial Design Guidelines provide a framework to guide the architectural and landscape design components of future phased improvements within the specific plan development area. In particular, Section II (Commercial Guidelines) and Section V (Streetscape) contain policies that have been adapted for applicability to future development as approved under the Development Plan. Following are the policies that have been included in the Specific Plan for implementation through the building and site improvement permit review, and Architectural Review processes:

(a) Retail Building Envelopes

1. All Major and Secondary Retail use buildings, as defined in Section 1, shall conform generally to the locations as shown in the Development Plan.

2. Orient building fronts and entries towards streets, parking lots and public spaces.

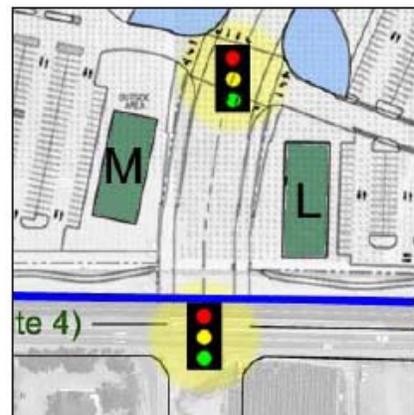


3. Corners should be focal points, with features such as special architecture, vertical elements, building entries or public plazas with focal elements and landscaping. Avoid placing auto oriented uses such as automotive services or drive through food merchants at corner locations.

4. Group buildings to encourage pedestrian travel within the site and between adjacent parcels. Cluster Secondary Retail buildings to create "outdoor rooms" with seating, shade and protection from wind and traffic noise.

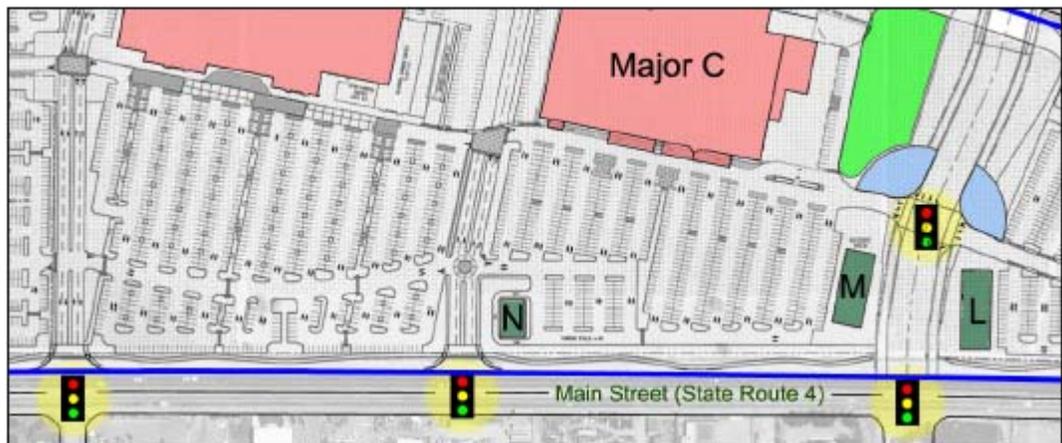
5. Provide public plaza and widened sidewalk areas between Major Retail building envelopes to accommodate a variety of outdoor functions, including seating, display, and food sales. These plazas should be visible from public circulation areas and inviting to the passersby. An example of such a plaza occurs between Secondary Retail building Envelopes E, F and G in Development Plan A; the size and configuration of plazas is subject to adjustment, depending on the scale and nature of retail users served.

6. Setbacks for building envelopes adjoining Main Street, Live Oak Avenue and Bridgehead Road shall be a minimum of 20 feet. Compliance will be verified by the Director of Community Development at the time of building and site improvement plan review. Internal driveway setbacks are required only as necessary to accommodate pedestrian walkways and plaza areas.

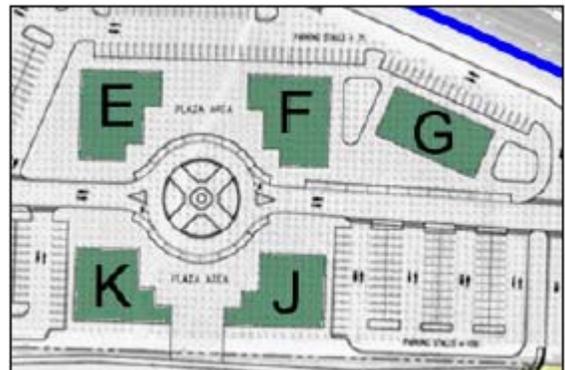


(b) Entries, Circulation and Parking

1. Entries on Main Street, Live Oak Avenue and Bridgehead Road should be clearly marked with entry elements such as signage, landmark elements, special paving, and significant landscaping, as approved by the Community Development Director.
2. Sight lines must be preserved for traffic safety.
3. A centrally located bus pull-out will be provided near the center of the project frontage on Main Street as referenced in Section 4.
4. Provide a clear travel route between the street and the entry to the Major Retail building envelopes, as shown in Figure SP 6-1. The primary access drive should be separated from parking by landscaping and pedestrian pathways, creating an internal “street”.



5. Roundabouts as depicted in Figure SP 4-15, or similar traffic control elements, are encouraged at the major internal driveway intersections in order to control vehicular movements, while adding visual character through use of special paving, raised landscape elements and/or water features.



6. Vehicle entries should terminate visually at a Major Retail building envelope.

7. Driveway locations on Main Street and Bridgehead Road shall be limited to the number and approximate locations shown in the Development Plan, in order to maintain pedestrian and traffic safety, and to unify the appearance of the street frontage. Adjustments to driveway locations shall be subject to review and approval by the City Engineer, and may require additional traffic analysis as determined by the City Engineer.
8. Secondary Retail building envelopes should be accessed by means of the primary center entry drive.
9. Access driveways shall provide adequate length to accommodate off-street vehicle stacking needs during times of peak use as shown in Figure SP 6-1.
10. Where a drive-through lane is required for a commercial use on a large site, it should be located to the side or rear of the property, and separated from any adjacent parking or drive aisles by landscaping.
11. Parking areas should not visually dominate any portion of the site.
12. The large on-site parking areas shall be divided into a series of smaller parking lots, as reflected in the Development Plan. Landscaping plans for parking lot areas shall provide shade and create visual breaks, and shall be submitted for review and approval by the Community Development Director.
13. Screen parking areas from Main Street and Bridgehead Road with landscaping and berms or low walls. Use low hedges or architectural walls to create a defined edge and to screen views of parking lots where they abut streets and pedestrian use areas. The minimum width of frontage landscape improvements shall average 25 feet; turn-outs and other traffic control improvements may be included within this area.
14. Provide convenient bicycle parking areas with bicycle racks near building entries, at a ratio as approved by the Community Development Director.
15. Reduce or detain storm water runoff by using vegetated swales between parking aisles and at the perimeter of the parking areas. Provide perforated curbs at swales. Explore the options of using porous paving for parking stalls, especially in more remote areas of parking lots.
16. Tree planting and selection and massing should be compatible with streetscape plantings. The plant palette should be relatively limited and applied in groupings of similar species rather than a few plants of many different species planted together.

17. Parking stalls should be double-striped.
18. Pedestrian circulation should be clear, safe, inviting and comfortable. Curb extensions and signage can enhance these features, as shown in Figure SP 4-17.
19. Provide clear and convenient pedestrian connections from the public streets, sidewalks, transit stops and trails to uses on the site.



20. Distinguish pedestrian pathways from vehicular drives through use of differing paving texture, color and/or materials. Figure SP 4-16 includes an example of pedestrian paving accents. Where pedestrian pathways cross vehicular drives, provide a clearly delineated crosswalk, and where possible, raise the pedestrian paving surface to improve visual differentiation.
21. Provide separated pedestrian circulation through parking areas. Where the pedestrian pathway acts as a “sidewalk” to the internal “street,” separate it from traffic by means of a raised curb and landscaping or bollards.
22. Create wide sidewalks and “outdoor rooms” adjacent to store fronts, with room for pedestrian amenities, such as cafe seating, display, benches and planting.
23. Provide adequate lighting for pedestrian safety. One foot-candle is the minimum light level required. Additional surface lighting may be used in special circumstances to enhance pedestrian safety, as shown in Figure 4-18.
24. Separate pedestrian circulation from parking and traffic.
25. All City standards for parking lot designs, including minimum 24’ aisle widths and minimum 9’ by 19’ parking space dimensions, shall be satisfied and verified at time of permit application.
26. Compact parking spaces may be provided in accordance with City standards, based on a minimum dimension of 9’ by 16’; however, such

spaces shall be distributed such that not more than 2 compact spaces are located within any group of 10 or fewer aggregate spaces.

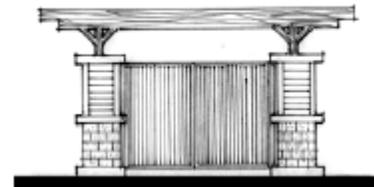
27. The use of pervious paving materials are encouraged in place of impervious paving materials where feasible and appropriate in order to assist in reducing the overall use of impervious surfaces on the site.
28. Enhanced traffic calming measures are required to be installed throughout the site, subject to the review and approval of the City Engineer.
29. Encourages the use of the Bridgehead Road driveway as a truck delivery access, and separate circulation for delivery trucks and customer vehicles as close as possible to the driveway entrances.

(c) Landscape Setbacks and Buffering

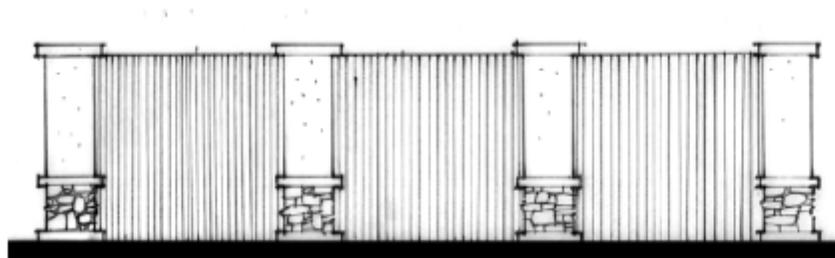
1. Provide a landscape strip, or raised planters or planter boxes, with shrubs and live ground cover to soften Secondary Retail building walls visible from Main Street, Bridgehead Road and Live Oak Avenue. Incorporate use of drip irrigation or equivalent systems to control water usage. Areas along exterior building walls that may conflict with a landscape strip, such as public dining areas and landing pads, may exclude the landscape strip upon review and approval of the Community Development Director.
2. Provide a minimum eight-foot wide landscape strip with trees, shrubs and live ground cover adjacent to the public-right-of-ways to separate sidewalks from parking.
3. Bio-swales shall be incorporated into landscaped areas at the rear property line, to aid in filtering pollutants and reducing stormwater runoff; swales should be planted with traditional landscape materials to avoid a “weedy” look.

(d) Trash Enclosures, Loading Areas and Mechanical Equipment

1. Locate trash, loading and service areas, and mechanical equipment to minimize their visibility from public areas.
2. Service, loading and trash areas should be placed to the rear or side of buildings, and not between buildings and adjacent public streets.
3. Loading facilities for Major Retail uses shall be located along the north property line, in order to allow for effective screening.



4. Design, materials and colors of trash enclosures shall match the main building.
5. Trash enclosures shall have a roof that matches the architectural character of the main building.
6. Service and loading areas should be screened with landscaping, or with walls or fences that are compatible with the architecture of the building.

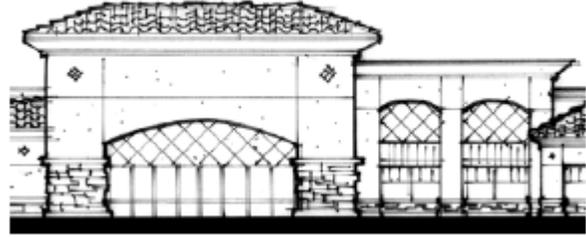


7. Trash areas shall be enclosed by an architectural structure compatible with the building. Trash areas must be fully covered. Openings shall be oriented away from public areas.
8. Ground level mechanical equipment shall be screened with landscaping or with walls or fences compatible with and integrated into the architecture of the building.
9. If a chain link enclosure is required around equipment, it shall be black or dark green vinyl-clad, and should be screened with planting.
10. Roof mounted equipment shall be screened from all points normally visible to the public, or from adjacent uses.
11. Roof mounted equipment shall be screened by the building roof or parapet. The use of secondary screens or false walls for screening is discouraged.

(e) Building Design

1. Create harmonious compositions of buildings which are appropriately scaled to their surroundings, and which present a varied visual experience for the pedestrian user.
2. Emphasize individual structures, but design all Major and Secondary Retail buildings to relate to each other. Connect Secondary Retail buildings with an arcade or pedestrian walk.

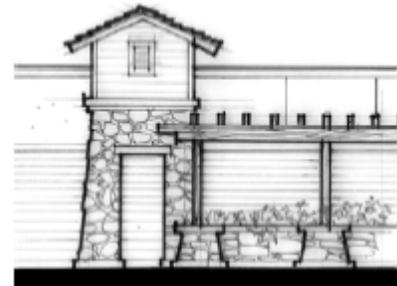
3. Large structures shall be visually broken up into smaller components through changes in massing, form and texture.



4. Major Retail buildings and Secondary Retail buildings at project entry points shall be visual landmarks. These buildings should be highlighted with features such as special architecture, towers or other vertical elements and entries facing the intersection, as depicted in Figure SP 6-1.

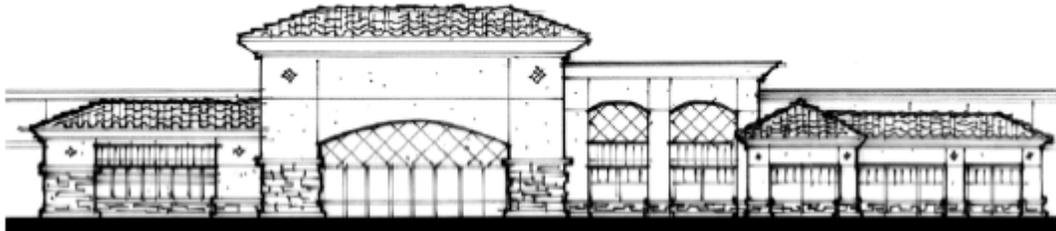
5. Vertically articulated elements such as enhanced entries, and varied rooflines used as architectural landmarks are encouraged. The height of vertical towers shall relate positively to the Major Retail buildings. Such elements shall not exceed twice the height of the adjacent building elements.

6. A series of central focus elements are to be included in the Specific Plan development area. These include public plaza areas between the Major Retail building envelopes, adjoining the elevated embankment of Live Oak Avenue, and at the far eastern end of the site adjoining Secondary envelopes as shown in Figure SP 1-1. Water features shall be designed and incorporated into improvement plans.



7. Major Retail buildings should be highlighted by strong architectural features.
8. All buildings shall be designed with detailed and visually interesting facades.
9. Building entries shall be clearly visible and inviting.
10. All visible building sides shall be designed with an integral and interesting appearance. A continuity of design, materials, color, form and architectural detail shall be carried throughout the project site.
11. Each commercial building shall have a visible base, a clear pattern of openings and surface features, a well defined entry, and an interesting roof line.

12. Sculpt building mass and roof lines to highlight building entries, corners and plaza areas.
13. Design building facades for visual interest and pedestrian scale using a creative mixture of elements such as arcades, columns, awnings, signage, displays, overhangs and fenestration. Articulated elements should appear integral to the building, rather than “pasted on.”
14. Use architectural elements to clearly define entries. Towers, changes in roof form, building recesses or projections, awnings, arches, porticos or other design features can highlight entries.
15. Pedestrian plazas shall be provided at entry areas, and shall include outdoor amenities such as planting and seating.
16. Define building entries with landscape elements.
17. Major Retail buildings shall have facades that include variations in massing, form and texture, and shall incorporate arcades, display windows, entry areas, awnings, or similar elements for at least half of the façade width.



18. Continuous surface treatments of a single material shall be minimized. Monolithic blank walls are not acceptable on any visible façade. Where the building surface lacks articulation, vine-covered trellises or other landscape features may be used to provide visual interest.
19. External downspouts shall be designed to blend with the architecture of the building, to minimize visibility.
20. Gable, hip and other two-sloped roof forms are encouraged.
21. Use of substantial roof overhangs is encouraged.
22. Partial gable or hip roof forms may be acceptable in combination with parapet roofs as reflected in Figure SP 6-2.

23. Decorative towers and other special design features may exceed the established building height limit so long as the elements do not exceed 15% of the building footprint and do not contain habitable space.
24. High quality materials shall be used for all building and site development elements. Materials shall convey a sense of permanence.
25. Preferred materials include brick, stucco, wood siding and stone; split-faced manufactured Concrete Masonry Units (CMUs) and architecturally treated CMUs may also be used for Major Retail buildings, as determined through Architectural Review.
26. Metal buildings are discouraged, however metal and artificial stone may be considered as architectural elements on a case by case basis.
27. Plywood as an exterior surface is not allowed.
28. Mirrored glass or heavily tinted glass are discouraged, as they cause glare, and/or appear dark and foreboding. Clear glass provides visual access to window displays and interior activities.
29. Each building shall use a maximum of six shades or colors that complement and relate to each other. Any unpainted material such as brick or stone, this material shall become one of the chosen colors.
30. Paint and building materials shall be superior in quality. Graffiti shall be controlled through use of one or more of the following: (a) use of graffiti-resistant paint; (b) use of building materials non-conductive to graffiti; and (c) use of landscaping on exterior walls to discourage graffiti. In addition, all Major and Secondary Retail tenants shall submit a graffiti removal program for review and approval by the Community Development Director prior to occupancy, and shall continuously maintain all exterior surfaces in accordance with the approved program.
31. Building colors shall be compatible, subdued and not garish. A building's color shall not become the identifying signage of the building or its use.
32. All parking and outdoor storage areas must be paved; D.G. surfacing is not acceptable for these areas.

(f) Landscape and Plaza Area Improvements

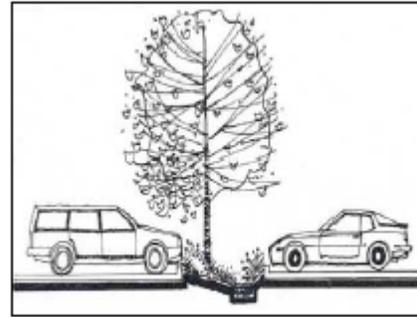
1. Landscaping shall consist of a combination of trees, shrubs, and groundcover and/or turf. Portions of required landscaped areas may be devoted to pedestrian walkways and special elements such as landscape,

- water features, or art forms. The use of mulch or stone as the sole ground cover material is not acceptable.
2. Landscaping shall provide for a lush appearance, be intense in its spacings, consist of a variety of plant material, and display varied colors throughout the year.
 3. Appropriate placement of landscape materials shall provide shade on parking spaces, drives, and paths.
 4. Plant materials shall be in proper scale with the environment. For example, large scale buildings and parking areas require large scale trees at maturity.
 5. Explore opportunities to highlight Oakley's agricultural and viticultural heritage with the creative use of landscape elements.
 6. Trees shall be provided at the ratio of at least one tree provided for every 600 square feet landscaped area, not including the required parking lot trees. Storm water retention areas shall be excluded from this requirement.
 7. Trees shall be minimum 15-gallon size, with at least 40% in 24-inch box size or larger. Shrubs shall be of 5 gallon size. Smaller container sizes may be used for perennials and groundcovers. Groundcovers shall be spaced to allow for complete infill within 6 months.
 8. Frontage landscaping shall relate to the adjacent streetscape.
 9. Landscape materials shall not interfere with safe sight distances for vehicular, bicycle or pedestrian traffic.
 10. The use of water conserving plantings, such as California natives and drought tolerant trees, shrubs and ground cover is encouraged.
 11. Landscaping at major site entries entrances shall help define the project and provide a sense of arrival. Use elements such as monoliths, low ornamental walls or fences, accent planting, fountains, sculpture, and special paving to help define the space. Accent trees shall be used to distinguish main access drives.
 12. To reflect Oakley's relationship to the Delta, incorporate the use of prominent water features at two locations as shown on the Development Plan: (a) along Live Oak Avenue at the internal access drive, and (b) at the easterly end of the site on Main Street. These water features shall

include fountains, ponds with moving water elements, or other water sculptures, as approved by the Community Development Director.

13. The use of stone as a decorative or focal element is encouraged at entries and plazas.
14. Enhanced paving shall be used to highlight entries and pedestrian areas. In pedestrian walkways and plazas, the use of stone paving, toned concrete, unit pavers or bricks is encouraged. Use toned concrete or similar paving material to define vehicular entries.
15. Oakley's hot summer climate makes shading of pedestrian areas extremely important. Use trees or shade structures along pedestrian walks, in outdoor rooms and plazas, and at seating areas.
16. Pedestrian comfort must be considered in the design of plazas and outdoor rooms. Where wind protection cannot be achieved by building placement, provide wind screens. Reduce glare in plaza areas by use of landscaping and colored paving. Separate pedestrian zones from parking and vehicle circulation with landscape areas, raised planters, trellises with vines, or large potted plants.
17. Provide a variety of seating in pedestrian areas. Provide seating and shaded tables in plazas. Encourage active use by cafes and restaurants.
18. Artwork shall be provided at one or more principal plaza or entry locations, as approved by the Community Development Director, to consist of a focal sculpture, unique trellis or tile mosaic.
19. Tree planting in parking areas shall create an "orchard" effect, shading and softening the appearance of the parking lot. At least 50% of the paved area shall be shaded at the trees' maturity.
20. Parking lot trees shall be provided at a minimum of 1 tree per 6 spaces, and may be clustered to define circulation routes and frame site views.
21. Large scale, high branching shade trees shall be used in all parking areas. Smaller accent tree plantings may be used within the parking area as special design features, with the larger trees as the primary trees. Parking lot trees shall be pruned for a minimum of 6' high clearance above ground.

22. Parking lot planting islands shall function as bioswales to absorb and filter stormwater runoff. These islands shall contain a usable minimum landscaped width of 5 feet, and be designed to accommodate the bio-swale, landscaping consisting of trees and shrubs and/or live groundcover, lighting and other utilities.



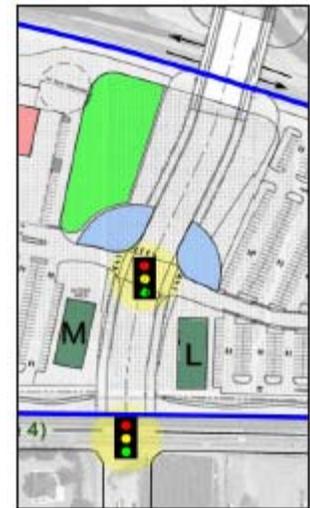
23. Parking lots shall be buffered by landscape areas at all property lines, as reflected in Figure SP 6-1.

24. Site lighting shall highlight building entries, plazas, walkways and outdoor rooms, and architectural features.

25. Site lighting shall incorporate cut-offs to prevent spillover laterally onto adjacent properties and upwards into the night sky.

26. Pedestrian scale lighting shall be used for pedestrian walkways through parking areas.

27. The use of grape vines as ornamental landscaping is encouraged throughout the site where feasible and appropriate, subject to the review and approval of the Community Development Department.



(g) Streetscape Improvements

1. A highlighted intersection occurs at Live Oak Avenue and the internal project drive. This intersection should receive enhanced treatment, which may take the form of greater setbacks, enhanced planting and a prominent water feature.
2. Components of this highlighted intersection should include increased setbacks for planting, specimen or accent trees, or other vertical elements, architectural monumentation, special pavement at crosswalks, colorful flowering perennials, a system of diagonal or curvilinear walls, with vertical monuments in key locations, landmarks, planting, signage, lighting and site furniture that are reflective of the character of the area.
3. Maintain clear sight lines at all intersections.

4. Extend special accent treatment to include the future adjacent median on Main Street.

(h) Street Furniture and Lighting

1. Benches, bollards, trash receptacles and other furnishings shall be provided at appropriate locations in the streetscape network.
2. All furnishings shall be resistant to weather and vandalism, and shall be approved by the City's Community Development Department.
3. A standardized thematic palette of lights shall be approved for consistent use in the Specific Plan project area. This palette shall include a hierarchy of lights ranging from tall poles with double fixtures, to shorter poles with single light fixtures. Where appropriate, banner attachments should be included as an option.
4. Illumination levels for the arterials and collector streets shall reflect the different right-of-way widths and functions. Electroliers shall be scaled in size to match the size of areas to be lit and should relate to human scale, especially in pedestrian areas. Streetlights should provide a safe and desirable level of illumination for both motorists and pedestrians.
5. Surface-level pedestrian marker lighting, as shown in Figure SP 4-18, may be employed at key pedestrian crossings to enhance safety.

(i) Street Trees

1. The primary planting trees along Main Street shall be *Platanus acerifolia* (Sycamore); the Alternate Tree shall be: *Quercus agrifolia* (Coast Live Oak); and the Accent Tree shall be *Prunus* species (Flowering Cherry).

(j) On-Site Signs

1. Master Sign Program. Prior to the installation of any permanent sign, a Master Sign Program for the entire project site shall be submitted to the Community Development Department for review and approval. This subsection sets forth general regulations of which the Master Sign Program shall comply. Where there is a conflict between the Oakley sign ordinance and regulations in this subsection or the Master Sign Program, the regulations in this subsection and the Master Sign Program shall take precedence.
2. Signs may be erected subject to the following provisions:

a. Wall Signs.

- i. Area. The total area of all signs attached to any one building shall not exceed three (3) square feet per foot of building frontage. For tenants with more than one frontage, maximum sign area may be based on the length of the longest frontage. Once the allotted sign area has been computed, it may be distributed over the front, sides, or rear of the building per the Master Sign Program.
- ii. Location. All signs attached to a building shall be attached flat against the building and parallel thereto. Signs shall not project above the building wall or peak of the roof of the building.

b. Shopping Center Freestanding Signs.

- i. Computation of Area. Freestanding signs are to be computed as the total of the copy area of all tenant identification, including graphics and logos on one side of a double faced sign.
- ii. Area. For the purposes of analysis in the DEIR the total area of all shopping center freestanding signs shall not exceed one thousand (1,000) square feet. The total square footage for freestanding signs allowed for by the Master Sign Program shall be determined at the time of approval of the Master Sign Program and may be less than, but not exceed 1,000 square feet.
- iii. Height. The maximum height of one (1) shopping center freestanding sign shall be forty-five (45) feet, to the highest point of the structure. The maximum height of all other shopping center freestanding signs shall be fifteen (15) feet.
- iv. Location. The maximum forty-five (45) foot high shopping center freestanding sign shall be located within the western part of the shopping center, (west of the project driveway located between Major Pads B and C).
- v. Number. There may be up to one (1) shopping center freestanding sign per shopping center driveway entrance.

- vi. Setback. Freestanding signs shall be located outside of the public right-of-way and at least ten (10) feet behind the back of sidewalk.
 - vii. Shopping center identification. Each shopping center freestanding sign shall include the name of the shopping center. The shopping center name shall be placed near the top of the sign, above tenant signage. The shopping center name shall not be included in the area calculation as defined in subsection b (ii).
- c. Illumination. Lighting shall be arranged so as not to produce a glare on other properties in the vicinity and the source of light shall not be visible from adjacent property or public street. Decorative exposed neon used for graphics, highlight lighting, or text shall be permitted. Backlighting of awnings, canopies and similar structures shall be based on the following:
- i. If the lighting system is fluorescent, the fixtures shall be UL rated for protected outdoor use with cool-white tubes not to exceed 800 milliamps and not to exceed 2 tubes per fixture.
 - ii. If the lighting system consists of (or includes) other than fluorescent tubes, the luminance level produced by such system shall not exceed the luminance level produced by the fluorescent tubes using the above criteria.
- d. The Specific Plan anticipates that the commercial center will include additional types of signs common to shopping centers, including but not limited to directional signs, temporary signs, and window signs. The Master Sign Program shall address additional signage that is otherwise permitted in commercial zones in accordance with the Oakley sign ordinance.

6.6 Energy Conservation and Waste Reduction

Individual development phases within the project area shall comply with the following standards, which are intended to reduce waste and use of non-renewable sources of energy, while promoting an overall program of long-term energy conservation. Compliance shall be verified by the Community Development Director or his/her designee with each application for a building permit.

- (a) Daylighting (compliance mandatory for all buildings 100,000 square feet or greater)

Each interior public space shall be equipped with a “daylighting system” to reduce use of electricity for area lighting. The daylighting system shall include switching mechanisms to automatically and continuously dim all lights as the daylight contribution increases through use of properly placed windows and skylights.

- (b) Night Dimming (compliance mandatory for all uses)

Each business that operates on extended hours shall provide an automatic switching system to dim lighting within all interior public spaces to between 60% and 70% illumination between the hours of 10:00 pm and 7:00 am (standard time).

- (c) Energy Efficient HVAC Systems (compliance mandatory for all uses)

All mechanical equipment provided for the purpose of heating and cooling interior public spaces shall satisfy all California title 24 requirements; in addition, all such equipment shall achieve a minimum EER (energy efficiency ratio) of rating of 10.0.

- (d) Central Energy Management (compliance mandatory for all buildings 100,000 square feet or greater)

Each Major Retail use as identified on the approved development plan shall be equipped with energy management systems which are monitored and controlled by a designated energy conservation manager whose office shall be located either on the premises or at an approved off-site location. The energy conservation manager shall be trained in optimization of all available technology, and shall monitor energy usage and performance to provide continuous compliance 24-hours a day.

- (e) Water Heating (compliance mandatory for all buildings 100,000 square feet or greater)

Waste heat from the refrigeration equipment shall be captured in order to pre-heat water for use in kitchen preparation applications or other on-premises uses requiring heated water.

- (f) White Roofs (compliance mandatory for all uses)

All built-up roof surfaces (excluding hip, gable and other decorative architectural elements) shall be provided with a "white" membrane roof, also known as a cool roof. The solar reflectivity of such roof membrane systems are intended to lower interior cooling loads in the Oakley climate zone by roughly 10%, compared to conventional darker roof colors. Solar reflectivity on roofs also reduced the amount of conversion of

UV rays to infrared heat, possibly reducing the heat island effect created by most large, developed parcels of land.

(g) Interior Lighting Systems (compliance mandatory for all uses)

All interior public spaces shall be provided with lighting systems that utilize high efficiency T-8 fluorescent lamps and electronic ballasts, or approved equivalent systems.

(h) LED Interior Signage Illumination (compliance mandatory for all buildings 100,000 square feet or greater)

Light emitting diode (LED) lighting, or an approved equivalent, shall be used for all internally illuminated building signage. LED lighting technology is recognized as consuming substantially less electricity than fluorescent or other illumination sources. In addition, the longer lamp life afforded by LED technology substantially reduces need to manufacture and dispose of fluorescent lamps.

(i) Integrally Colored Concrete Floors (compliance mandatory for all buildings 100,000 square feet or greater)

A minimum of 80% of the interior public floor space within Major Retail buildings shall be of a natural or integrally colored concrete finish. This requirement is intended to limit the relative volume of carpet and vinyl tile finishes, thereby reducing the ultimate need for manufacture and disposal of PVC, and also reducing the use of chemical cleaners, wax, and wax strippers.

(j) Recycled Materials in Building Construction (compliance mandatory for all buildings 200,000 square feet or greater)

The following materials shall be used in the fabrication of Major Retail use building systems:

(1) Steel Recycling: a minimum of 80% of the structural steel used in the construction of buildings shall be recycled.

(2) Recycled Plastic: a minimum of 80% of all plastic baseboards and plastic shelving used in Major Retail buildings shall be manufactured from recycled material.

SECTION 7. IMPLEMENTATION STRATEGIES

7.1 Future Project Entitlements

Adoption of this Specific Plan, authorizes development of up to 770,000 square feet of commercial buildings and related site improvements, based on the approved Development Plan and approved land uses identified in Section 3, and subject to the Development Standards established in Section 6. Phased buildout of the approved Specific Plan development must provide for concurrent funding and construction of circulation and infrastructure improvements as identified in Sections 4 and 5. The Specific Plan is accompanied by a concurrent rezoning to the SP-2 Zoning District, and requires implementation of all mitigation measures adopted as part of the concurrently prepared and certified project-level environmental impact report. Following certification of the EIR and adoption of the Specific Plan, the City of Oakley and its responsible agency partners will consider the following additional entitlements prior to authorization of construction:

(a) City of Oakley. Future development within the Specific Plan project area will be subject to compliance with the following additional entitlement actions, and application and review procedures:

1. *Zoning Ordinance Amendment to Create the SP-2 District*: An amendment to the Municipal Code shall be prepared and adopted, concurrently with approval of the Specific Plan, to establish new Section 9.1.1005 “Specific Plan No. 2” (SP-2). The SP-2 District will require that all new development and land uses be consistent with the Specific Plan.
2. *Rezoning*: Rezoning from current “Heavy Industrial” (HI) District to the Specific Plan No. 2 (SP-2) District to implement the Specific Plan. Rezoning to the SP-2 District shall take place concurrently with adoption of the Specific Plan project, in order to provide for implementation of the Development Plan, and all policies and standards adopted as part of the Specific Plan.
3. *Architectural Review*: No application for building permit may be applied for until the City Council has first granted Architectural Review approval for the proposed buildings and related improvements. Architectural Review approval shall be granted only upon adoption of a finding of consistency with Section 6 (Development Standards) of the Specific Plan. Applications shall include a complete set of preliminary building elevations, site plans, and landscape plans, identifying all materials, colors, textures for each Principal and Secondary Retail building, as well as landscaping and paving materials.
4. *Subdivision*: The subject 76.4-acre site may not be subdivided without review and approval of a parcel map or tentative and final map, in accordance with City Subdivision Ordinance and State Map Act requirements. Tentative and/or Parcel Map applications shall be reviewed for consistency with the Specific Plan. The

subdivision application shall specifically address all project phasing issues as discussed under Section 6.1 of this Specific Plan, and shall provide for the recordation of reciprocal cross easements to provide for shared use of parking, extension of utilities, and pedestrian and vehicular circulation throughout the entire site.

5. *Construction Permit Review:* Encroachment permits, grading and building permits, and related construction permits may be issued by the City based on the approved Specific Plan and the foregoing related discretionary entitlements. Prior to issuance any such permits, applicants shall prepare and submit improvement plans to the City's Public Works and Engineering Division for review and approval. Improvement plans shall address all off-site and common on-site utility and circulation system improvements, including those required pursuant to assigned mitigation requirements under the certified project-level EIR. In addition, each such construction permit shall be subject to a determination by the Community Development Director of consistency with the approved Development Plan and Development Standards contained in the Specific Plan.
6. *Project Maintenance Program:* As part of the Architectural Review for the first Major tenant the applicant shall submit a master Project Maintenance Program (PMP), to assure that all landscaping, water elements, pavement areas, buildings, mechanical systems, and other site and building improvements are properly cared for and will retain high-quality appearance and proper operation. The PMP will include plans for maintenance of all building(s) and site improvements throughout the life of the project. The PMP may include provisions acceptable to the Community Development Director that address reuse of the Major Retail building(s) in the event that the building(s) becomes vacant. The City may collect a Letter of Credit in an amount acceptable to the City Manager, but not to exceed \$25,000 per pad, from each of the Major Retail pads to guarantee adherence to the standards for maintenance and reuse as called for in the PMP. The City may draw upon these funds only in the event of violation of the PMP. This requirement will help to assure long-term compliance with a range of aesthetic, acoustical, land use, water quality and other mitigation measures from the project EIR.
7. *Minor Modifications – Community Development Director:* Minor modifications to the Development Plan involving the building areas, building footprint configurations, and related site improvements for both Major and Secondary Retail uses may be approved by the Director of Community Development, subject to the following: (a) individual changes to the floor area of Secondary Retail buildings D through S shall be limited to a maximum of 10,000 square feet; (b) the Total Floor Area of all buildings in the Specific Plan area shall not exceed 690,000 square feet; and (c) all Development Standards provided for in Section 6 shall be satisfied.

8. *Minor Modifications – City Council:* Additional modifications to Secondary Retail buildings shown on the Development Plan may be approved by the City Council, subject to compliance with all applicable Development Standards provided for in Section 6, and the Total Floor Area limitation of 690,000 square feet. Modifications to the approved Development Plan that split up any of the three Major Retail Pads to smaller individual stores are allowed, subject to the review and approval of the City Council. All other requirements of the Specific Plan regulations, design standards and goals would remain as approved. Any modification resulting in breaking up of a Major Pad would still be subject to review and approval to ensure all internal circulation functions, traffic counts remain within the threshold studied in the traffic analysis, adequate parking exists, and that the phasing of improvements are feasible. Also, any modification would be analyzed to ensure the mitigation measures adopted in the EIR certification resolution would still be adequately implemented. In the event a Major Retail Pad is split up, the City Engineer will determine how best to ensure any improvements tied to that pad are still implemented in a timely fashion.
 9. *Environmental Review:* The foregoing applications shall include detailed information relating to the size of buildings, proposed uses, and other physical and operational factors, as identified in the City’s CEQA Checklist review form. The project’s effects on the environment will be administratively evaluated in relationship to this Specific Plan and the corresponding certified project-level EIR. If found to be consistent with the Specific Plan and its EIR, no further environmental review shall be required for the application. Each such Checklist review shall specifically include assignment of applicable mitigation requirements from the Mitigation Monitoring Plan (MMP) prepared with the Final EIR.
 10. *Development Agreement:* A Development Agreement between the project’s developer(s) and the City of Oakley will address infrastructure, mitigation and monitoring, and other elements related to development of the site, all of which will be consistent with the River Oaks Crossing Specific Plan and certified Environmental Impact Report.
- (b) Other Responsible Local, State, and Federal Agencies. The following additional actions will be required in order to implement the River Oaks Crossing Specific Plan and permit development within the project boundaries:
1. *Diablo Water District Permit:* A separate permit shall be obtained from DWD, and applicable fees paid, prior to connecting to any of the District’s water lines serving the project.
 2. *Ironhouse Sanitary District Permit:* Permit shall also be obtained from DWD, and applicable fees paid, prior to connecting to the District’s trunk sewer lines serving the project for discharge of wastewater effluent.

3. *California Regional Water Quality Control Board:* National Pollution Discharge Elimination System (NPDES) compliance is required for all aspects of project design and discharge. The State Water Resources Control Board (SWRCB) has issued the General Construction Storm Water Permit (Water Quantity Order 99-08-DWQ) “General Permit” which governs storm water and non-storm water discharges from this project. The Permits require the preparation of a Storm Water Pollution Prevention Plan (SWPPP) in conformance with the requirements of the General Permit. These documents must be in place prior to initiation of project construction.
4. *California Department of Transportation Encroachment Permits:* Although State Route 4 will eventually be reassigned to the SR 4 Bypass alignment, Main Street will continue to function as the officially designated State Route 4 for the next several years. All frontage improvements, including pavement, landscaping, curb cuts, utility trenches and other work within the State right-of-way will therefore require a separate Caltrans encroachment permit during this period.

7.2 Facility Maintenance

All landscaping, water elements, pavement areas, buildings, mechanical systems and other site and building improvements are to be properly cared for to retain high-quality appearance and proper operation throughout the life of the project. In order to implement this policy, Section 7.1(a)(6) provides for the preparation and approval of a detailed Project Maintenance Program (PMP). This requirement will help to maintain project quality over time, and will specifically assure long-term compliance with several mitigation measures included in the project Mitigation Monitoring Plan (MMP).

7.3 Plan Administration

This Specific Plan has been prepared to serve as a tool to implement broad land use, economic development, and related policies included in the Oakley 2020 General Plan. Government Code Sections 65450-65457 call for the Specific Plan to be adopted by the City Council and Redevelopment Agency to provide for the systematic implementation of General Plan policies affecting the Cline Property. This Specific Plan includes an approved Development Plan, and a set of approved permitted uses, development standards, design guidelines, energy conservation and waste reduction measures, and related policies. Compliance with these policies must be demonstrated at every step in the discretionary entitlement and Construction Permit Review process for future site development. In particular, the subsequent review and approval of all such entitlements and permits must show consistency with the intent and policies outlined in this document. These and other related permits must include detailed requirements for implementation of plan policies, including mitigation measures and maintenance requirements. Any proposed project that deviates from these standards and requirements must be carefully evaluated to determine the extent and significance of exemption or plan modification. Minor Modifications to the approved Development Plan are provided for in Section 7.1(a) and may be considered separately by the Community

Development Director or Planning Commission. Conflicts with Specific Plan policies and cumulative development and/or land use standards shall be evaluated as part of a modification to the Specific Plan, which is subject to City Council approval. Deviations and modifications that are not within the scope of the certified EIR shall be subject to CEQA, including the preparation of an Initial Study and any applicable environmental document.